



MCPB
Item No.
Date:

C. Site Plan and Water Quality Plan No. 820110080 Winchester II

Staff Report Date 11-18-11

Mary Beth O'Quinn, Planner Coordinator, Marybeth.Oquinn@montgomeryplanning.org, 301.495.1322

Josh Penn, Senior Planner, Josh.Penn@montgomeryplanning.org, 301.495.4546

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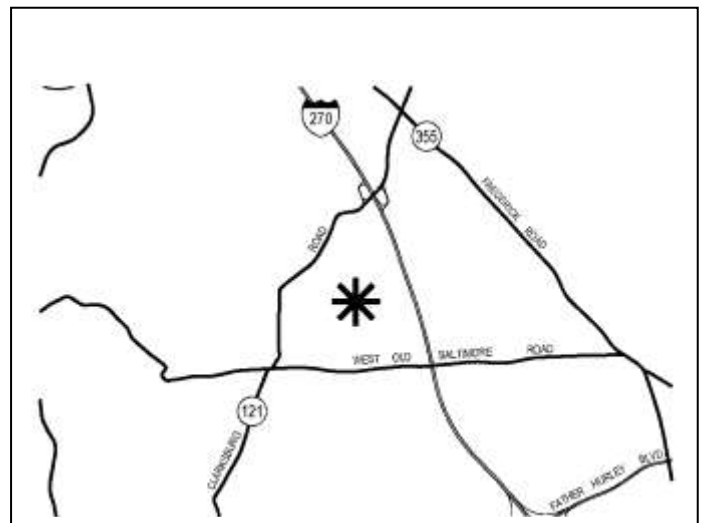
John Carter, Area 3 Chief, John.Carter@montgomeryplanning.org, 301.495.4575

Description

C. Site Plan No. 8-20110080: Winchester II of the Cabin Branch Neighborhood (includes a Water Quality Plan)

Adjacent to the intersection of Clarksburg Road (MD 121) and Broadway Avenue, west of I-270, 38.20 acres, MXPB and RMX-1/TDR Zones, Clarksburg Master Plan

Staff recommendation: *Approval with conditions*



Summary

Winchester Homes, Inc. has applied under Section 59-D-3 of the Zoning Ordinance, Site Plan Review, to construct 341 new homes, 232 dwelling units in the MXPB zone and 107 dwelling units in the RMX/TDR-1 zone. This site plan represents the first sequential, multi-phase residential development within the 535-acre tract that forms the heart of the Cabin Branch Neighborhood. The fully built plan, approved under the Development Plan G-806, allows 1,886 residential units, 2,420,000 square feet commercial space and 500 senior housing units.

The Winchester II Plan closely follows the Preliminary Plan and the Infrastructure Site Plan, encompasses Phases 1.1-1.3 of the Road Phasing Plan, (MD 121 improvements and two of the main north-south public streets internal to the development. The plan includes intra-block private streets, scaled residential blocks, an elementary school site and a local stream buffer park, in addition to a variety of attractive public open spaces distributed throughout the neighborhood enclaves.

RECOMMENDATION & CONDITIONS

Staff recommends approval of Site Plan and Water Quality Plan No. 820110080: Winchester II of the Cabin Branch Neighborhood.

Conformance with Previous Approvals

1. Development Plan Conformance
Comply with the binding elements, general notes, and the development program as shown on the Certified Land Use Plan of the Development Plan Amendment G-806, Exhibit 67(g), dated September 9, 2003; comply with the conditions and binding elements adopted in *Opinion 15-326*, Zoning Ordinance Amendment approved by the District Council, September 9, 2003. [Attachment
2. Preliminary Plan Conformance
Comply with the conditions of approval for Preliminary Plan Amendment 12003110B, *MCPB Resolution No. 08-117*, dated October 6, 2008, unless specifically modified or amended by the District Council *Ordinance 17-04*, Subdivision Regulation Amendment No. 11-01 (Validity Period) dated April 1, 2011, and/or District Council *Ordinance 16-35*, Subdivision Regulation Amendment No. 09-01 (Validity Period), dated April 1, 2009.
3. Infrastructure Site Plan
Comply with conditions of approval of the Infrastructure Site Plan, 820050150, as specified in the MCPB Resolution 07-131, dated July 19, 2007; comply with plan Amendment(s): 820050150A, per Resolution 08-68, dated June 9, 2008, and with Amendment 820050150B.
4. Cabin Branch Design Guidelines for Site Plan Review
 - a. Submit with building permit applications for each Development Program Phase an approval from the Town Architect stating that the proposed buildings, within that phase, are in conformance with the certified site plan and with design specifications of the Guidelines.
 - b. Submit to the Director of Planning (or designee) architectural plans Construction Documents from which Applicant will submit individual building permit applications for each Development Program Phase, for Design Guidelines conformance review. Submissions will be granted automatic approval after 10 days, unless Applicant is notified that the submission does not comply.

Density

5. Housing and Major Amenities
 - a. Provide within the 25.10 acres in the MXP Zone: 234 residential units, including 27 one-family detached units, and 207 one-family attached units of which 39 units are Moderately Priced Dwelling Units (MPDUs);
 - b. Provide within the 13.10 acres in the RMX/TDR-1 Zone, 107 residential units of which 80 are one-family detached units, and 27 are one-family attached units that include 2 MPDUs and 102 Transfer Development Rights (TDRs). All site development elements, details and specifications shown on the site and landscape plans stamped "Received" by the M-NCPPC on October 18, 2011 are required except as modified by the following conditions.

Transportation

6. Transportation and Transit Services

Comply with conditions detailed in the Transportation Planning memorandum, Preliminary Plan 12003110B, that remain in full force and effect as delineated in *MCPB Resolution No. 08-117*, dated October 6, 2008, and:

- a. Develop a Transit Services Plan showing location of bus stops, ride-share locations, and information kiosks; maintain a rideshare bulletin board on the Cabin Branch intranet website, accessible to all residents and employees at Cabin Branch, prior to the first occupancy permit. The website must be operational prior to issuance of the first building permit.
- b. Establish a carpool, vanpool, and bike matching program for all residents and employees at the Cabin Branch site by delineating locations and services as part of the Transit Services Plan, prior to 100th occupancy permit.
- c. Install permanent transit information kiosks and display areas in all office buildings and community centers with bus route (and the future Corridor Cities Transitway) schedules. The location and design of transit information stations must be coordinated with the Department of Transportation Commuter Services and installed prior to issuance of occupancy permits for each applicable building.
- d. Provide bike racks, located at the public green spaces, in accordance with the Development Program and as acceptable to MNCPPC Staff, timing to be determined at the time of Certified Site Plan. Bicycle parking must be provided at a bicycle-to-auto ratio of 1:20, using the number of on-street parking spaces.
- e. Provide a program for notifying residents regularly of the road construction schedule, access limitations, and arrangements to limit the impact of any access limitations to area roads, businesses, and residences.

7. Montgomery County Fire and Rescue Service

Comply with the conditions of approval per Fire Access and Parking Plan, approved September 9th, 2011.

Site Design

8. High Visibility Lots Treatment

Provide enhanced architectural treatments such as stone/brick walls, seat walls, architectural fences, and/or landscaping to be proposed by the Town Architect and subject to review and approval of MNCPPC Staff prior to Certified Site Plan:

- a. Block H, Lot 22.
- b. Block R, Lots 13, 14, 52, 53:
- c. Block R, Lots 23, 43.
- d. Block Q, Lots 4, 26, 27.
- e. Block S, Lots 1, 30, 31, 40
- f. Block T, Lot 25.
- h. Block V, Lot 9

Landscape and Lighting Design

9. Landscaping

- a. Provide street tree planting at a minimum 40 feet on center and a maximum spacing of 50 feet on center, except where physical site limitations do not allow for the spacing. Any deviation from standards must be approved by MCDOT.
- b. Provide for residential units, based on lot type and building type, an illustrative lot planting plan, showing size, number, species choices, and spacing of plants, subject to review and approval by staff prior to Certified Signature Set.

10. Lighting

- a. Deflectors must be installed on all fixtures causing potential glare or excess illumination, specifically on the perimeter fixtures abutting the adjacent residential properties.
- b. (Not Applicable to Win 2)
- c. Identify lighting proposed for the public right-of-way.

Recreation Facilities, Pedestrian and Bicycle Amenities

11. Comply with M-NCPPC Recreation Guidelines for all applicable proposed recreational facilities and demonstrate conformance for each facility with respect to size, grading, setbacks, location and targeted age group, as follows:

- a. Provide the following facilities for Cabin Branch Winchester II: one Tot Lot, 5 Sitting/Picnic Areas, one Open Play Area I, four Open Play Area II, Bike System, Pedestrian System, Nature Trails, and Natural Areas.
- b. Provide an 8-foot wide path (asphalt) off-street, within the public right-of-way as shown on the Winchester I and II Green space/Overall Plan Exhibit, in the following locations:
 - i. West side of Fulmer Avenue;
 - ii. Newcut Road;
 - iii. East side of Cabin Branch Avenue;
 - iv. Provide a Public Improvements Easement (PIE) for all paths not located with the public right-of-way, subject to DOT review & approval

Public Spaces and Amenities

12. Design

Provide the following information regarding the public space amenities: furnishings and fixtures, materials and design, grading, landscaping and planting for review and approval by MNCPPC Staff and to be finalized with Certified Site Plan. The drawings, at 1"=30' scale, must include the nearest public street(s) and housing footprints.

a. **North and South Lawns**

- i. Provide two ornamental light fixtures near the benches for each Open Play Area, compatible with Design Guidelines recommendations for the Lawn Play Areas (Block S, Block R);
- ii. Provide special paving for the drive aisles and parallel parking to integrate the functional and aesthetic aspects of the spaces.
- iii. Show street lighting on the landscape plans.
- iv. Provide additional seating near the street edge portion of each park; provide a raised lip, or architectural detail for the street edge to serve as a ball stop.

b. Shaw Park

- i. Provide details for the cemetery fence and gate, and tree protection as recommended by MNCPPC Historic Preservation.
- ii. Retain stones and mature trees in place on site, remove underbrush and clean up cemetery site.
- iii. Install a small sign to commemorate – and perhaps interpret – the cemetery site.
- iv. Install simple (or restore any extant) fencing, with operable gate to allow access for maintenance and visitation of the cemetery site.
- v. Reset stones, consistent with appropriate guiding principles for such an undertaking, such as the Maryland Historical Trust Historic Cemetery Preservation.
- vi. Provide a bike rack in the park.

c. The Commons

- i. Provide an additional seating area for each play area at the Godwit Street frontage;
- ii. Provide a bike rack.

13. Maintenance

The Applicant, its successors/assignees, and/or the successor HOA shall be responsible for maintaining community-wide amenity features such as HOA playgrounds, HOA open spaces, retaining walls, parking facilities, streetscape elements and natural features.

Sustainable Design

14. Energy Efficiency

- a. Demonstrate, for one-family detached residential units, means by which to achieve Energy Star rating under the national energy performance rating system [or the prevailing energy code in enforcement in the county at time of permitting](#)
- b. Provide a power feed for one electric car-charging station located at either the Local Park surface parking lot prior to Parks Department occupancy or near a major public green space.
- c. Provide, on HOA or Park site accessible to residents community garden space suitable for cultivation of edible food products, local CSA program information.

Transferable Development Rights (TDRs)

15. Acquisition and Recordation

- a. Provide verification, prior to recording of each residential plat for the Winchester II site plan, that the numbers of TDRs relevant to that plat have been acquired.
- b. Acquire 102 TDRs under the Cabin Branch Winchester II site plan.

Moderately Priced Dwelling Units (MPDUs)

16. Location and Phasing

- a. Provide 41 MPDUs as part of Cabin Branch Winchester II site plan, (17 % of total number of units), consistent with Chapter 25A.
- b. Execute the MPDU Agreement-To-Build prior to issuance of the first building permit.

17. Common Open Space Covenant

- a. Reference Common Open Space Covenant recorded at Liber 28045 Folio 578 (“Covenant”) on the record plat of subdivision.
- b. Provide verification to M-NCPPC staff prior to issuance of the 301st building permit that the recorded HOA Documents incorporate by reference the subject Covenant.

Development Program

18. Development Program Requirements

The Applicant must construct the proposed development in accordance with a development program that to be reviewed by M-NCPPC staff prior to the approval of the Certified Site Plan. The Winchester II Site Plan will be developed in two phases: Phase I proposes 224 residential units (117 attached townhouse units, 107 single family detached units), Phase2 proposes 117 residential units (117 attached townhouse units); The Development Program must include a phasing schedule as follows:

a. Streetscape

- i. Street lamps and sidewalks for public streets, private streets and alleys must be installed within six months after street construction is completed.
- ii. Street tree planting must be completed no later than six months after completion of the residential units or park and school facilities adjacent or adjoining the subject streets.

b. Local Recreational Facilities

- i. All Phase I Local Recreational Facilities shall be completed prior to the issuance of the 70% building permit for the units in Phase I. The recreation facilities in Phase I include integral sitting and picnic areas, bicycle racks, and the bicycle system paths within this phases. Landscaping and lighting associated with the Recreational Facilities must be installed no later than six months after the completion of those facilities
- ii. All Phase II Local Recreational Facilities must be completed prior to the issuance of the 70% building permit for the units in Phase II. The facilities in Phase II include the HOA amenity, North and South Lawns, Shaw Park, The Commons, integral sitting and picnic areas and the bicycle paths within this phase. Landscaping and lighting associated with the Recreational Facilities must be installed no later than six months after completion of those facilities.

c. Community-Wide Recreational Facilities

- i. The school site must be rough graded in accordance with the requirements of MCPS, and subject to MCPS approval; the Local Park must be rough graded prior to the issuance of the 440th building permit for the entire Cabin Branch development consistent with the Infrastructure Site Plan
- ii. In accordance with the Infrastructure Site Plan approval, a building permit must be obtained for the community center and pool prior to issuance of the 650th residential building permit (regardless of applicant or phase).
- iii. The bicycle and pedestrian paths must be complete prior to the issuance of occupancy permits for the adjoining buildings.

d. Phasing

- i. The Development Program must provide phasing of dedications, stormwater management, sediment and erosion control, afforestation, trip mitigation, and other features.
- ii. On-site amenities associated with each block, including but not limited to, sidewalks, pathways, furnishings such as benches, trash receptacles, and bicycle facilities must be installed prior to release of any building occupancy permit.
- iii. On-site landscaping and lighting associated with the construction of private roads and common areas shall be installed no later than six months after the completion of those roads and common areas.
- iv. Final design, construction phasing and building permit applications for the lots adjoining and near to the Toll Brothers property must be coordinated with the design, construction and permitting of the public streets serving those lots, subject to MNCPPC staff review and approval; The Commons, Stilt Street, Godwit Street, and Stent Street and their intersections must be substantially (70%) complete prior to the release of buildings permits for the affected lots; if construction of The Commons and the three public streets and their intersections has not commenced within 48 months from the date of Certified Site Plan approval, applicant shall file a Site Plan Amendment to propose alternative designs for The Commons and the subject road connections and termini. The lots listed below are subject to this condition:
 - Block W: Lots 1-5, 15-22 and The Commons;;
 - Block X: Lots 15, 16, 17, 18, 43, 44, 45, 46, 47;
 - Block S: Lots 7-13, 14-17;
 - Block T: Lots 26-28;
 - Block I: Lots 16-20.

e. Clearing and Grading

- i. Clearing and grading must correspond to the construction phasing to minimize soil erosion and must not occur prior to approval of the Final Forest Conservation Plan and the Certified Site Plan, except as specified in the conditions of approval for the Infrastructure Site Plan.

