



**MONTGOMERY COUNTY PLANNING DEPARTMENT**  
 THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

January 7, 2010

MCPB  
 Item # 8 B  
 1-14-10

**MEMORANDUM**

**TO:** Montgomery County Planning Board

**VIA:** Dan Hardy, Chief *DKH*  
 Move/Transportation Planning Division  
 Sue Edwards, Team Leader *SUE*  
 Vision/Community-Based Planning

**FROM:** Larry Cole: 301-495-4528, for Transportation Planning *LC*

**PROJECT:** Goshen Road from 650 feet south of Girard Street  
 to 1,000 feet north of Warfield Road  
 CIP No. 509337

**REVIEW TYPE:** Mandatory Referral No. MR2009805

**APPLICANT:** Montgomery County Department of Transportation (MCDOT)

**APPLYING FOR:** Plan Approval

**COMMUNITY-BASED PLANNING TEAM AREA:**

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**RECOMMENDATIONS:**

**Mandatory Referral**

Staff recommends that the Board approve the proposed project (see Attachments 1A and 1B: Vicinity Map) with the following comments:

**Roadway**

1. Reduce the width of the travel lanes adjacent to the median to 11.5 feet and increase the width of the bike lanes to 5.5 feet to be more consistent with the County's new road standards.
2. Curb radii should be reduced to thirty feet wherever possible but should not be greater than fifty feet.

## **Pedestrian and Bicycle**

3. Minimize locations where sidewalks and paths are immediately adjacent to the curb but where necessary, the sidewalk or path should be widened by two feet per AASHTO recommendations.
4. On the bridge over Cabin Branch, widen the sidewalk to eight feet and the shared use path to twelve feet.
5. Provide sidewalks or Master Plan-recommended shared use paths (where applicable) on all side streets within the limits of work. Where they cannot be accommodated as part of this project, grade the area to facilitate future sidewalk construction.
6. Consider reconstructing the existing sidewalk between Girard Street and the southern project limit as an eight-foot-wide shared use path and provide a better connection to the proposed path to the north.
7. Extend the proposed shared use path from Warfield Road to Lochaven Drive.
8. Construct a shared use path on the south side of Snouffer School Road.
9. Provide railings for sidewalks and shared use paths where they are adjacent to drop-offs, e.g. at culverts.

## **Handicapped Access and Ramps**

10. Wherever possible, construct all sidewalks and shared use paths at intersections to be outside handicap ramps so that a level surface is maintained.
11. Clearly identify and provide handicapped access to and from all bus stops within the project limits.
12. Provide ADA-compatible crossings at the intersection of all public streets.
13. Construct eight-foot-wide handicap ramps for shared use paths at intersections.

## **Lighting**

14. Provide higher levels of lighting at intersections, as recommended by AASHTO, to ensure the safety of pedestrians and bicyclists. Locate the street lights at intersections slightly ahead of crosswalks so that pedestrians are more easily visible to drivers approaching the crosswalk.
15. Provide continuous lighting of the sidewalk and shared use path to the levels recommended by AASHTO.
16. Consider using LED street lights to minimize maintenance and utility costs.

## **Streetscaping**

17. Plant 2-1/2"-3" cal. street trees along Goshen Road at fifty feet on center, Japanese Sephora in the median and Japanese Zelkova in the landscape panels between the curb and sidewalk/path. Adjust the typical section to increase the panel width to provide a sufficient clear zone where possible.
18. Provide a consistent landscaping treatment behind the sidewalk/path at major intersections to reflect their status as gateways to communities.
19. Plant street trees along intersecting streets within the limits of work.
20. Use an ashlar slate pattern for concrete medians.

## **Noise**

21. Include noise mitigation for the roadway construction in this project.

## **Parks**

22. A park permit will be required for all work proposed on parkland.

### Stewartown Local Park

23. Access to the park must be maintained to all park facilities during construction, and no loss of parking will be permitted.
24. Provide black vinyl-coated chain link fence for the length of the rectangular field (M-NCPPC Standard Detail No. 109, Outfield Chain Link Fence (10'-0" High).
25. Provide screening to replace buffer that will be cleared; M-NCPPC will provide a markup of the plant material, including quantities, spacing, species, and size. Trees must be planted with 2-1/2" caliper at 20' O.C. spacing, with additional ornamental trees and shrubs with two year maintenance, and invasive removal.

### Cabin Branch Stream Valley Park

26. Establish a 100 feet wide forested stream buffer on either side of the stream (total of 200 feet wide), from Goshen Road to 100 feet downstream of the pond limits, a total distance of approximately 1,000 feet. The planting plan must include non-native invasive removal, deer protection, and follow-up care for two years, as required in the latest version of M-NCPPC's "Planting Requirements for Land Disturbing Activities and Related Mitigation on M-NCPPC Montgomery County Parkland". Invasive removal treatment must be provided for the entire limit of disturbance within parkland. M-NCPPC Forest Ecologist will review the planting plan and determine whatever changes are necessary. The 200' wide stream buffer planting cannot be used to meet reforestation

requirements for the project. This buffer planting must be installed simultaneously with the roadway construction, and shall be completed prior to the completion of the contract for roadway construction.

27. Provide a stable outfall, including improvements as needed to the stream within the project limits. These will be determined during final design but will include spot treatments of rock grade and bank stabilization structures in the vicinity of the outfalls as well as the steep horizontal bends approximately 400 feet from Goshen Road.
28. Design the pond to provide habitat for amphibians and birds by providing different zones to create a pond/wetland system, similar to figure 3.8 of the Maryland Department of the Environment's Stormwater Design Manual. The details of the appropriate system will be determined after groundwater and soil characteristics have been investigated during final design.
29. Construct a natural surface trail along Cabin Branch Stream to provide access to Goshen Road within the grading and forest buffer creation limits per M-NCPPC natural surface trail design standards. The pond and trail shall be integrated together, with grades that are natural and organic in appearance, so that the pond is an attractive feature along the trail and provides passive recreational opportunities for residents. If, during final design, in Parks staff's judgment, the pond cannot be attractively graded into the landscape because of design constraints that arise during final design, then a portion of the water must be stored underground in pipes.
30. During final design, replace the proposed sand filter with a regenerative stormwater conveyance system between Goshen Road and the pond to provide water quality treatment. This practice is more natural in appearance than the sand filter and would provide increased habitat opportunities. This replacement must occur unless MCDOT can demonstrate to Parks staff's satisfaction that it cannot be accomplished.
31. The pond maintenance will be performed by Montgomery County Department of Environmental Protection, and the property will be placed in a stormwater management easement to accommodate construction and maintenance activities.