Landscape Bonds

19. Requirements

The Applicant must provide a surety (letter of credit, performance bond) in accordance with Section 59-D-3.5(d) of the Montgomery County Zoning Ordinance with the following provisions:

- a. Include in the surety amount, plant materials, on-site lighting, recreational facilities and site furnishing and fixtures with the relevant block of development. Surety must be posted prior to issuance of the first building permit within each relevant block of development and tied to the Development Program. The surety should include the North and South lawns, Shaw Park, and the Commons areas, but should not include areas within public ROW or SWM pond easements, or improvements on residential lots.
- b. Provide a cost estimate of all materials, treatments, and installation and construction labor, which will establish the initial bond amount.
- c. Completion of plantings by block, followed by inspection will allow for a bond reduction. Inspection approval starts the 1-year maintenance period; bond release occurs at the expiration of the one-year maintenance period.

- d. Provide a Landscape, Furnishings, Fixtures, and Landscape Structures Agreement that outlines the responsibilities of the respective parties and incorporates the cost estimate, prior to issuance of the first building permit.

Environmental

20. Water Quality and Forestation Plan

- a. The Applicant is bound by all conditions of the approved Infrastructure Site Plan No. 820050150 dated June 14, 2007, except as modified herein.
- b. The Applicant must comply with the conditions as stated in the Montgomery County Department of Permitting Services (MCDPS) letter dated September 15, 2011. The conditions may be amended by MCDPS, provided the amendments do not conflict with other conditions of the Site Plan or Final Forest Conservation Plan approval.
- c. All off-site reforestation must occur within the Clarksburg Special Protection Area unless specifically approved by the Planning Board.
- d. The record plat must show all areas of stream buffers within a Category I conservation easement unless the area is to be dedicated to the Montgomery County Department of Parks or is otherwise within a road right-of-way, utility easement or stormwater outfall.
- e. No retaining walls are permitted within any environmental buffer unless approved by the Planning Board.

21. Noise

Limit the acceptable noise levels to a maximum 65 LdN for residential rear yards.

Certified Site Plan

22. Requirements

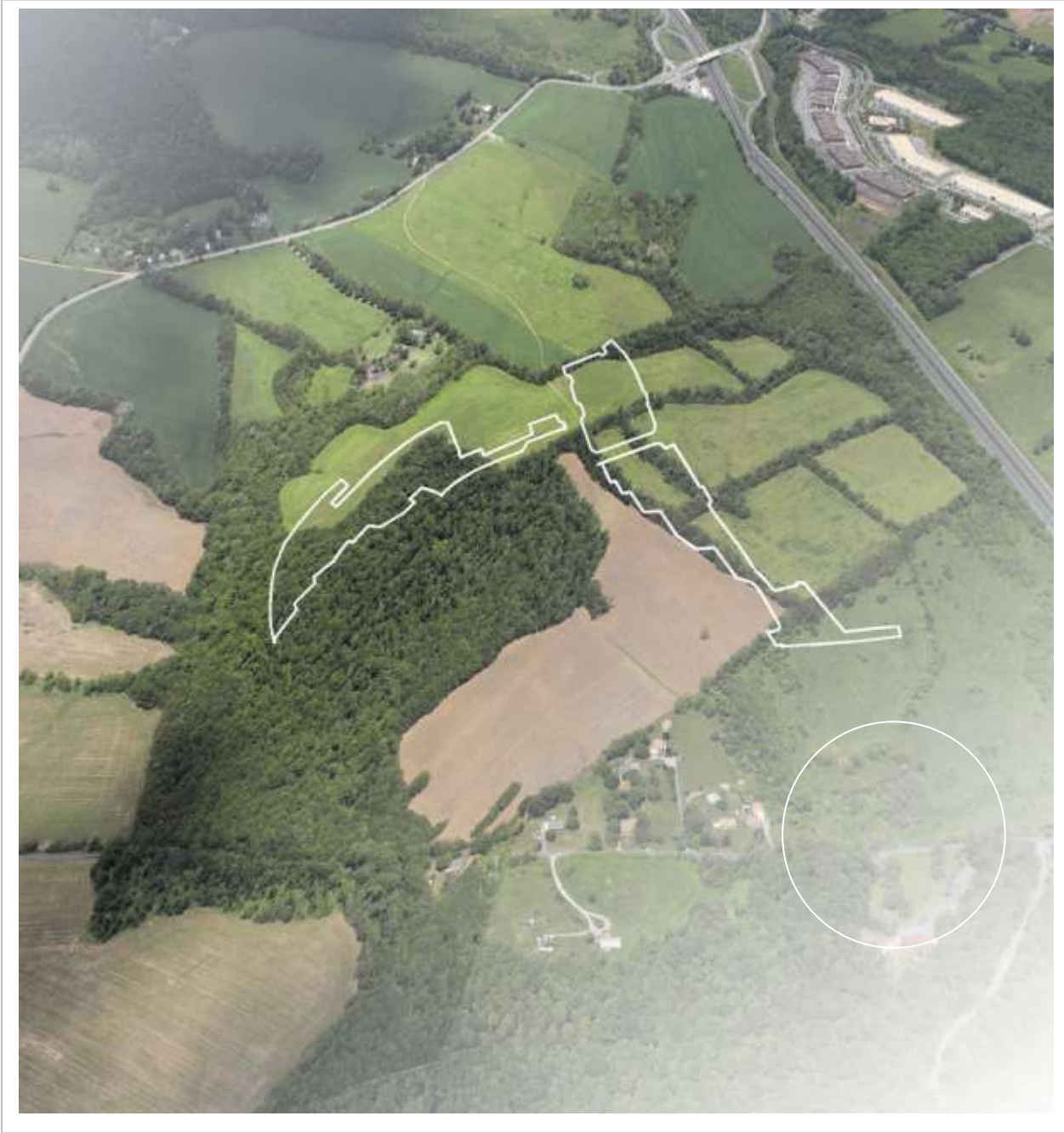
- a. Phasing
 - i. Site Plan Resolution
 - ii. Development Program, inspection Schedule
 - ii. Detailed Phasing Plan showing phasing lines consistent with the Development Program;
- b. Environmental Elements
 - i. Limits of Disturbance
 - ii. Undisturbed stream buffers, except where approved outfalls are located
 - iii. Methods and locations of tree protections
 - iv. Forest Conservation easement areas;
 - v. Note stating that M-NCPPC staff must inspect tree-save areas and protection devices prior to clearing and grading;
 - vi. Environmental setting protection for the historic resource or site.
- c. Site Design
 - i. Definitive lot sizes, with building envelope setbacks labeled;
 - ii. Details for all site furnishings and fixtures: mailbox cluster, signage, seating, trash receptacles, bike racks;
 - iii. Wayfinding and signage
 - iv. Details for all public spaces, including grading, furnishings and fixtures, lead walks.
- d. Landscape and Lighting
 - i. Details for all public spaces, including grading, furnishings and fixtures, lighting lead walks, retaining walls showing top and bottom elevations, and wall thickness, materials, planting (size, spacing, proposed species and alternate species).

- ii. Provide a Cabin Branch Street Tree Plan, showing the proposed tree locations and species for each public street; provide a supplementary plan showing proposed street tree planting for all private streets and/or alleys.
- iii. Provide street trees at minimum caliper of 3 inches at the time of planting; shade trees on HOA property or lots to be 2.5-3.0 inches at the time of planting; coniferous trees to be 6 feet in height at the time of planting.
- iv. Ensure consistency of all details and layout between site plan and landscape plan.
- e. Building Design
 - i. Provide drawings with details and dimensions for all site amenities, including kiosks, walls, monument signage, pergolas, entry features, sculptures and their bases, etc.
 - ii. Provide hard line drawings of each unit type, showing materials, entrances, parking, unit height from finished floor level.
- f. Development Data

Verify and/or update all development data including TDRs, MPDUs, impervious area, public use space, HOA space, public dedication land, density calculations, and development tracking with respect to the Development Plan and Preliminary Plan.

Project Summary

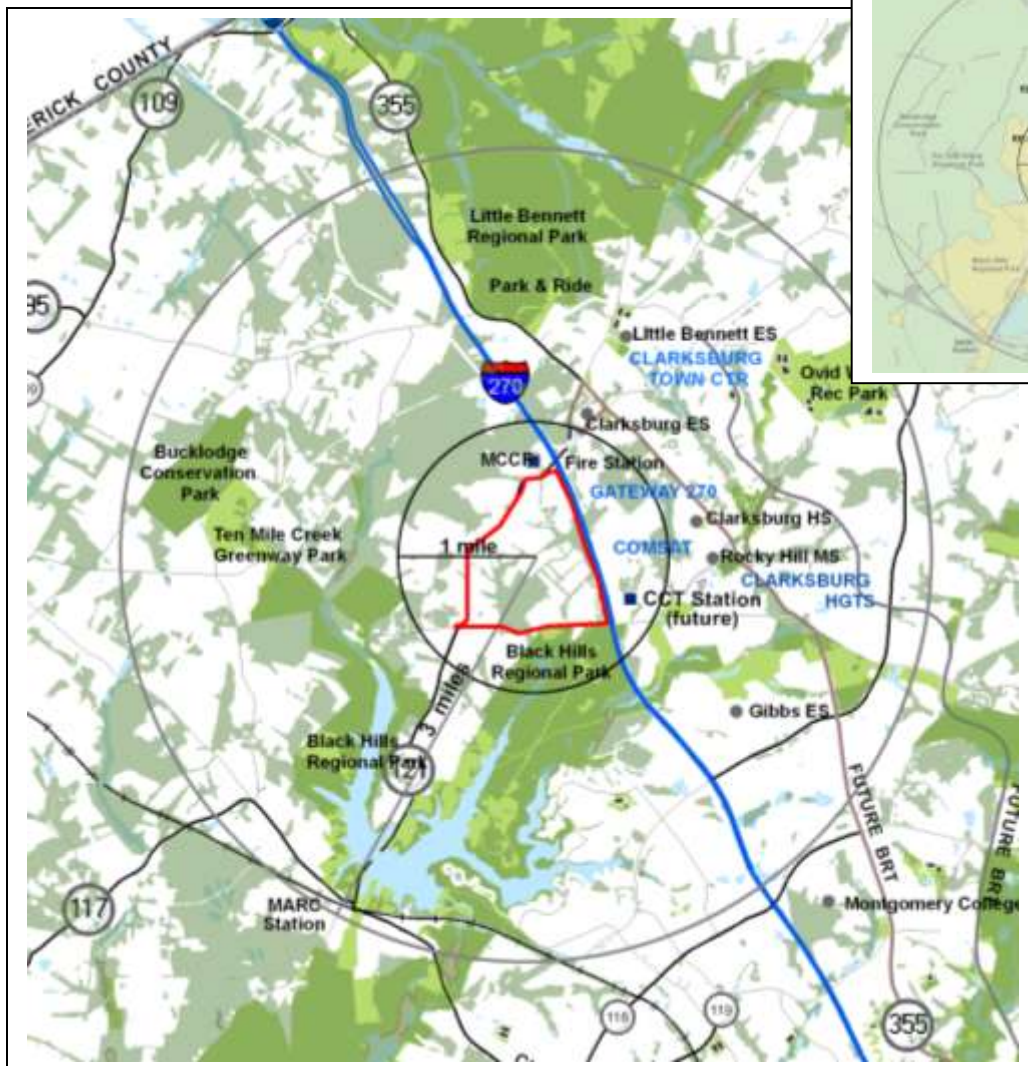
The Applicant, Winchester Homes, Inc. has applied under Site Plan Review to construct 341 new residences on a portion of its property, under the Cabin Branch Winchester Phase One Site Plan. The subject property comprises 38.20 acres, sited within the larger Cabin Branch tract of 535.04 acres under multiple owners. The larger plan proposes a highly articulated neighborhood form, characterized by a dense mix of housing types that, in its striking natural setting, preserves two stream valleys, and provides a school site adjoining a new local park to achieve a visionary figure ground pattern. This site plan augments the neighborhood housing mix with additional detached homes on the west side and continues the block pattern on the east side as shaped by Winchester I plan. Note Black Park entrance at bottom right.



PROJECT DESCRIPTION

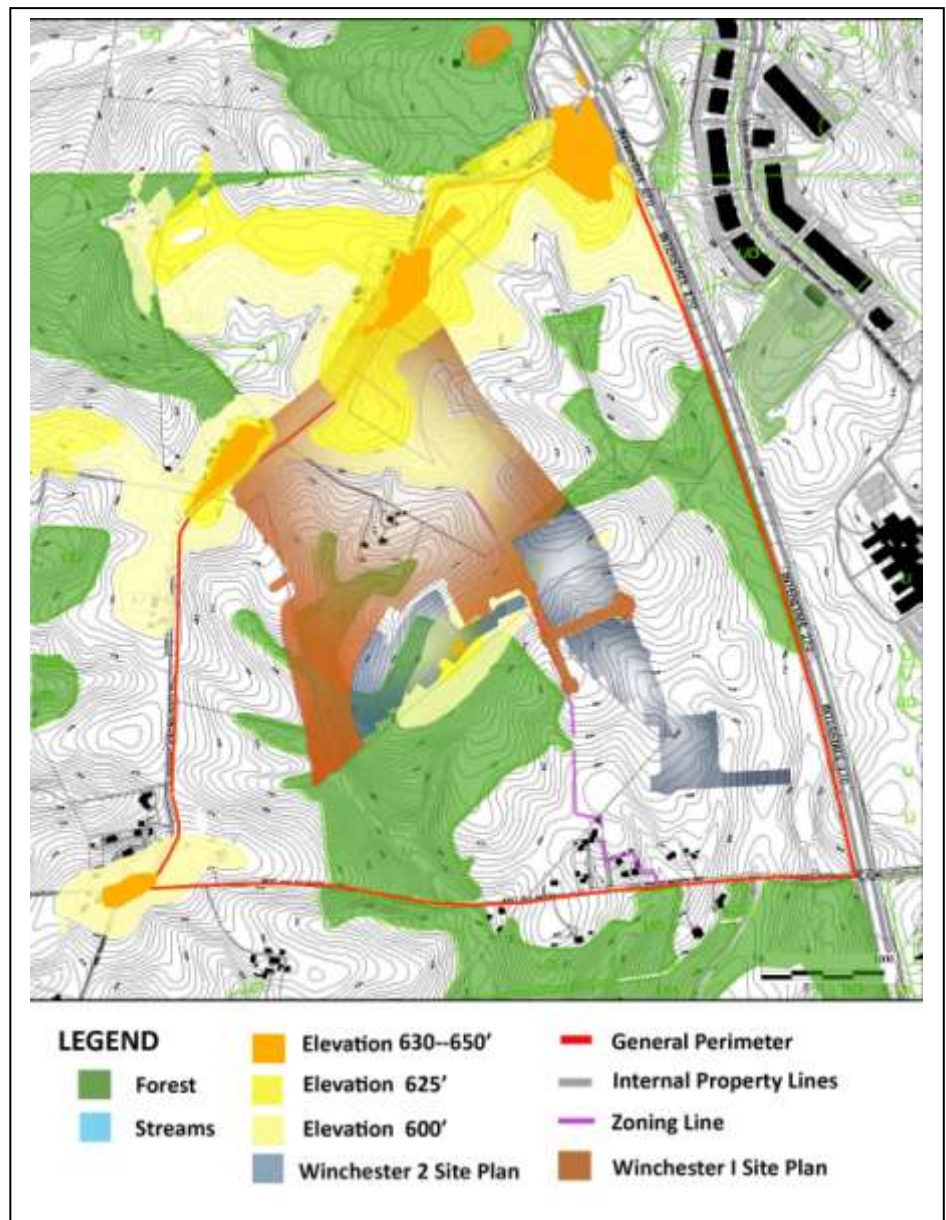
Vicinity and Surrounding Neighborhood

The Cabin Branch Neighborhood rests at the confluence of richly contrasting elements that form the ecology and economy of Montgomery County: the I-270 Technology Corridor, the Agricultural Reserve, Black Hills and Little Bennet Regional Parks and existing and proposed transitways. To the south lies large-lot suburban residential development (RE-1/TDR, R-200/TDR), and further south is Gaithersburg and the Shady Grove Life Sciences Center (LSC). The Clarksburg Town Center (RMX-2) lies to the northwest, surrounded by residential zoning. The subject neighborhood lies almost equidistant from the two regional parks. The natural assets of the general area, as well as the Cabin Branch site itself, fit snugly between farmland (RDT) to the west and the highway to the east, featuring steep slopes that are connected hydraulically to the Waters of the United States, the headwaters. The area is designated a Special Protection Area because of its identification as a sole source aquifer. The central position of the Cabin Branch tract, posed between high-volume interstate traffic with employment intensity (I-3) and the bucolic, heavily forested parks, flanked by rolling, working farmland highlights the unique opportunity to mediate within the Cabin Branch neighborhood itself, these diverse elements: the built and unbuilt, science and nature, and local and regional influences.



The natural elements inherent to the Cabin Branch tract give pause to considerations of development, and further, to the remarkable vision of the Master Plan in identifying the unique future role of the site for employment, housing, and the preservation of the local aquifer that supports extensive forestation. The striking organic form of this land demands a thorough understanding of the topography and hydrology that, in turn, create challenge and opportunity both, in the application of a built environment. This triangular canvas, is ultimately rendered as a three-dimensional sculpture shaped reflexively by the two stream valleys that reach deeply into the site from the Black Hills, almost touching, yet restrained by an emergent linear topography that rises between them. This spine (600' elevation), carries northward at a nearly consistent elevation, forming, not coincidentally, the foundation for the main roadway before rising, tying into the ridge that forms the highest site elevations (625-650').

This prevailing ridge marks the north and western boundaries of the site, and Clarksburg Road. So, it is the ridge, in inverse relation to the stream valleys that provides the stable soils suitable to road construction. Not coincidentally, this central ridge provides the foundation for Broadway as well as marking the zoning line between MXP zone to the east and the RMX-TDR1 zone to the west. Each side of the tract features a stream valley that hugs the central ridge, and frames the pattern of solid and void that becomes a signature element of this plan. Note the relationship between Winchester 1 Site Plan boundary (brown) and the Winchester 2 Site Plan boundary that encompasses the high point of the site's center.



The Proposed Plan

The planning and design for a project on the scale of Cabin Branch has proven to be an extensive multidisciplinary effort. Precision engineering, civil, geo-technical, electrical, mechanical, transportation and environmental, with architecture, urban design, and landscape architecture are essential to the development of a site so challenging in terms of program, existing conditions, environmental constraints, and potential market forces. The infrastructure necessary to “insert” an urban-style community into such a protected setting forces design that exploits every inch of space, negotiating every turn at tight tolerances to achieve workable, functioning utilities. Contrasted with the huge, largely unseen (underground) infrastructure are the aesthetic surroundings that create the humane environment envisioned by the Clarksburg Master Plan.

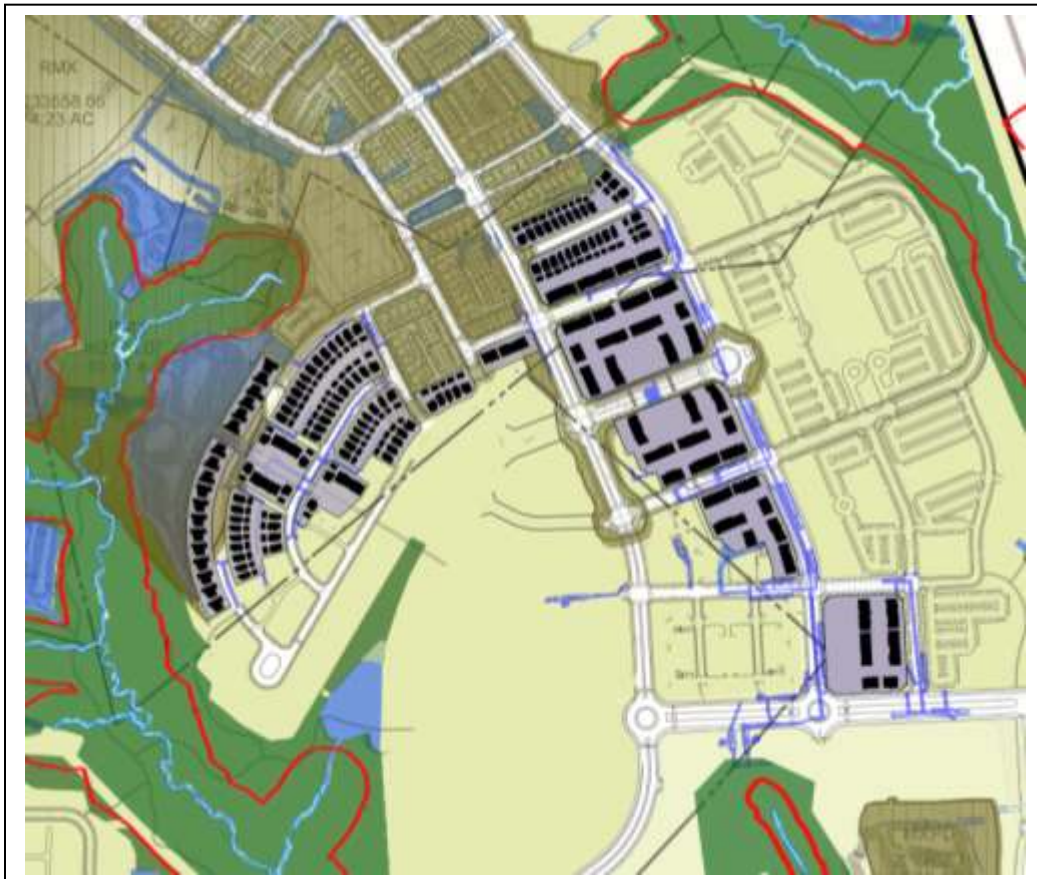


Description of the Project

The applicant, Winchester homes, proposes to build the second phase of the Cabin Branch Neighborhood that extends, in fragmented form, the housing blocks programmed for Phase I. (Phase 2 shown in blue shade, Phase 1 in brown, below.) This phase, if only in area, is based largely on residual land segmented by the original property lines in still in effect. The resulting “site,” is splayed across the tract’s two zones, forming non-relational clusters of three residential unit types. The fulfillment of a rationalist progression, an ideal of neo-traditional planning, rests with future “infill” development supplied by the adjoining property owner, Toll Brothers.

The most easily-read form in this phase are the dense blocks of row houses that march down Broadway’s slope, terraced in rhythm which lengthens, block by block, through massing and height, until culminating in the broad gesture of the North and South Lawns framed by tight walls of housing. Here, the continuous slope becomes, surprisingly, the visual and physical bounty in itself—a simple, controlled grade, expansive across the boulevard, shaped by landscaped horseshoe drives, spatially arrested by the symmetric façade of housing that firmly anchors the space.

The string of blocks, fragmented portions, continue down the slope, disconnected except for the street, until reaching Shaw Park, which contains the remnants of the Shaw Family Cemetery. This block’s modest, attached housing units form the backdrop to a sizable “front yard,” that is, the cemetery that rests in the center and the two large spaces that surround it. Little Seneca Parkway lines the southern boundary of the block, and forms the finished edge of the carpet of housing that drapes the slope falling from Clarksburg Road to Black Hills Regional Park.

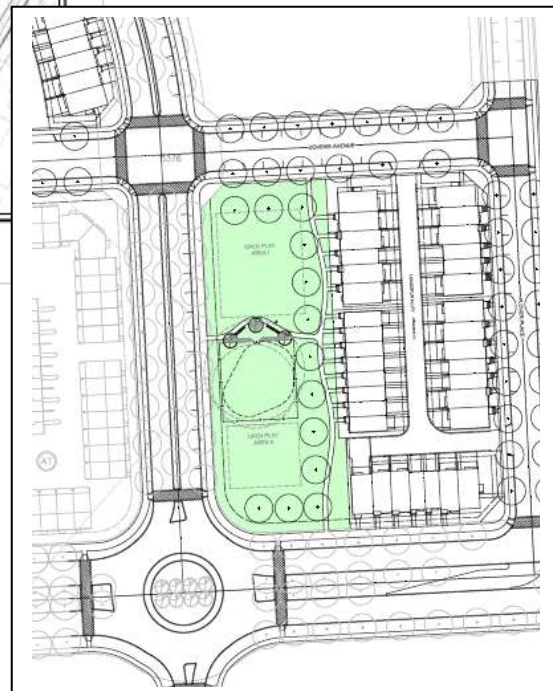


The “west side,” in its tenuous connection to the east side of row house blocks, ties into curved street forms that feature the most traditionally suburban models in Cabin Branch. It is here that the natural assets of the site, the sensitive treatment of urban form, and the desire for sustainable density meet the the challenge of market driven housing production. Although the units are tightly spaced, far more so than typically seen, it is the unit type itself that sets a character that is at odds with the patterns established “up the hill.” The thoughtful design embodied in the “urban” blocks begs for extension here—for an innovative unit type, detached or semi-detached that uses the grade for environmental advantage, in structure and energy, that at the same time fulfills the Master Plan’s emphasis on detached one-family homes.



**Shaw Park
Winchester Phase 2**

**The Commons,
Winchester Phase 2**



Architectural Design

The handling of texture, scale and proportions are critical to the reading of housing in an urban setting. Variation in materials, façade relief that interprets scale, and proportions that relate, but not copy, housing form set the lot-to-lot, block-to-block, and section-to-section relationships. In this way, an urban fabric evolves, where the incremental is appreciated as is the large sweep, the parts make the whole, and the whole is the sum of the parts. It is this holistic realization that is essential in the organic development of a vibrant, engaged neighborhood. It is even more difficult to “design” such an inherent process, jump-starting a process that typically matures over decades or even centuries. Nevertheless, there is much to gather from the study of historically interesting neighborhoods that support large quantities of housing.

The requirement, through previous Site Plan approvals, for the Applicant’s development of Design Guidelines under the auspices of a Town Architect promises to guide this process as streets are detailed and constructed. The Architect will review individual units, and their placement within each block, and make recommendations regarding the effective street wall with respect to façade materials, porches and windows, building height, finished floor levels, and exterior architectural details. The applicant’s decision to provide an ample number of unit types, with numerous detail selections, offers the opportunity to achieve variety and similarity, with consistency and surprise, all elements to be realized in the in context of a city residential street. The treatment of the side yards will be critical with respect to privacy and noise attenuation on the narrowest lots, requiring the Town Architect’s review of side walls, construction materials, and exterior spaces.

Manor Town Homes proposed as one-family detached units, include a 20-foot wide unit and a 22-foot wide, in addition to the 16-foot wide unit offered as Moderately Priced Dwelling Units. Note the side-by-side MPDUs, (above center) that offer that offer the appearance of larger unit. The selections offered as rear-loaded one-family detached homes vary considerably, with 24-foot, 30-foot, and 36-foot units. Exterior detailing, such as porches and dormer windows, shutters, and gables are



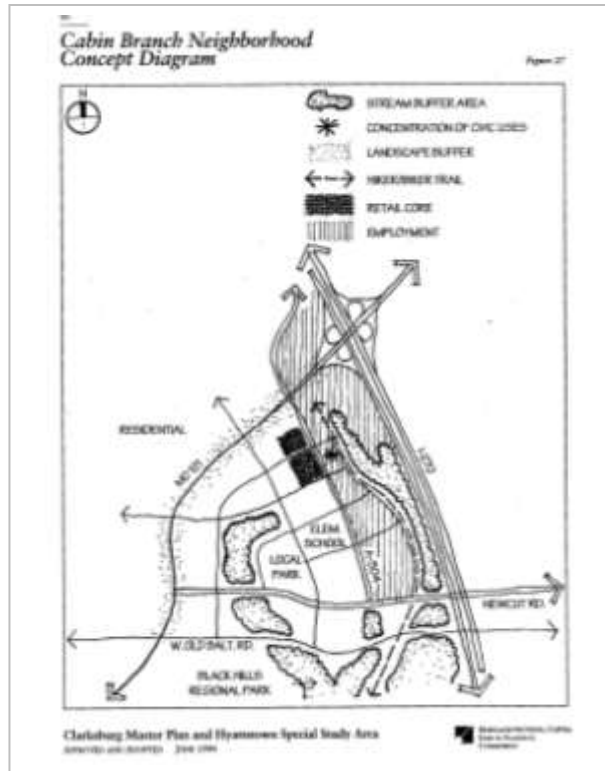
Schematic Site Plan and Infrastructure



The extents of the infrastructure foundation may be seen in the above illustration. Note the careful underlay of the stormwater management facilities, block-to-block and street by street. The tract features numerous, stormwater ponds distributed throughout the site, that surround the edges of the stream buffers. The inter-laced elements cross property parcels and development phases.