### **Post-Construction Monitoring**

32. Monitor the safety of the road for a period of three years after construction and make publicly available a comparison with the existing safety record to determine the effectiveness of the improvements and the design decisions made in implementing them.

**PREVIOUS BOARD ACTION, RELATED PROJECTS, AND SITE DESCRIPTION:** The Planning Board approved the Facility Planning Project Prospectus for this project on June 23, 2004. Staff's memo for that review may be found on [http://www.montgomeryplanningboard.org/meetings\\_archive/04\\_meeting\\_archive/agenda\\_070104/item3\\_070104\\_opt.pdf](http://www.montgomeryplanningboard.org/meetings_archive/04_meeting_archive/agenda_070104/item3_070104_opt.pdf)

The Mandatory Referral for the Replacement of Goshen Road Bridge over Cabin Branch was approved by the Board on October 19, 2000. The project constructed half of the ultimate bridge in 2005. Many of the Board's comments were directed at minimizing the impact to the Goshen Elm, an English Elm whose trunk is 236" in circumference and is believed to be the largest of its species. Since the Board's review, the land around the tree was acquired by M-NCPPC and was dedicated as Goshen Elm Neighborhood Conservation Park in 2006.

### **PROJECT DESCRIPTION**

The project would widen Goshen Road between 650 feet south of Girard Street to 1,000 feet north of Warfield Road for a distance of approximately 3.5 miles. The segment from the southern limit to Odendhal Avenue is entirely within the City of Gaithersburg (about 0.2 mile). The segment from Odendhal Avenue to Emory Grove Road is bordered by the City of Gaithersburg on the east side of the right-of-way; the right-of-way and the west side is in the unincorporated area of Montgomery County (about 0.5 mile). The rest of the project is within the unincorporated area of Montgomery County.

The existing two-lane undivided roadway of Goshen Road would be widened to a four-lane divided roadway plus five-foot-wide on-road bike lanes in a 107-foot-wide (min.) right-of-way. An eight-foot-wide shared use path would be constructed on the west side of the road and a five-foot-wide sidewalk would be constructed on the east side of the road.

### **FINDINGS**

#### **Master Plan**

The project to widen Goshen Road to four lanes is consistent with the Gaithersburg Vicinity Master Plan recommendation for a four- to six-lane road.

The proposed shared-use path is recommended in the Countywide Bikeway Functional Master Plan; the proposed bike lanes are consistent with the recommendation for on-road bike accommodation.

#### **Road Code and Associated Executive Regulations**

The proposed typical section does not conform to an approved County Standard since no standard exists for a four-lane Major Highway in a Suburban area.

The proposed 12-foot width of the lanes adjacent to the median is not consistent with the new Suburban standard width of 11.5 feet. The proposed five-foot bike lane width is not consistent with the new standard of 5.5 feet.

The proposed width of the shared use path is eight feet, consistent with the new County standard of eight-to-ten feet.

Shade trees would be provided in the median, consistent with the new Street Tree Standard, but would not be provided along the sides of the road between the curb and the sidewalk.

The proposed target speed of 40 mph is within the recommended target speed range of the Target Speed Standard.

The goal of the new Stormwater Management Standard is to infiltrate 25% of stormwater runoff within or adjacent to the right-of-way of closed-section (curbed) roads using Vegetated Integrated Management Practices. The proposed project would not meet that goal, but would infiltrate about 12% of the runoff from paved surfaces.

## **AASHTO**

The long lengths of sidewalks and shared use paths immediately adjacent to the roadway curb are not consistent with the AASHTO recommendation to be two feet wider in these circumstances to provide a greater measure of pedestrian safety and comfort.

The proposed width of the shared use path is eight feet, inconsistent with the AASHTO standard of ten feet.

## **Mandatory Referral of the Replacement of Goshen Road Bridge over Cabin Branch**

The project is generally consistent with the Planning Board's 10/19/00 comments on the Mandatory Referral for the bridge (see Attachment 2), with the exception that an ADA-compatible crossing of Goshen Road at Rock Elm Drive has not been provided.

## **Planning Board Comments on the Project Prospectus**

The project is consistent with the Planning Board's 6/23/04 comments on the Project Prospectus (see Attachment 3) with the following exceptions and/or comments:

- Alternatives were investigated to avoid impacts to the Black and White Inn. The proposed design will require that the building be moved back from Goshen Road on its current site, a condition reflected in the County's Master Plan for Historic Preservation, as part of the *Amendment to the Master Plan for Historic Preservation: Damascus-Goshen Historic Resources*, approved by the County Council in January 2009. [http://www.montgomerycountymd.gov/content/council/pdf/res/2009/20090224\\_16-828.pdf](http://www.montgomerycountymd.gov/content/council/pdf/res/2009/20090224_16-828.pdf) , page 10.

- Noise impact mitigation was not incorporated into Phase II of Facility Planning. Noise impacts are discussed in further detail below.

## **STAFF ANALYSIS**

### **Master Plan Background**

The Gaithersburg Vicinity Master Plan recommends Goshen Road between Odendhal Avenue and Warfield Road as a 4-6 lane Major Highway in a 120-foot right-of-way. The Master Plan discussion of the recommended improvements for this road is as follows (from page 80):

*Improvements are recommended from Odendhal to Snouffer School Roads. These may include the reduction of horizontal and vertical curves, improvement of intersections, and widening. This highway is anticipated to be heavily used by traffic generated from several major developments along its length, major residential development of Montgomery Village East, north of Snouffer School Road and east of Goshen Road. The transportation analysis for this Plan indicates the need for such improvements.*

The Countywide Bikeways Functional Master Plan recommends that Goshen Road have both a shared use path and on-road accommodation via a signed, shared roadway or bike lanes.