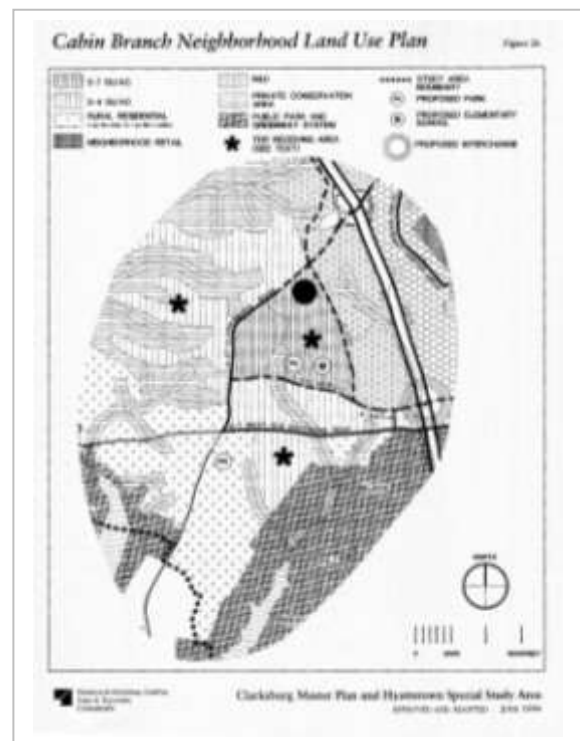
REGULATORY HISTORY

The Master Plan and the Development Plan



The Cabin Branch community is a 540-acre triangular tract of land, located on the west side of I-270, bordered by Maryland Route 121, I-270 and West Old Baltimore Road.

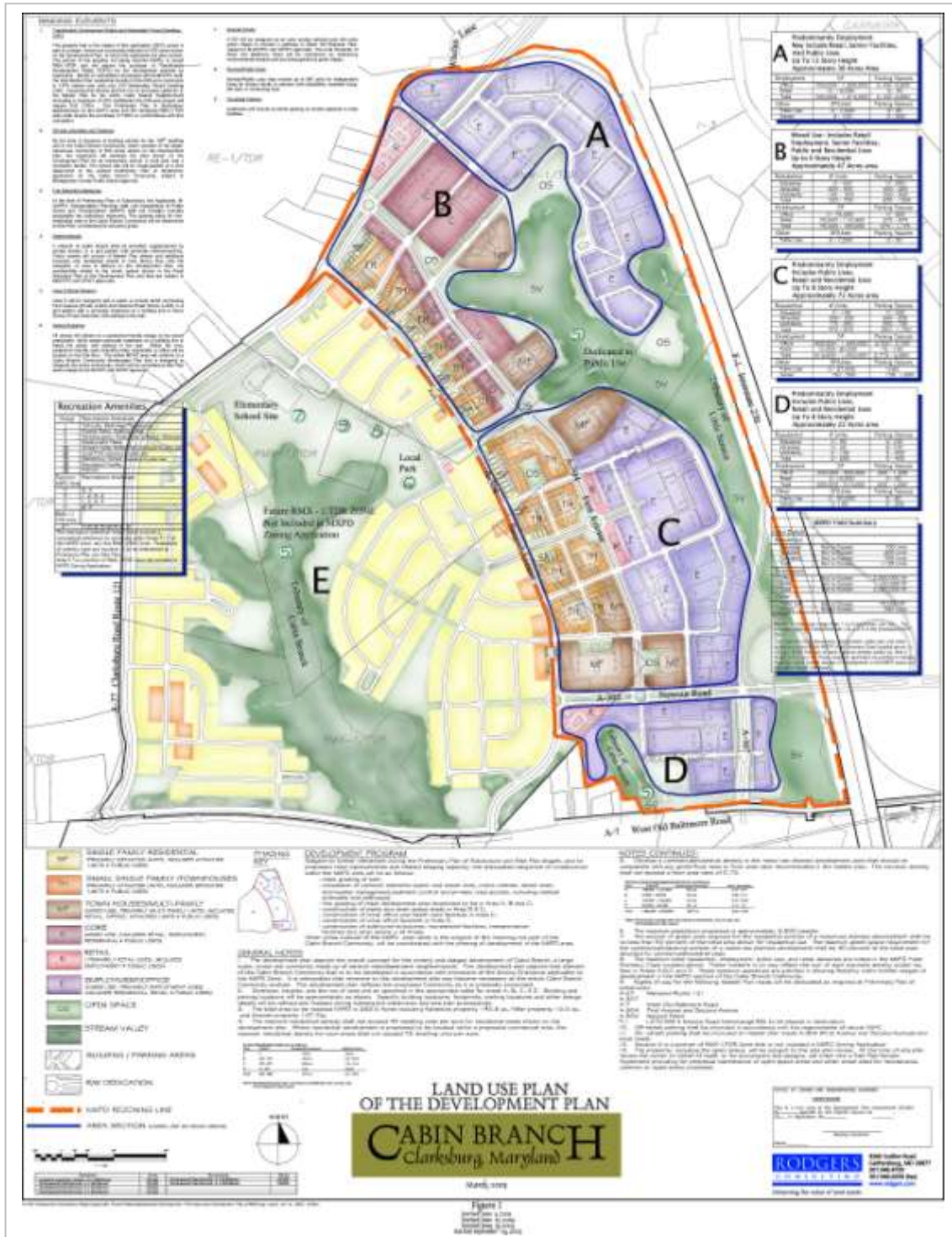
It is part of the larger Cabin Branch Neighborhood identified in the 1994 Clarksburg Master Plan which includes properties south of West Old Baltimore Road. The Clarksburg Master Plan calls for the Cabin Branch Neighborhood to be a mixed-use, planned community. The Plan recommends MXP and RMX/TDR zoning to accommodate the mix of uses and proposed densities.



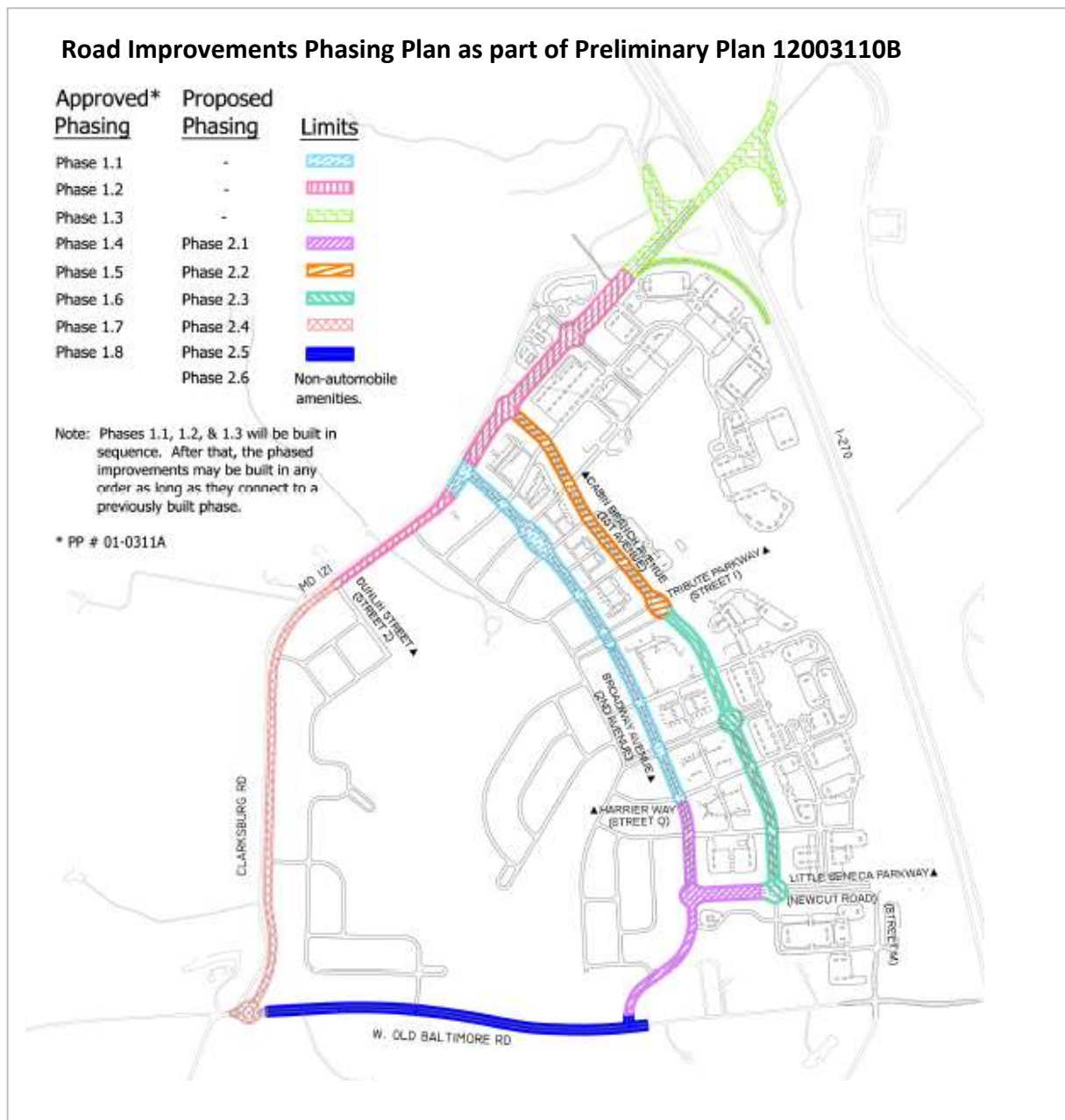
Although the Master Plan called for an integrated mix of uses including employment, all forms of housing, recreation and open space facilities, retail space, park areas and a site for an elementary school, it did not prescribe how the various property owners might jointly accomplish the goals. In 2002, the four major property owners of the 540 acre tract joined together to create an integrated vision for this major portion of the Clarksburg Neighborhood where the greatest mix of uses would occur. In effect, they ignored the property lines in order to establish a cohesive plan that would achieve all of the Master Plan objectives.

Those owners then presented the overall concept plan to the Montgomery County Planning Board in the form of a Pre-Preliminary Application, which the Planning Board unanimously endorsed. Following the Pre-Preliminary Application, the owners then filed a rezoning application (Local Map Amendment No. G-806) to reclassify 283.5 acres of the tract to the MXP zone in order to accommodate the Master Plan land uses and densities, leaving the remainder (approximately 257 acres) in the RMX/TDR zone, again pursuant to the Master Plan.

As part of the MXPD rezoning, the Planning Board supported and the Montgomery County Council approved a Development Plan for the tract that identified the various land uses, densities, overall plan orientation, general roadway configuration and the location of the prescribed parks and public school site. That Development Plan approval included nine binding elements that defined the allocation of TDRs and MPDUs, off-site amenity features, trip reduction measures and roadway issues among others. Also, at the request of WSSC, in 2005 the property owners prepared a Facility Plan for WSSC for Stages 3 and 4 which refined the framework for WSSC CIP improvements for the Clarksburg area west of I-270.



With the general vision of the Master Plan in mind, and the approved Development Plan in hand, the property owners of the Cabin Branch community then applied and obtained approval of a Preliminary Plan of Subdivision. (Preliminary Plan No. 12003110 and 12003110B). Some of the original property owners had conveyed their interests to Winchester Homes, Adventist HealthCare and other members of Cabin Branch Management who proceeded to implement the project. The Preliminary Plan, and an amendment to it, approved 1,886 dwelling units (in a mix of various single-family detached, single family attached and multi-family configurations), 500 senior housing units, 2,420,000 square feet of commercial space and the various open space and public school sites. The Preliminary Plan approval provided additional definition with respect to the location and configuration of on and off-site roadway improvements, development staging and other features. The Preliminary Plan approval also called for the submission and approval of an Infrastructure/Roads Site Plan to further define the location, configuration and timing of these improvements.



In 2007, the Planning Board approved the Cabin Branch Infrastructure/Roads Site Plan (Site Plan No. 820050150) and, in 2008, approved a Minor Amendment to that Site Plan relating to West Old Baltimore Road improvements. The Infrastructure Site Plan provided detailed definition for the location and cross section of roads within Cabin Branch and features of the parks, open space, as well as the public school site. That approval also included approval of a final Water Quality Plan for the project (No. 820050150).

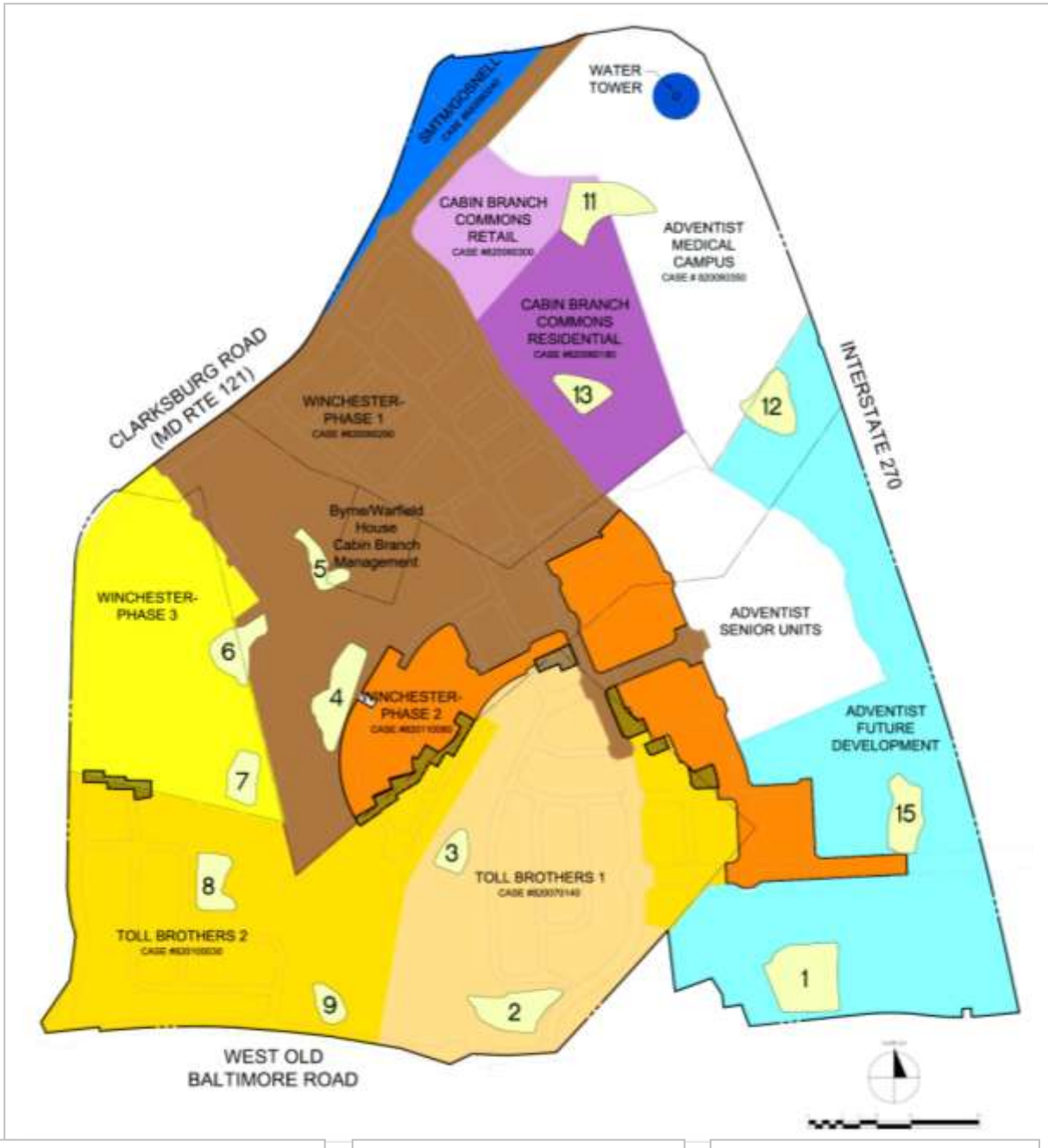
Case No.	Opinion	Type	Use Approved	Status
G-806	9/9/2003	Local Map Amendment	Rezoned 283.5 acres to MXPB on the eastern portion of the tract. Development Plan for 535 acres to allow 1,886 dwelling units, 2,240,000 sf commercial and 500 senior housing units.	approved
120031100 12003110A	6/22/2004	Preliminary Plan & Amendment	1,600 DU, 500 Sr. units, transportation improvements, LATR review, road construction phasing.	approved
12003110B	10/6/2008	Preliminary Plan Amendment	1,886 DR, 2,420,000 sf commercial, 500 sr. units.	approved
820050150	6/9/2008	Infrastructure Site Plan	Density per 12003110B , maximum 635 TDRs, 236 MPDUs, 28.32 acres green space, public water and sewer (W-3), Master Plan Unit Mix, 2,436,000 sf retail and employment.	approved
820060290	10/6/2008	Winchester I Site Plan	428 DU, including Multi-family, attached and detached one-family homes, 64 MPDUs, 128 TDRs, 62.55 green space.	approved, amendment filed
820060240	9/29/2010	Gosnell Site Plan	Hotel, banks (employment) 87,500 sf and 8,600 auto-related uses, parking waiver.	approved
MR08001	4/3/2008	Mand. Ref	WSSC elevated water storage tank	transmittal
16-35 17-04	4/1/2009 4/1/2011	Subd. Regl Subd. Regl	Extension of validity period Extension of validity period	approved

Since obtaining approval of the Infrastructure/Roads Site Plan, various property owners that comprise the Cabin Branch community, have proceeded with more detailed site plans for development of the various blocks and areas. Winchester Homes obtained the first approval, Site Plan No. 820060290, in 2008. Given its status as the initial detailed Site Plan, that Site Plan included approval of various overarching features for the whole project including the approval of Design Guidelines, design and construction details for the dedicated public park, grading and configuration of the proposed elementary school site, allocation of TDRs and MPDUs, infrastructure forest conservation, stormwater management. It updated the Final Water Quality Plan to reflect the more detailed aspects.

Since that Site Plan was approved, a Site Plan for part of the employment area along Maryland Route 121 (the Gosnell Site Plan) also has been approved and others filed by Winchester Homes, Toll Brothers and Concordia are pending. The project obtained approval for construction of a trunk line sewer, through Black Hills Park and into the property, which has since been constructed. The developers of the project also worked with the Washington Suburban Sanitary Commission under mandatory referral to obtain approval for the water tower to be built on the property that will service both this project and the rest of Clarksburg.

Approved Phasing	Proposed Phasing	Improvement(s)	Approved Development Program (PP 01-03110A)	Proposed Development Program (PP 01-03110B)
I.1		2 nd Ave (A-304 West) – from MD 121 to Street Q, two-lane business district roadway.	210 Units 190,000 S.F.	–
I.2		MD 121 – from Street Z to the I-270 interchange, two-lane to four-lane to six-lane arterial.	210 Units 125 Senior Units 190,000 S.F.	–
I.3		MD 121 – I-270 inter-change and bridge improvements.	210 Units 125 Senior Units 190,000 S.F.	–
I.4	2.1	2 nd Ave (A-304 West) – Street Q to W. Old Baltimore Road, two-lane bus. District road. Newcut Rd (A-302) – from 2 nd Ave to I st Ave, four-lane divided roadway.	194 Units 125 Senior Units 190,000 S.F.	57 Units 101,250 S.F.
I.5	2.2	I st Ave (A-304 East) – from MD 121 to Street I, two-lane business district roadway.	194 Units 190,000 S.F.	57 Units 101,250 S.F.
I.6	2.3	I st Ave (A-304 East) – from Street I to Newcut Rd (A-302), two-lane business district roadway.	194 Units 125 Senior Units 190,000 S.F.	57 Units 101,250 S.F.
I.7	2.4	MD 121 – from W. Old Baltimore Rd to Street Z, upgrade to two-lane arterial.	194 Units 190,000 S.F.	57 Units 101,250 S.F.
I.8	2.5	West Old Baltimore (A-7) – MD 121 to Newcut Road, upgrade to two-lane arterial.	194 Units 208,000 S.F.	58 Units 101,250 S.F.
	2.6	Non-automobile Transportation amenities.	–	375,750 S.F.

Case No.	Filed	Type	Pending Plans	Status
820060180	10/1/2005	Resd. Site Plan	Concordia Phase 1 Residential Site Plan: 116 one-family attached townhouse units, 6 MPDUs, combined 2-over-2 units, 7,208 sf office	
820060300	2/1/2006	Retail Site Plan	Concordia Phase 2 Retail for 87,878 sf commercial retail and 4.387 sf employment	
820060350	3/1/2006	Site Plan	Phase I Adventist Medical Campus Site Plan: 234,000 sf of medical office space	
820070140	2/13/2007	Resd. Site Plan	Toll Brothers Phase 1: 159 one-family detached and 60 one-family attached unites, including 24 MPDUs	
820100030	12/23/2010	Resd. Site Plan	Toll Brothers Phase 2: 174 one-family detached units and 65 one-family attached units 78 4-story multi-family units.	



APPROVED

Overall	Pre- Application #7-20020420 Date: 05/27/2002 MXPDP Zoning/DPA #G-606 Date: 09/09/2003
Overall	Preliminary Plan #1-20033110A Date:06/22/2004
Overall	Infrastructure Site Plan #8-2005015A Date:09/17/2007
	Water Tower Mandatory Referral #06001-WSSC-1 Date: 04/03/2008
Overall	Preliminary Plan #1-20033110B Date:10/06/2008
	Winchester- Phase 1 Site Plan #8-20060290 Date: 10/20/2008
	SMTM/Gosnell Site Plan #8-20060240 Date:09/29/2010

PENDING

	Cabin Branch Commons Residential Site Plan #8-20060180 Submitted: 10/01/2005
	Cabin Branch Commons Retail Site Plan #8-20060300 Submitted: 02/01/2006
	Adventist Medical Campus Site Plan #8-20060350 Submitted: 03/01/2006
	Toll Brothers 1 Site Plan #8-20070140 Submitted: 02/13/2007
	Toll Brothers 2 Site Plan #8-20100030 Submitted: 11/24/2009
	Winchester- Phase 2 Site Plan #8-20110080 Submitted: 03/22/2011
	Winchester- Phase 1 Site Plan Amendment #8-20060290A Submitted: 05/2011

FUTURE

	Winchester- Phase 3 Site Plan
	Adventist Future Development
	Adventist Senior Units
	Win/Toll Border Site Plan Amendment

ANALYSIS

Relationship to the Master Plan

The plan proposed for the Cabin Branch Winchester Phase 1 site plan conforms substantially with the recommendations of the 1994 Clarksburg Master Plan.

The July 2011 Approved and Adopted Limited Amendment to the 1994 Master Plan (for retail staging) modifies the 1994 Master Plan provision that required 90,000 sq. ft. of retail space in the Town Center to occur prior to retail uses in the designated Newcut Road and Cabin Branch neighborhood village centers. The Amendment allows retail uses, having been approved with Preliminary Plans, in the two village centers, including Cabin Branch, to proceed at any time.

The subject plan applications for the amended Win 1 Site Plan and the Win 2 Site Plan, with a total of 780 residential units, are therefore, as demonstrated by the applicant's revised plan, are consistent with the recommendations of the approved master plan and overall density guidelines for the Cabin Branch neighborhood project. The overall plan will retain the:

- Variety and mix of land uses as approved
- Land uses and density limit totals as approved
- Required public use and community building components including a school
- Overall infrastructure and roadway development program included required ROW dedications
- Project Design Guidelines



Clarksburg Road/ MD Route 121 Arrival & Greenway

Conformance with the Master Plan

Mixed Use Neighborhood, Housing Density, and Unit Mix

The two concurrent Winchester site plans propose a total of **781** residential units within two Winchester phases of development. Restoration is proposed for the historic Byrne-Warfield house that occupies the center of the Winchester 1 Plan. A variety of housing types are distributed throughout the site, including three sizes of one-family detached units, several attached unit types, and the newly introduced unit type, the Urban Loft. The plan amendment for Winchester 1, proposes to eliminate the multi-family, 2-over-2 unit type, which affords greater compatibility in an attractive market mix of residential products across the site’s two zones. A rich variety of mixed uses anchor the neighborhood’s east side with housing, retail and employment, while the west side features major public amenities of the public school, the local parkland the community center and swimming pool.

The Master Plan recommends a housing density of 5-7 units per acre for the residential areas north of the Newcut Road extension. The residential density of Winchester I and II site plans, averaged over the proposed phases, is consistent with Master Plan recommendations.

- Win 1 MXPB Zone: 4.1 du/acre
- Win1 RMX-TDR1 Zone: 2.43 du/acre
- Win2 MXPB: 9.3 du/acre
- Win2 RMX-TDR1 Zone: 8.2 du/ac.

The residential unit mix recommended by the Master Plan is as follows:

	MP %	Win 1 %	Win 1 No.	Win 2 %	Win 2 No.
Sf detached	45 - 55	26.4%	117	31.3	107
Sf attached	35 – 45*	76.4%	323	68.7	234
Multi-Fam.	10 - 20		0		0
	Totals		440		340

Strong Neighborhood-Park Relationships

A major feature of the overall Cabin Branch plan is the significant internal greenway network that easily accessible with excellent visibility from the adjacent roadways and developed areas. The green way network connects directly to the east and west stream buffers.

The I-270 Technology Corridor

Following the master plan recommendations and the binding elements of the Development Plan, the land adjacent to I-270 is planned for employment uses. Employment uses in this location will be of major importance to the project and the future of Clarksburg.

Transit-Oriented Neighborhood Serving the Tech Corridor

The street pattern and hierarchy provides the basis for an easily navigable transportation system that supports transit use, bus links, and use of the existing MARC station and the future CCT station, both less than three miles away.

Environmental Protection and Landscape Buffers

The plan proposed to protect the stream valleys with 100-175 foot buffers. Stormwater control, vaults and sand filters, are designed as small dry ponds distributed throughout the site, landscaped in native plants. Stormwater outfalls are excluded from the stream buffers, with rare exception. The landscaping proposed for the MD 121 frontage follows the Master Plan guidance to provide abundant foliage and compatible structures as a mediated transition to the more rural land uses. [See illustration, p.27.]

Development Standards: Regulatory Analysis [See also: Findings]

The planning history of the Cabin Branch neighborhood, starting with the Master Plan adopted in 1994, has consistently considered the 535-acre site as a single entity, whose development, even with numerous property owners, is intended to achieve a unified, mixed-use community. The sequence of development approvals that followed the Development Plan, that is, the Preliminary Plan and the Site Infrastructure Plan continue to reinforce the concept of a single tract, irrespective of individual parcels. That the subsequent site plan submissions are drawn by parcel limits, not the tract, seemingly contradicts the rationale upon which the planning vision, and the foundation plans are based.



Green Area

Numerous factors such as erodible soils, steep slopes, the size and locations of water quality and quantity controls, and limited areas of soils suitable for road construction, all constrain the developable area available for housing. However, in the MXPDP portion of the tract, it is the large expanse of the two stream valleys and their generous protective buffers flanking the center that create the statistically significant *weighted average* of open space that favors the land parcels in which the valleys are located--and, by inference, inversely affects the location and density of the housing yield on the remaining central parcels, bound as they are by the zone’s required percentage of green space. In other words, preserving the sensitive eco-system through front-loading housing in the areas with soils most suited for transportation, requires a trade-off in the provision of the green space in the MXPDP zone.