### **Road Code Background**

The general update of the County's Road Code was adopted in late 2007 and the Executive Regulations with the new County Road Standards were adopted in late 2008. This project had been under design before the Road Code effort started and although it was almost ready for Mandatory Referral, it was substantially redesigned to meet the goals of the Road Code. The two design considerations were a change in the design speed from 45 mph to 40 mph and a large expansion in the number of stormwater management facilities. Because the policy and design efforts were done concurrently, the project is not a perfect fit with the new Road Code and standards, but is much closer than it would have been without this redesign.

### **Typical Roadway Section**

The project area lies within the Suburban area of the county, as defined by the 2007 Road Code update. The project does not conform to the road standards recently approved as Executive Regulations as a follow-up to the Road Code update. This is true primarily because no standard for a four-lane Major Highway was created. Staff believes that the typical roadway section is satisfactory with the 0.5-foot change in roadway striping and adjustments to sidewalk/path locations noted below.

## **Pedestrian and Bicyclist Accommodation**

### **On-Road Bike Accommodation**

The Master Plan recommends either a signed shared roadway or bike lanes; five-foot-wide bike lanes are proposed and would provide the better accommodation. The width in Std. 2004.10 (for a four-lane arterial roadway) calls for 5.5-foot-bike lanes; we recommend that the 5.5-foot-wide bike lanes be provided and that the 12-foot-wide travel lanes adjacent to the median curb be reduced to 11.5 feet, also consistent with Std. 2004.10. Making both recommended striping changes would result in no change to the proposed curblines.

### **Off-Road Bike Accommodation**

Off-road bike accommodation would be provided via an eight-foot-wide shared use path along the west side of Goshen Road. While the previous County standard width for paths was eight feet, the new Road Code increased this to eight-to-ten feet.

The path would be separated from the roadway by a five-foot-wide landscape panel in most areas, but the panel would not be provided in some areas. We believe that it would be difficult to increase the general width of the path to ten feet, but it should be increased in certain areas as discussed in greater detail in Pedestrian Accommodation below.

At the northern and southern project limits, the proposed shared use path does not extend as far as the roadway construction. Between Girard Street and the southern project limit, a widening of the existing sidewalk to eight feet should be considered and a better connection to the proposed path to the north should be provided. North of Warfield Road, the proposed shared use path should be extended to Lochaven Drive.

A shared use path is shown on the north side of Wightman Road extending from the Goshen Road/Snouffer School Road intersection. The Master Plan of Bikeways does not recommend this path but instead recommends one on Snouffer School Road. We recommend that a path be constructed on the south side of Snouffer School Road within the project limits.

### **Pedestrian Accommodation**

Pedestrians would be accommodated on the proposed shared use path on the west side of Goshen Road as well as on the five-foot-wide sidewalk proposed on the east side of the road.

While most of the proposed sidewalks are separated from the roadway by landscape panels with street trees, there are long lengths of sidewalks and shared use paths that are proposed adjacent to the curb to avoid or minimize environmental or community resource impacts – approximately 1,700 linear feet of sidewalk and 2,200 linear feet of path. The proposed bike lanes would mitigate the usual problem of the users of such sidewalks being subjected to splashes from the roadway since vehicles will be farther away, but such sidewalks and paths may still be made unusable at times in the winter because of snow plowed from the roadway, a problem that was made even clearer than usual this year. These locations should be minimized, but where location



adjacent to the curb is necessary to minimize property or environmental impacts, the sidewalk or path should be widened by two feet per AASHTO recommendations.

On the bridge over Cabin Branch, where the shared use path and sidewalk would be constrained by the curb on one side and the bridge parapet on the other, more widening should be provided. We recommend that the sidewalk on the bridge be widened to eight feet and the shared use path to twelve feet.

Sidewalks should be provided on all side streets within the limits of work. Where they are not provided, the area should be graded for future sidewalk construction.

All of the intersections along Goshen Road within the limits of this projects would of course become much bigger with the widening of the road. Dual handicap ramps should be provided at each intersection corner to meet ADA Best Practices and to provide the shortest road crossing for all pedestrians.

Two Ride-On bus routes travel along portions of Goshen Road within the project limits. We recommend that the bus stops be clearly identified and that handicapped-accessible access be provided to and from all bus stops.

### **Target Speed**

The existing speed limit of this road is 35 mph. The design of this project was begun before any substantial progress had been made on the Road Code revision and originally had a design speed of 45 mph. Because of the influence of the Road Code changes, MCDOT decided to lower the design speed and target speed to 40 mph and raise the speed limit to 40 mph.

The guidance in the County's new Target Speed standard states that the designer must evaluate existing conditions along the corridor including existing operating speeds, speed limits, the safety record of the road, and the pedestrian and bicycle accommodation provided. The existing operating speed exceeds the existing posted speed of 35 mph, the abutting development along the road does not provide much in the way of encouraging lower speeds, and it will be more difficult to moderate speeds with the wider typical road section. While these are factors that influenced the designer to choose the higher target speed, we also note that the road has a pedestrian crash rate five times the statewide average for similar roads.

Pedestrian and bicyclist accommodation, particularly at intersections, should be given higher consideration than usual given that the posted speed is being raised on a road that is currently very unsafe for pedestrians. Since the road would be brought up to standard with the proposed construction, the crash rates should be significantly reduced if these precautions are taken, but a greater effort toward minimizing crossing distances and providing median refuges is needed.

We recommend that the safety of the road be tracked for a period of three years after construction and compared to the existing safety record to determine the effectiveness of the improvements and the design decisions made in implementing them.

## Lighting

Continuous street lighting is not yet shown on the plans but is intended to be included in the project. We recommend that the following is considered when the street lighting is designed:

- Providing continuous lighting of the sidewalk and shared use path to the levels recommended by AASHTO.
- Using LED street lights to minimize maintenance and utility costs.
- Providing higher levels of lighting at intersections, as recommended by AASHTO, to ensure the safety of pedestrians and bicyclists; and locating the street lights at intersections slightly ahead of crosswalks so that people in the crosswalks are front-lighted rather than just silhouettes, making them more easily visible to drivers approaching the crosswalk.