Whereas the MXPDP zone requires 50% green space in residential areas, and 40% in commercial areas, the proposed sharing of green area, determined by the placement of the stream buffers is as follows:

Cabin Branch MXPDP Zone Tract Portion - Use	Acres	Green Area Required	%	Green Area Provided	Addl Green Area Reqd	Devel. Area Available	% DAA Reqd for Green Area
MXPDP Tract Area Gross Acreage	283.50 ac.						
Road Dedications	27.90 ac.						
Residential Acreage	68.50 ac.	35.30 ac.	50%	12.50 ac.			
Win-1, Win-2, Toll, Concordia Resd.							
Commercial/Industrial	187.10 ac.	74.8 ac.	40%	69.20 ac.*		117.90 ac.	
Adventist, Concordia Retail							
Total Acreage	283.50 ac.	109.10 ac.		81.70 ac.	27.40 ac.	117.90 ac.	23.20%

* East Side Stream Valley Buffer

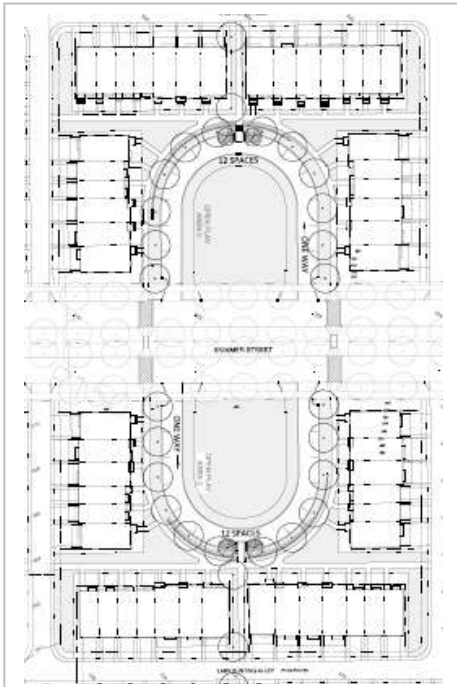
Such is the case for the area zoned MXPDP within the two concurrent site plans under this review: Winchester 1 and Winchester II. Winchester 1 provides approximately 47% green space, while the tight site area of Winchester 2 results in 22% green space. Here, the stream buffers lie within the Adventist and Cabin Branch Commons parcels that align the I-270 right-of way on the tract’s eastern edge, while the Clarksburg Road frontage, the primary access point with structurally sufficient soils extending to the tract center, suggests housing. It is this complementary relationship that propels the proposed pattern of development for the Cabin Branch Neighborhood. This plan offers abundant areas of green space as averaged across the entire MXPDP zone area, with the 27.5-acre residential section shortfall provided from within the commercial area at a percentage of 23.30% of the available developable land. Thus, among the employment, commercial and residential areas: mixed-uses with a striking mix of green spaces

It should be noted that, the “East Side,” the RMX-TDR-1 section, provides ample green space, 57%, enhanced by the dedication of the schools and parks sites. [See the above chart.]

Recreation Analysis



Skimmer Road- North Lawn/South Lawn



Recreational amenities proposed under the Winchester 2 site plan exceed the requirements of the Recreation Guidelines. Pedestrian and bike systems traverse the entire site; from stream buffer to stream buffer, nature trails link to sidewalks and bike paths. The natural areas are easily accessible, as is Black Hills Regional Park that adjoins Cabin Branch on its south edge at Old West Baltimore Road.

Three open space amenities are proposed for this section of housing: The Commons, The North and South Lawns, and Shaw Park. Each is a formal green, tailored distinctively for its enclave, in proportion and orientation. These amenities are generous in dimension, with well-marked edges— an assertion of spatial relief critical to the balance of the figure ground, the pattern of solid and void.

Winchester I: Recreation Supply and Demand

CATEGORY	Tots	Children	Teens	Adults	Seniors
DEMAND	49	64	60	393	31
SUPPLY	54	84	107	551	47

Recreation Type		Tots	Children	Teens	Adults	Seniors
Tot Lot Supply Values						
Total Number of Tot Lots Provided	1	9	2	0	4	1
SFD III Tot Lot Provided - HOA	1	9	2	0	4	1
TH Tot Lot Provided - HOA	0	0	0	0	0	0
Tot Lot Total Supply Value		9	2	0	4	1
Picnic / Sitting Area Supply Values						
Total Number of Picnic / Sitting Areas Provided	5	1	1	1.5	5	2
SFD III Picnic/Sitting Area Provided - HOA	2	2	2	3	10	4
TH Picnic/Sitting Area Provided - HOA	3	3	3	4.5	15	6
Picnic / Sitting Areas Total Supply Value		5	5	7.5	25	10
Open Play Area I Supply Values						
Total Number of Open Play Area I Provided	1	6	9	12	30	2
SFD III Open Play Area I Provided - HOA	1	6	9	12	30	2
TH Open Play Area I Provided - HOA	0	0	0	0	0	0
Open Play Area I Total Supply Value		6	9	12	30	2
Open Play Area II Supply Values						
Total Number of Open Play Area II Provided	4	6	9	12	30	2
SFD III Open Play Area II Provided - HOA	1	6	9	12	30	2
TH Open Play Area II Provided - HOA	3	18	27	36	90	6
Open Play Area II Total Supply Value		24	36	48	120	8

Bike System Supply Values						
		0.05 x D1	0.10 x D2	0.15 x D3	0.15 x D4	0.10 x D5
Total Bike Systems Provided	2					
SFD III Bike System Provided	1	0.749	2.033	3.6915	20.3835	1.391
TH Bike System Provided	1	1.989	5.148	6.318	45.279	2.106
Bike System Total Supply Value		2.738	7.181	10.0095	65.6625	3.497
Pedestrian System Supply Values						
		0.10 x D1	0.20 x D2	0.20 x D3	0.45 x D4	0.45 x D5
Total Pedestrian Systems Provided	2					
SFD III Pedestrian System Provided	1	1.498	4.066	4.922	61.1505	6.2595
TH Pedestrian System Provided	1	3.978	10.296	8.424	135.837	9.477
Pedestrian System Total Supply Value		5.476	14.362	13.346	196.9875	15.7365
Natural Areas Supply Values						
		0.00 x D1	0.05 x D2	0.10 x D3	0.10 x D4	0.05 x D5
Total Natural Areas Provided	2					
SFD III Natural Areas Provided	1	0	1.0165	2.461	13.589	0.6955
TH Natural Areas Provided	1	0	2.574	4.212	30.186	1.053
Natural Areas Total Supply Value		0	3.5905	6.673	43.775	1.7485
Nature Trails Supply Values						
		0.05 x D1	0.10 x D2	0.15 x D3	0.15 x D4	0.15 x D5
Total Nature Trails Provided	2					
SFD III Nature Trails Provided	1	0.749	2.033	3.6915	20.3835	2.0865
TH Nature Trails Provided	1	1.989	5.148	6.318	45.279	3.159
Nature Trails Total Supply Value		2.738	7.181	10.0095	65.6625	5.2455

FINDINGS

1. *The Site Plan conforms to all non-illustrative elements of a development or diagrammatic plan, and all binding elements of a schematic development plan, certified by the Hearing Examiner under Section 59D-1.64, or is consistent with an approved project plan for the optional method of development if required, unless the Planning Board expressly modifies any element of the project plan.*

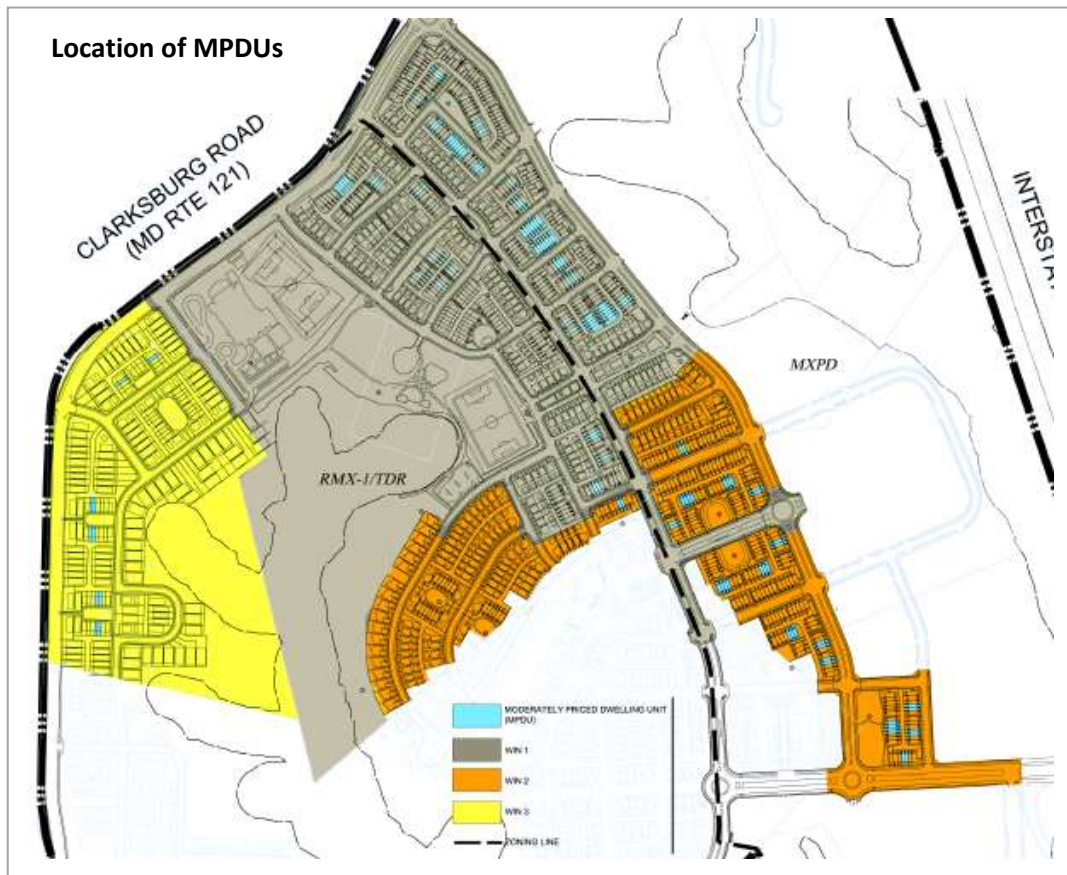
The proposed development, as conditioned, is consistent with Development Plan G-806, adopted September 9, 2003, with respect to land use, density, building locations, building height, and development staging as noted in the nine Binding Elements:

a. *Transferable Development Rights and Moderately Priced Dwelling Units*

The Property that is subject to this application (283.5 acres) is part of a larger, mixed-use community planned for 535 acres shown on the Development Plan, of which the Applicants are also owners. The portion of the property not being rezoned MXPd, is zoned RMX-1/TDR and will require purchase of Transferable Density Rights (TDR's) for the development planned by Applicants. Based on calculations developed with M-NCPPC staff, the total Master Plan residential density for the 535-acre community is 1,676 market rate units plus 210 Moderately Priced Dwelling Units. Assuming this density and the mix of unit types called for in the Master Plan for the entire Cabin Branch Neighborhood (including a maximum of 20% multi-family) the 535-acre project will require 635 TDR's. The Preliminary Plan of Subdivision application(s) for the MXPd area and the remaining RMX-1/TDR area shall require the purchase of TDR's in conformance with this calculation.

This application, Winchester 2, provides 41 MPDUs (12.02%), using credit from the Winchester 1 Site Plan's MPDU credit to meet the required number. The MPDUs are provided entirely as attached one-family dwellings. The TDRs provided total 102, as required.

MPDU UNITS		PROPOSED MXPd ZONE (1)	PROPOSED RMX1/TDR ZONE (1)	TOTAL (1)
SITE PLANS	SMTM (GOSNELL) (#820060240) APPROVED 9/29/10	0	0	0
	WINCHESTER PHASE 1 (#82006029A)	53	17	70
	WINCHESTER PHASE 2 (#820110080)	39	2	41
	WINCHESTER PHASE 3 (FUTURE)	0	10	10
	TOLL BROTHERS PHASE 1 (#820070140)	0	27	27
	TOLL BROTHERS PHASE 2 (#820100030)	26	14	40
	CABIN BRANCH COMMONS-RESIDENTIAL (#820060180)	15	0	15
	CABIN BRANCH COMMONS-RETAL (#820060300)	0	0	0
	ADVENTIST PHASE 1 (#820060350)	0	0	0
	ADVENTIST (FUTURE)	TBD	0	TBD
PARTIAL SUBTOTAL	133	70	203	
PRELIMINARY PLAN	PRELIMINARY PLAN (#120031108) (236 UNITS= 12.5% * 1895 UNITS)	NA	NA	236
TDRS		PROPOSED MXPd ZONE (1)	PROPOSED RMX1/TDR ZONE (1)	TOTAL (1)
SITE PLANS	SMTM (GOSNELL) (#820060240) APPROVED 9/29/10	0	0	0
	WINCHESTER PHASE 1 (#82006029A)	0	132	132
	WINCHESTER PHASE 2 (#820110080)	0	102	102
	WINCHESTER PHASE 3 (FUTURE)	0	TBD	0
	TOLL BROTHERS PHASE 1 (#820070140)	0	74	74
	TOLL BROTHERS PHASE 2 (#820100030)	0	83	83
	CABIN BRANCH COMMONS-RESIDENTIAL (#820060180)	0	0	0
	CABIN BRANCH COMMONS-RETAL (#820060300)	0	0	0
	ADVENTIST PHASE 1 (#820060350)	0	0	0
	ADVENTIST (FUTURE)	0	0	TBD
PARTIAL SUBTOTAL	0	401	401	
PRELIMINARY PLAN	PRELIMINARY PLAN (#120031108)	NA	NA	609



b. Off-Site Amenities and Features

By the time of issuance of building permits for the 100th dwelling unit in the Cabin Branch Community, which consists of the larger, mixed-use community of 535 acres shown on the Development Plan for an elementary school, a local park and a recreation facility. The school site will be rough-graded at a time determined at the earliest Preliminary Plan of subdivision application for the Cabin Branch Community, subject to Montgomery County Public School approval.

As conditioned, the proposed plan conforms to the binding elements for off-site amenities and features. Thresholds for amenities and public facilities are as follows:

100th Building Permit: School site must be dedicated.

440th Building Permit: School site and Local Park must be rough graded, according to standards of MCPS and the Parks Department, respectively; Community Entrance features must be constructed.

650th Building Permit: Community center and pool building permit obtained. Local Park must be constructed (or concurrent with the adjacent homes, if first).

c. Trip Reduction Measures

At the time of Preliminary Plan of Subdivision, the Applicant, M-NCPPC Transportation Planning staff, and Department of Public Works and Transportation (DPWT) staff will consider mutually

acceptable trip reductions measures. The parking ratios for non-residential uses in the Cabin Branch Community will be determined at Site Plan, considering trip reduction goals.