## Landscaping/Streetscaping

The proposed project would provide street trees along both sides Goshen Road as well as in the eighteen-foot-wide median. We recommend that the street trees should be 2-1/2"-3" cal. planted at fifty feet on center, Japanese Sephora in the median and Japanese Zelkova in the landscape panels between the curb and sidewalk. We recommend that street trees be planted also along intersecting streets within the limits of work.

Where the median width is less than six feet, concrete is proposed rather than grass because of difficulties with maintenance. We recommend that an ashlar slate pattern be used for these areas to improve their appearance and hide the grit thrown from the roadway better.

MCDOT has agreed to plant the recommended trees in the median but has not agreed to plant street trees between the curb and sidewalk/shared use path. The typical section for Goshen Road is 107 feet wide, less than the 120 feet recommended in the Gaithersburg Vicinity Master Plan. Even so, the typical section shown in the plans could be adjusted to provide sufficient space for street trees by shifting the curb and sidewalk/path three feet to meet the 10-foot minimum offset between the center of the tree and the travel lane that is required for 40 mph roads per the new Tree Standard. MCDOT has met with two neighborhood groups who have concerns about the impact of the project, and MCDOT has agreed to reduce the width of the typical section further to 103 feet, which would make it impossible to get street trees in their normal location and still meet the new standard (see Attachment 4).

While this project is four miles long, it is unusual in that it runs through a very developed area but has less than a dozen single family homes fronting on it. Thus, we do not have much of a problem with what is usually the hardest issue to tackle – impacts on people's front yards. There are other obstacles however to getting a consistent line of street trees between the curb and sidewalk/shared use path: conservation easements, proposed stormwater management facilities, wetlands, and existing development. But we believe that locating trees in the landscape panel can be achieved through about half of the project length and that the proposed trees in the median

would help visually to tie the whole project together even where trees cannot be located in the landscape panel. Where we have major intersections such as Midcounty Highway, the focus should be on having a consistent landscape treatment behind the sidewalk/path on all four corners.

Staff's recommendation to allow such a large SWM facility on parkland was based on MCDOT's good work toward meeting the stormwater management goals of the new Road Code as well as the constraints of the local topography. It was also based however on their providing a good design for the rest of the project. Shifting the sidewalk/path to accommodate street trees adjacent to the curb will improve the streetscape, providing a greater sense of separation and protection for pedestrians and bicyclists on the sidewalk and path; providing passive traffic-calming by narrowing the apparent width of the roadway; and shading the pavement, cooling stormwater runoff. It is important that MCDOT make a good faith effort toward meeting this recommendation to receive the park permit in the spirit in which it has been offered.

### **Noise Impacts**

The Planning Board's 2004 comments on the Project Prospectus included a recommendation to include noise impact mitigation into the project scope. The noise impacts caused by the project were not investigated in Phase II of Facility Planning, but MCDOT staff have said that this work will be done during final design. They note that the project's design changed during Phase II and that this could have caused a change to the design of the noise mitigation also.

Including noise impact mitigation in projects is not consistent with the County's Noise Policy, which states that all candidates for mitigation must essentially get in line and be judged against previously identified candidates in the Noise Mitigation Program. MCDOT included this information in their response to the Board (see letter dated 7/28/04, Attachment 5), but they now note that the Noise Mitigation Program is currently on hold.

When the Planning Board reviewed the Montrose Parkway West project in 2001, you recommended that noise walls be included; the Council subsequently included the noise walls in the PDF for the project, making an exception to the County's Noise Policy. Staff believes the Board should repeat your earlier recommendation that the noise mitigation for the roadway construction be included in the project as a similar exception to ensure that it is done in a timely manner.

### **HISTORIC RESOURCES**

The Black and White Inn, at the southeast corner of Goshen Road and Warfield Road, is the only resource within the project limits that is listed in the Master Plan for Historic Preservation. The wording in the Amendment states,

*If Goshen Road is widened, the resource will need to be moved from its current location. Its new location should be on its current parcel, outside the road widening project, but retaining a reasonable environmental setting and also retaining the orientation of the structure to Goshen Road.*

*Environmental Setting: One acre, parcel P258. The house is currently set within the rights of way for Goshen and Warfield Roads. One non-contributing, non-historic garage is located on the parcel. The garage may be demolished in the event that the Black and White Inn structure is moved for the Goshen Road widening project.*

The proposed project is consistent with the above guidance.

## **ENVIRONMENTAL**

Forest Conservation requirements and the request to work in an existing Conservation Easement are addressed in a separate staff packet.

### **Stormwater Management**

The new Stormwater Management standard that was created as an Executive Regulation as part of the Road Code revision promotes stormwater runoff infiltration within or adjacent to the roadway right-of-way using Vegetated Integrated Management Practices, and sets a goal of 25% for closed-section roads. While the proposed project would greatly expand the use of these techniques and provide a substantial increase in infiltration from current conditions, it will still infiltrate only about 12% of the runoff from paved surfaces, demonstrating the challenges to be addressed in meeting the goals.

In the past, the Planning Board's direction has been that stormwater management facilities for non-park projects should be located on parkland only as a last resort. With the changes in the Road Code, and subsequently more stringent requirements under State Law, more SWM will be required and requests to locate these facilities on parkland are expected, as has occurred on this project. While these facilities can be located underground at some locations, they are much more expensive than surface facilities. We support the project proposal to locate some of the SWM on parkland, with the conditions described below under Park Impacts and reflected in the proposed comments.

## **PARK IMPACTS**

### **Stewartown Local Park**

The project would require taking 0.6 acre of parkland in fee simple and 0.44 acre of grading easement in Stewartown Local Park to accommodate the road widening.

Access – The project would construct a median that would prohibit left turns for northbound vehicles into the smaller of the two park parking lots; vehicular access would be provided via a median break at the entrance to the larger park parking lot.

Grading and clearing on Parkland – The road widening eliminates a vegetated buffer that exists along the entire park frontage. During final design, MCDOT has agreed to the following to minimize impacts to parkland:

- a. Adjust grading to minimize LOD to the fullest extent possible
- b. Re-establish existing screening berms where possible
- c. Keep grading as natural as possible and provide slopes that will accommodate our use of platform mowers

Goshen Elm: The Goshen Elm is located within the public right of way of Goshen Road. M-NCPPC owns parkland adjacent to the elm which was purchased to protect the tree's roots. Parks staff currently maintains the tree at the ground level (the County performs pruning operations). M-NCPPC recommends that the project restrict grading and disturbance to the Goshen Road side of the existing asphalt swale, including the relocation of the existing fire hydrant.

### **Cabin Branch Stream Valley Park**

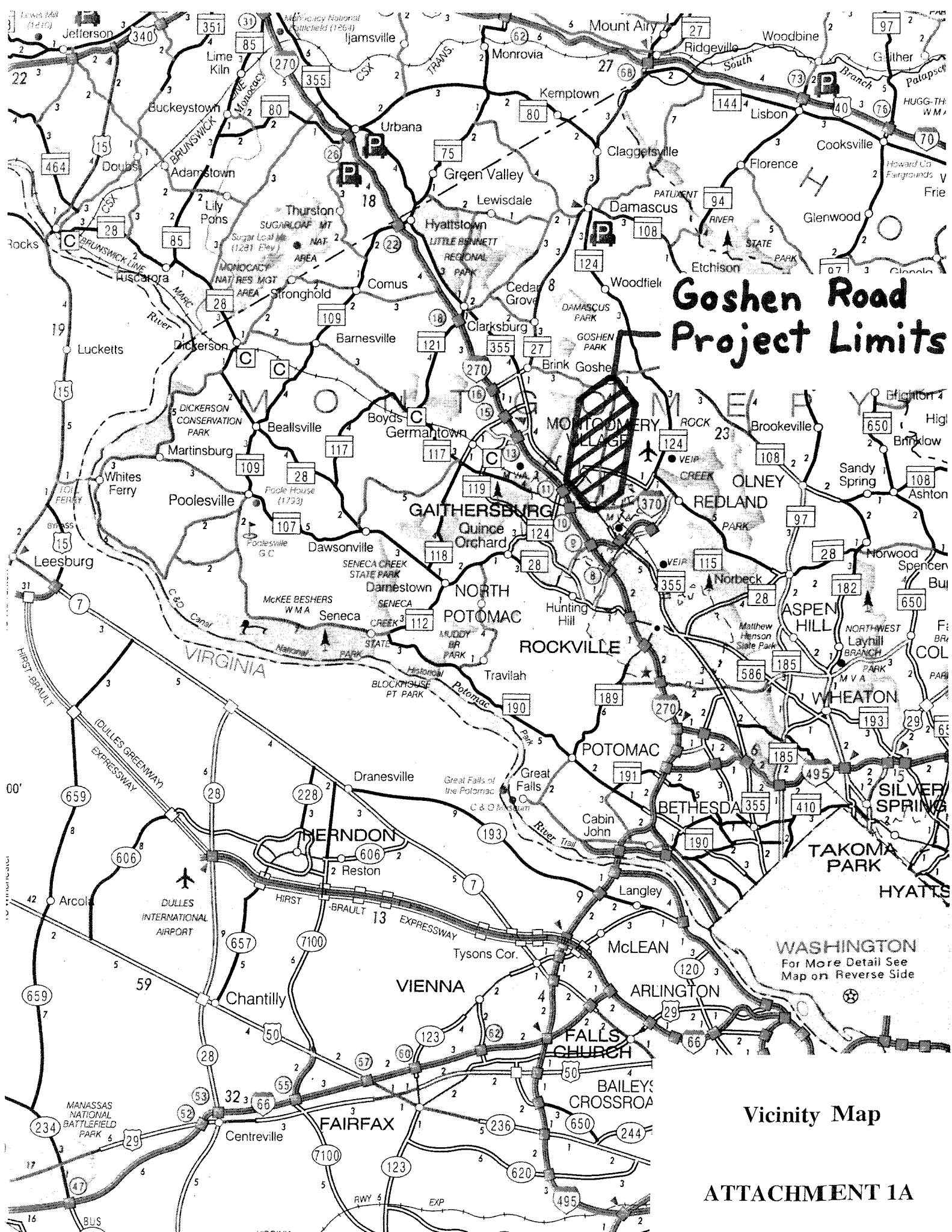
The project would require taking 0.01 acre of parkland in fee simple, 2.30 acres of parkland in permanent easement for stormwater management within the Cabin Branch Stream Valley Park, and 0.47 acre of grading easement. We have indicated to MCDOT that our policy is not to allow stormwater management (SWM) facilities in parkland except as a last resort.

This project proposes three stormwater management facilities on parkland due to the lack of available right of way, infeasibility of measures within the available ROW, and substantial increased cost for placing SWM in pipes underground. The site of the proposed SWM is currently a flat meadow, with only minimal trees of invasive species. The park manager mows these meadows once or twice a year. The proposed ponds are adjacent to the Cabin Branch stream, which MCDEP has recently studied and determined to be in relatively good condition and is not scheduled as a high priority for restoration. M-NCPPC Natural Resource Stewardship staff indicated that this would be a good location to create a forested stream buffer.

Staff supports the construction of the stormwater facilities, with the detailed conditions noted above as mitigation for the proposed impact to parkland. Work on parkland must meet Park's specifications and details. Final Design must be approved by Park's staff during park permit.