Trip reduction goals were established during the preliminary plan approval. The Planning Board's approval of the Infrastructure Site Plan established the road hierarchy that accommodates underground utilities and stormwater management facilities. The plan proposed fulfills the transportation management goals iterated in the Preliminary Plan and the Infrastructure Site Plan. Specific street design, addressed within the subject site plan review, established the public parking supply, including the Local Park surface lot (60 spaces), and on-street parking (MXP: 92 spaces, RMX: 233 spaces) throughout the Winchester I site plan, as approved by the Fire and Rescue Service.

d. Street Network

A network of public streets shall be provided, supplemented by private streets, in a grid pattern that promotes interconnectivity. Public streets will consist of Master Plan streets and additional business and residential streets to form blocks that, with the exception of Area D defined on the Development Plan, are substantially similar to the street system shown in the Road Hierarchy Plan of the Development Plan and that are subject to MNCPPC and DPWT approvals.

The Applicant has provided a network of public streets in a grid pattern that defines scaled blocks typical of neo-traditional type of development. The street network is patterned to accommodate the following: negotiate significant grades, accommodate underground infrastructure, and create a cohesive relationship with public and private uses proposed in the Cabin Branch neighborhood. The public streets proposed are consistent with the Master Plan and hierarchy defined in the Development Plan. See also *Street Character* section, below.

e. Area D Street Network

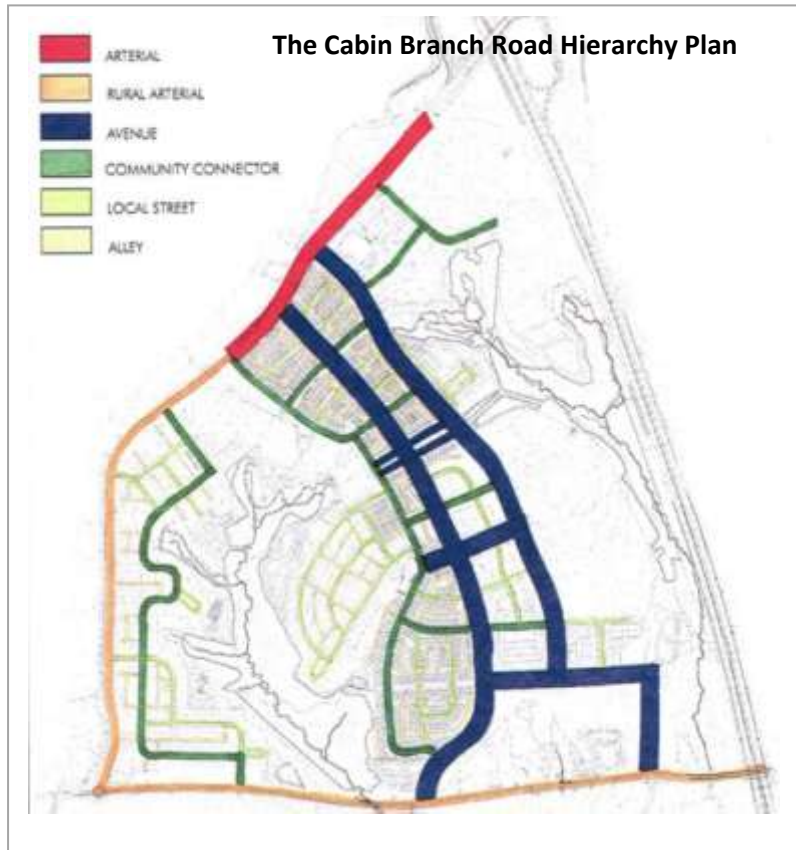
Area D will be designed with a public or private street connecting First Avenue (Route A-304) in a grid pattern with a particular emphasis on a building line to frame Newcut Road Extended, with parking in the rear.



Area D, the area located between future A-302 (Newcut Road), Second Avenue and West Old Baltimore Road, at the southern site boundary, will be reviewed in more detail when the site plan for this portion of the site is submitted. Area D is not included in the subject application.

f. Street Character

All streets will adhere to a pedestrian-friendly design to the extent practical, which places particular emphasis on a building line to frame the street, with parking in the rear. Within the core, pedestrian friendly uses including retail or office will be located on the first floor. The entire MXPDP area will conform to a Cabin Branch Community Streetscape Plan that is designed to integrate the entire community, which will be submitted at Site Plan and is subject to MNCPPC and DPWT approvals.



The Road Hierarchy Plan, shown above, as part of the Preliminary Plan, established the framework, not only for transportation functions, but more importantly, for the neighborhood character, set the block pattern. That in turn, crafts the figure ground pattern, solid and void, that forms the scale and texture of the environment. The resulting linear routes, both the dominant longitudinals and their subordinate transverses, provide appropriately scaled pedestrian experiences that connect directly to the numerous public amenities located throughout the site.

g. Special Roads

A-307 will be designed as an open section arterial road with wide green edges to provide a gateway to Black Hill Regional Park, subject to MNCPPC and DPWT approvals. The rural character of West Old Baltimore Road will be maintained by minimizing environmental impacts and providing generous green edges.

A-307 has been designed as an open-section 80-foot-wide arterial road with green edges that line up with the entrance to Black Hill Regional Park. West Old Baltimore Road was not reviewed with the Infrastructure Plan nor the subject site plan, in terms of impact to adjacent properties, including Black Hill Regional Park. The road is proposed as a two-lane arterial roadway with changes to vertical and horizontal alignment. The Applicant is required to submit a limited amendment and alternatives analysis to address impacts to adjacent properties including alignment, necessary easements, well and septic, clearing and grading, park boundaries, sight distance and a no-build option.

h. Service/Public Uses

Service/Public uses may include up to 500 units for independent living for Senior Adults or persons with disabilities, assisted living, life care or continuing care.

This binding element does not apply to the subject site plan.

i. On-street Parking

Applicants will include on-street parking on streets adjacent to retail facilities.

See above, 1.C, *Trip Reduction Measures*, for discussion of on-street parking for residential areas. The retail section of Cabin Branch is not included in the subject application.

-
2. *The Site Plan meets all of the requirements of the RMX/TDR-1 zone and the MXPDP zones and where applicable conforms to an urban renewal plan approved under Chapter 56.*

The Site Plan meets all of the requirements of the MXPDP and RMX-1/TDR Zones as demonstrated in the two project Data Tables shown below. The subject site plan establishes the minimum setbacks and building restriction lines, maximum building heights and coverage requirements in terms of lot area, open space and green space for the respective zones.

This site plan application features a tightly circumscribed plan boundary, drawn so that its blocks are incised almost precisely to the curb line. This selective site boundary abuts, but does not claim, any natural forestation, and combined with the expansive area of stream valley buffers outside of the plan boundary, create a development envelope that cannot meet an internal green space requirement on its own, however close the proximity. The extension of the development pattern from Winchester 1, and the close relationship of the RMX-TDR portion of the site, suggest logically, a green space requirement that is calculated overall, per each zone, for the entire tract. That is, the total requirements for the 25.10 acres in the MXPDP zone and the 13.1 acres in the RMX/TDR-1 zone are quantified, with the shortfall to be “claimed,” subsequently from the various commercial and residential areas that remain available for development. The green space provided under Winchester 1, for the MXPDP zone and the RMX zone comprises approximately 22% of the site plan boundary area. The difference must be accounted for under subsequent site plans.

See also: *Site Plan Findings* and the following attachments:

- Overall Development Data, Cabin Branch
- Winchester 2, MXPDP Zone and
- Winchester 2, RMX-TDR-1 Zone

Development Data Tables

		PROPOSED	PROPOSED	TOTAL (1)	
SITE AREA - ACRES (+/-)					
SITE PLANS	SMTM (GOSNELL) (#820060240) APPROVED 9/29/10	7.24	0	7.24	
	WATER TOWER MANDATORY REFERRAL (#08001-WSSC-1) APPROVED 4/3/2008	1.487	0	1.487	
	WINCHESTER PHASE 1 (#82006029A)	56.92	84.99	141.91	
	WINCHESTER PHASE 2 (#820110080)	25.1	13.1	38.2	
	WINCHESTER PHASE 3 (FUTURE)	0	TBD	TBD	
	TOLL BROTHERS PHASE 1 (#820070140)	1.06	56.23	57.29	
	TOLL BROTHERS PHASE 2 (#820100030)	39.02	69.54	108.56	
	CABIN BRANCH COMMONS- RESIDENTIAL (#820060180)	29.49	0	29.49	
	CABIN BRANCH COMMONS -RETAIL (#820060300)	12.06	0	12.06	
	ADVENTIST PHASE 1 (#820060350)	34.77	0	34.77	
	ADVENTIST (FUTURE)	TBD	0	TBD	
	ADVENTIST SENIOR UNITS (FUTURE)	TBD	0	TBD	
	PARTIAL SUBTOTAL	207.147	223.86	431.007	
INFRASTRUCTURE PLAN	INFRASTRUCTURE SITE PLAN (#82005015A) APPROVED 9/17/2007	283.50	251.54	535.04	
PRELIMINARY PLAN	PRELIMINARY PLAN (#12003110B)	283.50	251.54	535.04	
		PROPOSED	PROPOSED	TOTAL (1)	
RESIDENTIAL DENSITY- DWELLING UNITS					
SITE PLANS	SMTM (GOSNELL) (#820060240) APPROVED 9/29/10	0	0	0	
	WINCHESTER PHASE 1 (#82006029A)	232	208	440	
	WINCHESTER PHASE 2 (#820110080)	234	107	341	
	WINCHESTER PHASE 3 (FUTURE)	0	TBD	TBD	
	TOLL BROTHERS PHASE 1 (#820070140)	0	216	216	
	TOLL BROTHERS PHASE 2 (#820100030)	121	196	317	
	CABIN BRANCH COMMONS- RESIDENTIAL (#820060180)	128	0	128	
	CABIN BRANCH COMMONS -RETAIL (#820060300)	0	0	0	
	ADVENTIST PHASE 1 (#820060350)	0	0	0	
	ADVENTIST (FUTURE)	TBD	0	TBD	
	PARTIAL SUBTOTAL	715	727	1442	
	PRELIMINARY PLAN	PRELIMINARY PLAN (#12003110B)	1026(1)	860(1)	1886
		PROPOSED MXPD ZONE (1)	PROPOSED RMX1/TDR ZONE (1)	TOTAL (1)	
GREEN AREA- ACRES (2)					
SITE PLANS	SMTM (GOSNELL) (#820060240) APPROVED 9/29/10	2.49	0	2.49	
	WINCHESTER PHASE 1 (#82006029A)	12.3	54.09	66.39	
	WINCHESTER PHASE 2 (#820110080)	5.5	2.9	8.4	
	WINCHESTER PHASE 3 (FUTURE)	0	TBD	TBD	
	TOLL BROTHERS PHASE 1 (#820070140)	0	27.99	27.99	
	TOLL BROTHERS PHASE 2 (#820100030)	4.52	46.52	51.04	
	CABIN BRANCH COMMONS- RESIDENTIAL (#820060180)	22.13	0	22.13	
	CABIN BRANCH COMMONS -RETAIL (#820060300)	2.07	0	2.07	
	ADVENTIST PHASE 1 (#820060350)	23.8	0	23.8	
	ADVENTIST (FUTURE)	TBD	0	TBD	
	ADVENTIST SENIOR UNITS (FUTURE)	TBD	0	TBD	
	PARTIAL SUBTOTAL	TBD	TBD	TBD	
	PRELIMINARY PLAN	PRELIMINARY PLAN (#12003110B)	283.50	251.54	535.04

Winchester 2: RMX-TDR-1 Zone Development Standards



Lot/Tract and Residential Density

RMX-1/TDR ZONE	DEVELOPMENT DATA STANDARD METHOD	RMX/TDR STD.	MASTER PLAN ZONING	PRELIMINARY PLAN	PROPOSED WINCHESTER-PHASE 2
§59-C-1.339	LOT AND TRACT AREA				
	GROSS TRACT AREA		251.8 AC.S±†		13.1 AC.S ±▲
	NET AREA FOR RESIDENTIAL USE - AC.S			N/A	9.2 AC.S ±▲
	NET AREA FOR RESIDENTIAL USE -(SF)			N/A	(400,752 SF ±▲)
	DEDICATION - PUBLIC ROADS- AC.S			N/A	2.2 AC.S ±▲
	DEDICATION - PRIVATE ROADS- AC.S			N/A	1.0 AC.S ±▲
	DEDICATION - PUBLIC USES - AC.S			N/A	0.7 AC.S ±▲
§59-C-1.332 & §59-C-1.335	RESIDENTIAL DENSITY (1)				
	NUMBER OF DWELLING DR FOR SITE > 30 AC.S		3 OR GREATER DWELLING DR	3.22 DU/AC.† (1)	8.2± DU/AC.▲
	TOTAL NUMBER DWELLING DR THIS APPLICATION			N/A	107 DR.▲
	DWELLING DR THIS APPLICATION AS % OF TOTAL 1,886			N/A	5.7% ± (107 DR/1886 DR)▲
	ONE-FAMILY DETACHED (SF)				
	ONE-FAMILY DETACHED DU/AC.			N/A	6.1 DU/AC ± (80 DR/13.1 AC.S)▲
	ONE-FAMILY DETACHED % OF ALL DR THIS APPLICATION			N/A	74.8% ± (80 DR/107 DR)▲
	ONE-FAMILY ATTACHED (TH)				
ONE-FAMILY ATTACHED DU/AC.			N/A	2.1 DU/AC ± (27 DR/13.1 AC.S)▲	

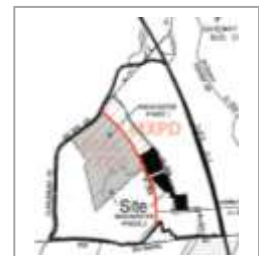
Building Setbacks & Height, Parking, Green Area

§59-C-1.334(b)	SETBACKS	MIN.			
§59-C-1.335	ONE-FAMILY DETACHED (SF)				
	FRONT	MIN.		10'	10'
	REAR	MIN.		0'	0'
	SIDE (STREET SIDE)	MIN.		10'	10'
	SIDE (INTERIOR LOT)	MIN.		3'	3'
	SIDE (ALLEY)	MIN.		3'	3'
	R/W TRUNCATION	MIN.		0'	0'
	ONE-FAMILY ATTACHED (TH)				
	FRONT	MIN.		10'	10'
	REAR	MIN.		0'	0'
SIDE (STREET SIDE)	MIN.		10'	10'	
SIDE (INTERIOR LOT)	MIN.		3'	3'	
SIDE (ALLEY)	MIN.		3'	3'	
R/W TRUNCATION	MIN.		0'	0'	
§59-C-1.335	BUILDING HEIGHT (1) (3)	MAX.			
	MAIN BUILDING - FEET	MAX.		N/A	50 FEET MAX.† (3)
	MAIN BUILDING - STORIES	MAX.		4 STORIES MAX.†	4 STORIES MAX.† (3)
§59-E	OFF-STREET PARKING PER §59-E				
	§59-E-2.2 SIZE	MIN.	8.5' x 18'		8.5' x 18'
	§59-E-3.7 PARKING - ONE-FAMILY DETACHED PER UNIT	MIN.	2 /UNIT 160 ▲		136 ±▲ (4)
	PARKING - ONE-FAMILY ATTACHED PER UNIT	MIN.	2 /UNIT 54 ▲		36 ±▲ (4)
	PARKING - OFF STREET			N/A	232 ±▲ (4)
	PARKING - ON STREET			N/A	SEE FIRE & RESCUE PLAN (5)
	PARKING - VISITOR			N/A	0 ±▲
PARKING - BIKES			N/A	2 ±▲	
§59-C-1.335	GREEN AREA (7)				
	RESIDENTIAL PORTION	MIN.	35%	N/A	2.3 AC.S ±▲ (7)