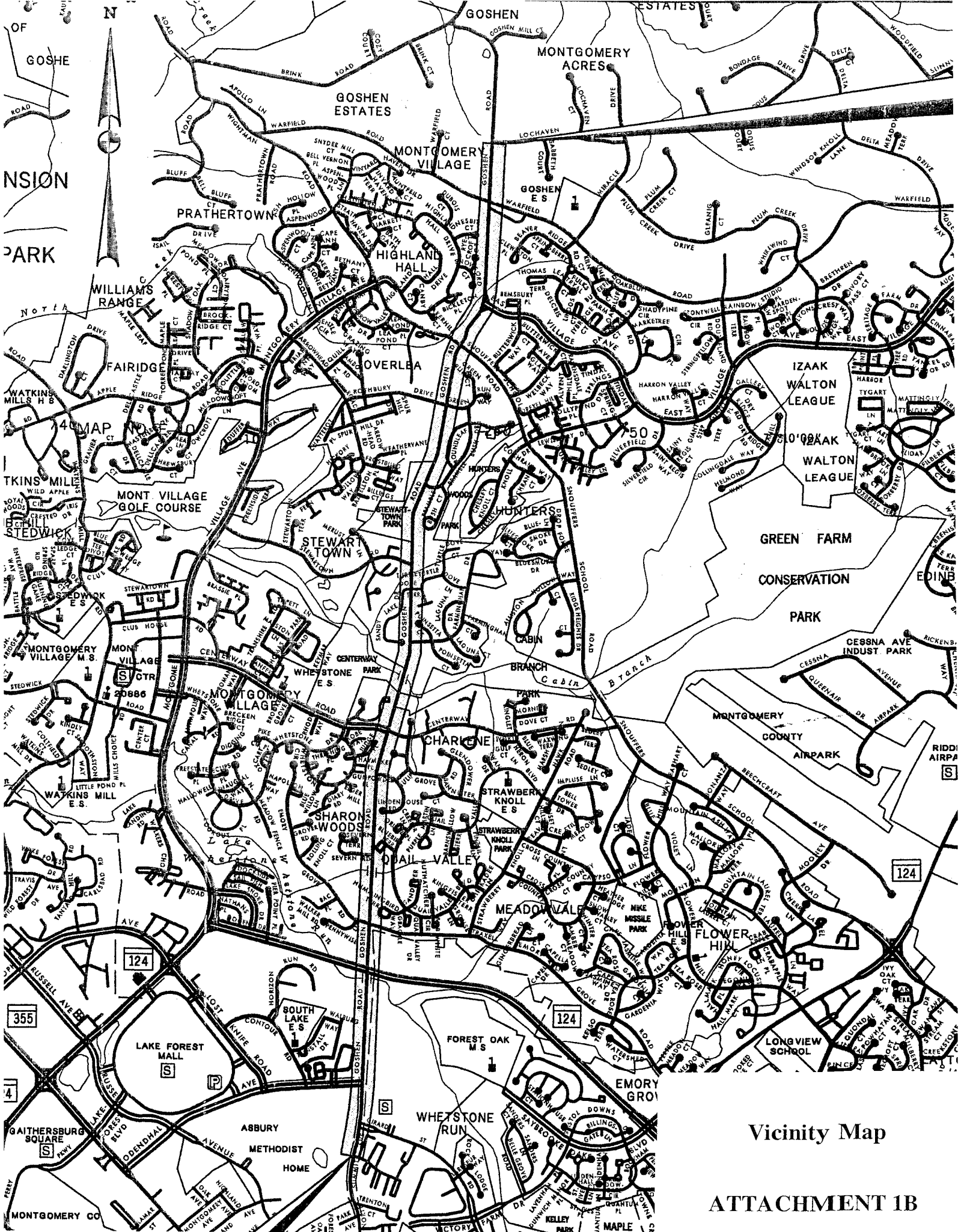
### **PUBLIC OUTREACH**

A public meeting for this project was held on September 8, 2009, attended by about 50-60 people. This was followed by MCDOT meetings with the Greater Goshen Civic Association and the Montgomery Village Foundation. Public meetings were also held during the Facility Planning Phase on April 29, 2003 and February 10, 2004.



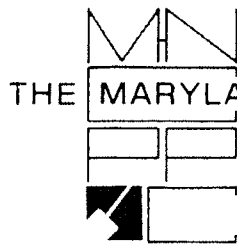
# Goshen Road Project Limits

Vicinity Map



Vicinity Map

ATTACHMENT 1B



THE MARYLAND-NATIONAL CAPITAL PARK AND PLANNING COMMISSION

8787 Georgia Avenue • Silver Spring, Maryland 20910-3760

(301) 495-4605

Montgomery County Planning Board  
Office of the Chairman

November 7, 2000

Albert J. Genetti, Jr., Director  
Department of Public Works and Transportation  
Executive Office Building, 10<sup>th</sup> Floor  
101 Monroe Street  
Rockville, MD 20850

RE: Goshen Road Bridge No. 061B over Cabin Branch  
CIP No. 509132  
Mandatory Referral No. 00813-DPW&T-1

Dear Mr. Genetti:

The Planning Board reviewed the Goshen Road Bridge Replacement project at our regularly on October 19, 2000. We made the following recommendations at that time:

1. Finalize a grade establishment plan for the ultimate configuration of Goshen Road before proceeding with the construction of this project.
2. The ultimate eastern roadway curb should be no closer to the Goshen Elm than the existing payment edge.
3. The design of this project should be revised as necessary to reflect the conditions of the final signed Memorandum of Understanding between DPWT and M-NCPPC.
4. Increase headroom for the shelf under the bridge to eight feet minimum (8.5 feet preferred), increase the width of the shelf on one side to twelve feet, if possible, and flatten the slopes to two percent. Consider using asphalt paving for the trail area under the bridge.
5. Offset the proposed sidewalk from the face of curb at the southeast corner of the Rock Elm Way intersection and eliminate the proposed sidewalk at the northeast corner. Provide a crosswalk on the south side of the Rock Elm Way intersection and construct a handicap ramp on the west side of Goshen Road.
6. Offset the proposed sidewalk at the northern end of the project as much as possible from the roadway.



7. Offset the proposed sidewalk from the face of curb at the Framingham Road intersection.
8. This project will require a Parks permit. All conditions of the permit must be adhered to.
9. Coordinate with our staff on an aesthetic treatment of the proposed concrete wingwalls which will complement the adjacent parkland.
10. The color of the proposed bridge railings should be National Park Service brown. The proposed guardrail should be weathering steel or National Park Service brown.
11. Provide landscape plans to M-NCPPC staff for comment. Landscape plans shall include the provision of street trees along the west side of Goshen Road.

In addition, we have enclosed a copy of the Forest Conservation Plan exemption for this project signed by our Environmental Planning Staff.

Thank you for your attention to this matter. If you have any questions concerning the above, please call Mr. Larry Cole of our staff at 301-495-4528.

Sincerely,



William H. Hussmann  
Chairman

LC:cmd

Attachments

MR Goshen Road Bridge follow-up ltr.doc



Montgomery County Planning Board  
Office of the Chairman

July 7, 2004

Mr. Michael C. Hoyt, Acting Director  
Montgomery County Department  
of Public Works and Transportation  
101 Monroe Street, Tenth Floor  
Rockville, Maryland 20850

RE: Goshen Road South Phase I Transportation Facility Planning Study  
Project Prospectus Recommendations

Dear <sup>Mike</sup> Mr. Hoyt:

The Planning Board reviewed the referenced project at its regularly scheduled meeting of July 1, 2004, and endorsed the project with the recommendations enumerated below.

Before proceeding with the particulars of the Goshen Road South project, I would like to alert you to the fact that our overall agreement on where this project should be headed is being hindered by the continuing lack of agreement between our agencies on where street trees should be planted in the public right-of-way. Your Department generally discourages street trees on arterials and major highways, except for locations behind the sidewalk, while the State Highway Administration is much more willing to place street trees between the curb and sidewalk and in the median. Our views on this issue are fairly close to the State's.

I believe that you were involved with the Intersection Workgroup, staffed by both agencies, that was working on this issue a few years ago, but that effort did not come to a resolution. A new DPWT policy was put forth on this topic earlier this year, but it had flaws that will take some discussion to work through, including possible Design Standard and Master Plan changes that would be necessary to implement the policy as presented. We request that you reconvene the workgroup to resolve this issue and that you invite staff from the Department of Environmental Protection to take part since that department is now in charge of the County's Street Tree Program.

1. The Goshen Road South Transportation Facility Planning Study should proceed to Phase II of the Facility Planning process as recommended in the May 2004 Draft Project Prospectus to develop a detailed design for Alternative 8, a four-lane divided roadway.
2. Per the Project Prospectus recommendation, the design for Goshen Road should be a four-lane facility, rather than the six-lane facility contemplated in the 1985 Gaithersburg Vicinity Master Plan. Recent long-range forecasting efforts have indicated that the six-lane facility will not be needed and this change will be reflected in the on-going Gaithersburg Vicinity Master Plan Update.

Mr. Michael C. Hoyt  
July 7, 2004  
Page 2

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3. DPWT should incorporate the new Goshen Road over Cabin Branch Bridge into the design for the new roadway, and include a minimized road section near the Goshen Elm (a 200+ year-old specimen tree), to avoid impacting its critical root zone. DPWT has previously committed to protection of this tree in the MOU for Protection of the Goshen Elm.
4. DPWT should incorporate street trees in the median while retaining the narrowed 16-foot to 18-foot median width in Alternative 8 to minimize community impacts. On all projects, DPWT should amend its policies and practices to facilitate planting street trees on the landscape panel between the curb and adjacent sidewalk/bikeway and on the median strip to allow for better shading of the roadway, reduced warming of stormwater, and increased pedestrian safety.
5. DPWT should pursue alternatives to avoid impacts to the Black and White Inn during Phase II regardless of the status of the property's historic designation. This property is under consideration for addition to the Locational Atlas and Index of Historic Sites as part of the Damascus/Goshen area amendment to the Master Plan for Historic Preservation. The amendment schedule will include a Planning Board public hearing during autumn 2004. Regardless of the site's historic status, the Planning Board finds it a cultural resource worthy of protection.
6. The Phase II Facility Planning study for the Goshen Road South should consider the following design details:
  - a. Investigate the presence of hydric soils at the stream crossing north of East Village Drive and include measures to reduce the incidence of flooding at that location
  - b. Exercise flexibility in final road alignment to save most of the specimen trees and minimize park and private property impacts
  - c. Minimize impacts to wetlands and waters of the US along the roadway alignment as much as possible by considering the use of reduced sections, grading adjustments, retaining walls, and culvert/bridge modifications
  - d. Incorporate noise impact mitigation for affected eligible under the County's Noise Abatement Policy into the Phase II facility design

Mr. Michael C. Hoyt  
July 7, 2004  
Page 3

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The Board thanks you and your staff for providing us this opportunity to comment on the Phase I study. We look forward to continuing to work with you during the next study phase.

Sincerely,



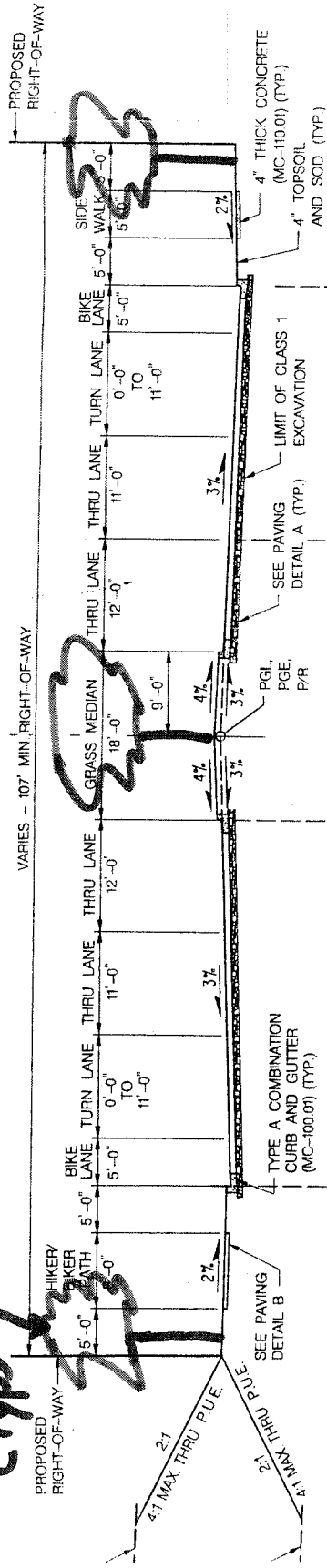
Derick P. Berlage  
Chairman

DPB:KHK:kcw  
Enclosure

ltr to Hoyt re Goshen Road South Phase I

# Typical Section

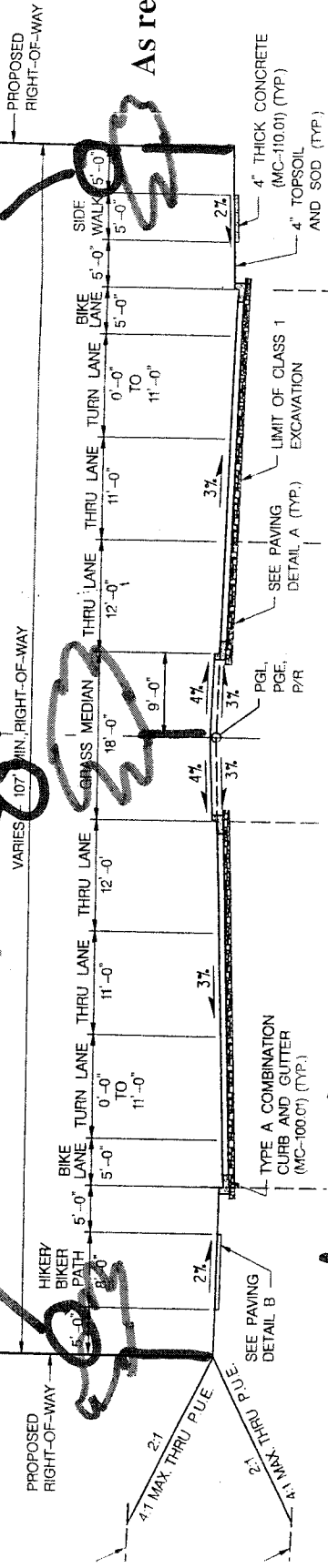
tree  
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As Submitted

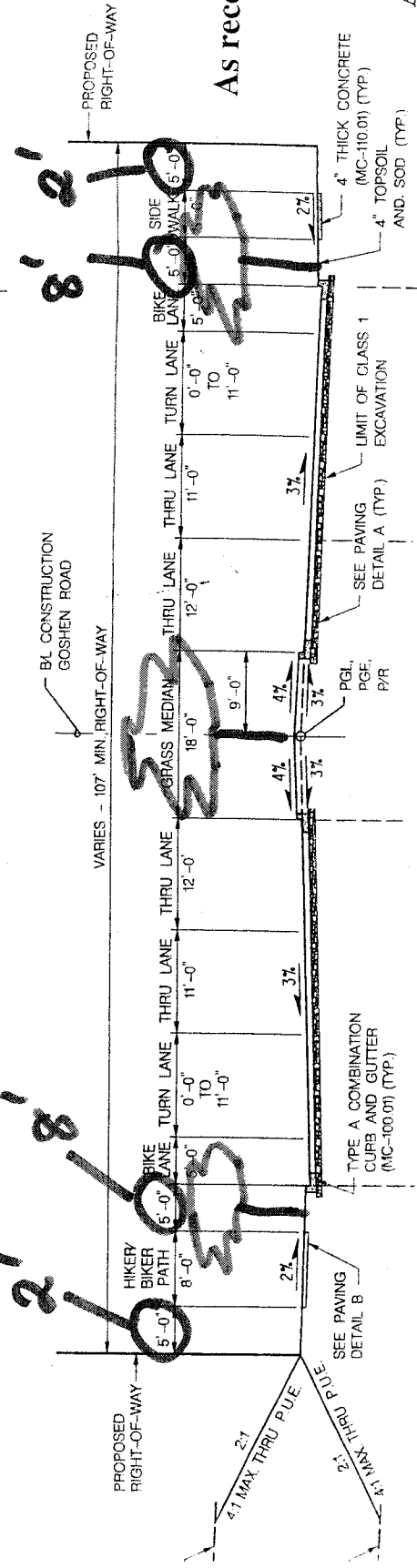
## GOSHEN ROAD NORMAL TYPICAL SECTION

103'



As revised by MCDOT

2' 8'



As recommended by Staff



DEPARTMENT OF PUBLIC WORKS  
AND TRANSPORTATION

Douglas M. Duncan  
County Executive

Michael C. Hoyt  
Acting Director

July 28, 2004

Mr. Derick P. Berlage, Chairman  
Maryland-National Capital Park and Planning Commission  
8787 Georgia Avenue  
Silver Spring, Maryland 20910

RECEIVED  
1235  
AUG 03 2004

Dear Mr. ~~Berlage~~ *Derick*

OFFICE OF THE CHAIRMAN  
THE MARYLAND NATIONAL CAPITAL  
PARK AND PLANNING COMMISSION

Thank you for your July 7, 2004, letter concerning the Planning Board's review of Goshen Road South Phase I Facility Planning. I will now respond to each of your comments.

1. Proceed to Phase II with Alternative 8. **Agree.**
2. Goshen Road should be a 4-lane facility. **Agree.**
3. Abide by the MOU for protection of the Goshen Elm. **Agree.**
4. Street trees in the clear zone. **Disagree.** This issue will be resolved during the re-writing of the Road Code, which is currently being performed by Glenn Orlin, Chief of staff for the County Council.
5. Pursue alternatives to avoid impacts to the Black and White Inn. **Agree.** This has already been incorporated into the prospectus.
6. A. - C. Minimize impacts to environment. **Agree.**  
D. Incorporate Noise Impact mitigation into Phase II. **Disagree.** The Noise Policy adopted by the County Council says that noise walls are not automatically incorporated into new projects. Each impacted area must compete under approved criteria with all other areas of the county. Phase II, will however, determine the exact extent of the impacts and suggest mitigation measures for the Council's consideration.

Thank you for your comments and suggestions concerning this project. We look forward to working with you on Phase II.

Sincerely,

*Mike*  
Michael C. Hoyt  
Acting Director

MCH:mwc



ATTACHMENT 5

Office of the Director

101 Monroe Street, 10th Floor • Rockville, Maryland 20850-2540 • 240/777-7170, FAX 240/777-7178  
Located one block west of the Rockville Metro Station