Winchester 2: RMX-TDR - Notes for Data

(1) SECTION 59-C-10.2.1 STANDARD METHOD OF DEVELOPMENT REGULATIONS. RMX/TDR ZONE: STANDARD METHOD PROJECTS IN THESE ZONES MUST OCCUR IN ACCORDANCE WITH THE DEVELOPMENT METHODS, STANDARDS, AND SPECIAL REGULATIONS OF THE R-200/TDR ZONE AS CONTAINED IN 59-C-1.331, 59-C-1.332 & 59-C-1.33. THE MAXIMUM DENSITY WITH THE USE OF
(2) N/A
(3) BUILDING HEIGHTS AS DEFINED BY THE ZONING ORDINANCE (SEC. 59-A-2.1) WILL BE DETERMINED AT TIME OF BUILDING PERMIT. FINAL BUILDING HEIGHTS WILL CONFORM TO THE REQUIREMENTS SET FORTH IN THE PRELIMINARY AND DEVELOPMENT PLANS, AND WILL NOT BE HIGHER THAN THE MAXIMUM HEIGHT NOTED ABOVE.
(4) OFF STREET PARKING= 21- 2 CAR GARAGE TOWNHOUSES= 42 GARAGE + 42 TANDEM . 6- 1 CAR GARAGE TOWNHOUSES= 6 GARAGE + 6 TANDEM . 80 ONE-FAMILY DETACHED DR= 160 GARAGE + 36 TANDEM FOR THE FRONT LOADED DR.
(5) PUBLIC ON-STREET PARKING IS BEING PROVIDED IN ADDITION TO THE PARKING THAT IS REQUIRED BY SECTION 59-E OF THE ZONING ORDINANCE. SEE THE FIRE AND RESCUE PLAN FOR TOTAL ON-STREET PARKING PROVIDED.
(6) N/A
(7) SEE OVERALL GREENSPACE EXHIBIT FOR CALCULATIONS THAT SHOW HOW GREENSPACE REQUIREMENTS ARE MET IN EACH ZONE FOR OVERALL 535.04 AC. SITE.
† NUMBER OR PERCENT FOR TOTAL 535.04 AC. SITE
▲ PARTIAL CALCULATION FOR THIS PORTION OF THE WINCHESTER- PHASE 2 SITE PLAN. SEE OVERALL TRACKING CHART FOR FULL PROJECT AND SITE PLAN CALCULATION(S).

Winchester 2 Development Standards: MXPD Zone



Lot/Tract and Density Data

§59-C-7.51	LOT AND TRACT AREA	MIN.				
			20 AC.†	283.5 AC.±†		
	GROSS TRACT AREA					25.1 AC. ±▲
	NET AREA FOR RESIDENTIAL USE -AC.				N/A	10.3 AC. ±▲
	NET AREA FOR RESIDENTIAL USE -(SF)				N/A	(448,668 SF ±▲)
	DEDICATION - PUBLIC ROADS - AC.				N/A	10.2 AC. ±▲
	DEDICATION - PRIVATE ROADS - AC.				N/A	1.6 AC. ±▲
	DEDICATION - PUBLIC HOA AREAS - AC.				N/A	3.0 AC. ±▲
	NET AREA FOR COMMERCIAL USE				N/A	N/A
§59-C-7.54	COMMERCIAL/INDUSTRIAL DENSITY	MAX.	0.75 FAR†		0.3 FAR†	N/A
	COMMERCIAL/INDUSTRIAL PER MASTER PLAN	MAX.		2,300,000 SF†		N/A
§59-C-7.52(b)(2)	RETAIL COMMERCIAL % OF GROSS COMMERCIAL	MAX.	20%†	120,000 SF†		N/A
§59-C-7.53	RESIDENTIAL DENSITY (1)					
§59-C-7.5(a)(4)	RESIDENTIAL EXCLUSIVE AREA LIMITATIONS	MAX.	40% TRACT†		N/A	N/A
§59-C-7.53(a)	RESIDENTIAL DENSITY IN RESIDENTIAL AREA	MAX.	44 DU/AC†		10.5 DU/ACRE† (1)	9.3± DU/ACRE▲
	RESIDENTIAL DENSITY IN COMMERCIAL AREA	MAX.	0.75 DU/AC.†		N/A	N/A
	TOTAL NUMBER DWELLING DU THIS APPLICATION				N/A	234 DU▲
	DWELLING DU THIS APPLICATION AS % OF TOTAL 1,886				N/A	12.4% ± (234 DU/1886 DU)▲
	ONE-FAMILY DETACHED (SF)					
	ONE-FAMILY DETACHED DU/AC.				N/A	1.1 DU/AC ± (27 DU/25.1 AC.)▲
	ONE-FAMILY DETACHED % OF ALL DU THIS APPLICATION				N/A	1.4% ± (27 DU/1886 DU)▲
	ONE-FAMILY ATTACHED (TH)					
	ONE-FAMILY ATTACHED DU/AC.				N/A	8.2 DU/AC ± (207 DU/25.1 AC.)▲
	ONE-FAMILY ATTACHED % OF ALL DU THIS APPLICATION				N/A	11.0% ± (207 DU/1886 DU)▲

Building Setbacks & Height

§59-C-7.55	SETBACKS	MIN.				
	FROM RESIDENTIAL ZONING OTHER THAN ONE-FAMILY DETACHED BUILDINGS (2') ONE-FAMILY DETACHED (SF)	MIN.	100 FEET		N/A	N/A
	FRONT	MIN.			10'	10'
	REAR	MIN.			0'	0'
	SIDE (STREET SIDE)	MIN.			10'	10'
	SIDE (INTERIOR LOT)	MIN.			3'	3'
	SIDE (ALLEY)	MIN.			3'	3'
	R/W TRUNCATION	MIN.			0'	0'
	ONE-FAMILY ATTACHED (TH)					
	FRONT	MIN.			10'	10'
	REAR	MIN.			0'	0'
	SIDE (STREET SIDE)	MIN.			10'	10'
	SIDE (INTERIOR LOT)	MIN.			3'	3'
	SIDE (ALLEY)	MIN.			3'	3'
	R/W TRUNCATION	MIN.			0'	0'
§59-C-7.55(c)	BUILDING HEIGHT (3)	MAX.				
	(b) HEIGHT TO SETBACK RATIO	MIN.			N/A	N/A
	RESIDENTIAL STRUCTURES- FEET	MAX.			N/A	50 FEET † (3)
	RESIDENTIAL STRUCTURES- STORIES	MAX.			4 STORIES MAX. †	4 STORIES † (3)

Parking, Green Area, and Public Amenities

	R/W TRUNCATION	MIN.			0'	0'
§59-C-7.55(c)	BUILDING HEIGHT (3)	MAX.				
	(b) HEIGHT TO SETBACK RATIO	MIN.			N/A	N/A
	RESIDENTIAL STRUCTURES- FEET	MAX.			N/A	50 FEET † (3)
	RESIDENTIAL STRUCTURES- STORIES	MAX.			4 STORIES MAX. †	4 STORIES † (3)
§59-C-7.58	OFF-STREET PARKING PER §59-E					
§59-E-2.2	SIZE	MIN.	8.5' x 18'			8.5' x 18'
§59-E-3.7	PARKING - ONE-FAMILY DETACHED PER UNIT	MIN.	2 /UNIT 54 ▲			54 ±▲ (4)
	PARKING - ONE-FAMILY ATTACHED PER UNIT	MIN.	2 /UNIT = 414 ±▲			715 ±▲ (4)
	CARRIAGE HOUSE	MIN.	ADDITIONAL SPACE/LOT ▲			TBD IN FUTURE
	PARKING - OFF STREET				N/A	769 ±▲ (4)
	PARKING - ON STREET				N/A	SEE FIRE & RESCUE PLAN (5)
	PARKING - VISITOR				N/A	24 ±▲
	PARKING - BIKES				N/A	6 ±▲
§59-C-7.56	GREEN AREA (7)					
	RESIDENTIAL PORTION	MIN.	50% †		45% † (6)	5.5 AC. ±▲ (7)
	COMMERCIAL PORTION	MIN.	40% †		45% † (6)	NA
	TOTAL GREENSPACE REQUIRED IN MXPD ZONE				45% † (6)	5.5 AC. ±▲ (7)
§59-C-7.57	PUBLIC FACILITIES & UTILITIES					
	PUBLIC SCHOOL - ACREAGE				N/A	NA
	PUBLIC PARKLAND - ACREAGE				N/A	NA
	PUBLIC AMENITIES - PRIVATE OWNERSHIP - ACREAGE				N/A	3.0 AC. ±▲

Winchester 1: MXPD - Notes for Data

(1) DENSITY MUST NOT EXCEED MASTER PLAN RECOMMENDATIONS AS DELINEATED IN THE DEVELOPMENT PLAN. PER APPROVED PRELIMINARY PLAN I2003110B AND SUBJECT TO CHANGE PER CONDITION #21 WHICH STATES "FINAL APPROVAL OF THE NUMBER AND LOCATIONS OF THE BUILDINGS, DWELLING DU, ON-SITE PARKING, SITE CIRCULATION, SIDEWALKS AND BIKE PATHS WILL BE DETERMINED AT SITE PLAN."
(2) (a) NO BUILDING OTHER THAN ONE-FAMILY DETACHED DWELLINGS SHALL BE CONSTRUCTED WITHIN 100 FEET OF ADJOINING PROPERTY THAT IS DEVELOPED WITH ONE-FAMILY DETACHED HOMES. (b) NO BUILDING PROPOSED FOR COMMERCIAL/INDUSTRIAL USE SHALL BE CONSTRUCTED LESS THAN 100 FEET FROM ANY ADJOINING PROPERTY RECOMMENDED FOR RESIDENTIAL ZONING AND LAND USE ON THE APPLICABLE MASTER PLAN.
(3) BUILDING HEIGHTS AS DEFINED BY THE ZONING ORDINANCE (SEC. 59-A-2.1) WILL BE DETERMINED AT TIME OF BUILDING PERMIT. FINAL BUILDING HEIGHTS WILL CONFORM TO THE REQUIREMENTS SET FORTH IN THE PRELIMINARY AND DEVELOPMENT PLANS, AND WILL NOT BE HIGHER THAN THE MAXIMUM HEIGHT NOTED ABOVE.
(4) OFF STREET PARKING= 165- 2 CAR GARAGE TOWNHOUSES= 310 GARAGE + 301 TANDEM. 52- 1 CAR GARAGE TOWNHOUSES= 52 GARAGE + 52 TANDEM. 27 ONE-FAMILY DETACHED DU= 54 GARAGE.
(5) PUBLIC ON-STREET PARKING IS BEING PROVIDED IN ADDITION TO THE PARKING THAT IS REQUIRED BY SECTION 59-E OF THE ZONING ORDINANCE. SEE THE FIRE AND RESCUE PLAN FOR TOTAL ON-STREET PARKING PROVIDED.
(6) DUE TO THE NATURE OF THE MXPD ZONE IN THE CODE, GREEN SPACE AVERAGE VARIES BASED ON THE DEFINED AREAS OF "RESIDENTIAL" AND "COMMERCIAL" WITHIN THE MXPD PORTION OF THE SITE. A BLENDED 45% MAXIMUM GREEN AREA REQUIRED FOR BOTH THE RESIDENTIAL & COMMERCIAL ZONE WAS APPROVED AT TIME OF PRELIMINARY PLAN.
(7) SEE OVERALL GREENSPACE EXHIBIT FOR CALCULATIONS THAT SHOW HOW GREENSPACE REQUIREMENTS ARE MET IN EACH ZONE FOR OVERALL 535.04 ACRE SITE.
† NUMBER OR PERCENT FOR TOTAL 535.04 AC.ITE
▲ PARTIAL CALCULATION FOR THIS PORTION OF THE WINCHESTER- PHASE 2 SITE PLAN. SEE OVERALL TRACKING CHART FOR FULL PROJECT AND SITE PLAN CALCULATION(S).

3. *The locations of the buildings and structures, the open spaces, landscaping, recreation facilities, and pedestrian and vehicular circulation systems are adequate, safe, and efficient.*

a. Buildings and Structures

General

The development form established with the Road Hierarchy Plan, approved as part of the Preliminary Plan, and engineered through the Infrastructure Site Plan, clearly defines the block pattern, and forms the basis of the neighborhood texture, broadly grained in the retail and commercial areas, and varied, yet finely grained for the residential lots that largely comprise this site plan. The buildings hold the street firmly, rigidly in some places, creating a consistent building line from block-to-block that shapes the street view, organizes the public realm, and provides direct pedestrian and vehicular axes, north and south, east and west. The resulting tapestry is further refined by variation in lots sizes augmented by an ensemble of housing types that offer a range of footprints, widths, and exterior architectural features—elements that are critical to achieving spatial variety and relief at level of density proposed. This density, with tightly spaced houses, create close-knit blocks that rely enormously on the public realm, that is, the scale and furnishings of the streets, and more importantly, the proximity and quality of the public use space.

Further complexity in the fundamental planning of the housing blocks is met with grading and slope issues, challenging the placement of underground utilities and stormwater management facilities into the desired “tapestry.” It is essentially an iterative process, back-and-forth, engineering and architecture, function and aesthetics, that arrives at the solution for a site such as Cabin Branch. The challenge, essentially, is finding a housing solution from the repertoire of American production housing, suitable for insertion into the topography of rolling hills. It is problem that begs for a small-scale, incremental approach, one not typically present in the large-scale developer’s armamentarium.

Winchester 2

The plan boundaries, splayed across the two zones, with little logic, creates a disjointed form, dense attached housing on the east, while on the west, tight Queens-type footprints on narrow lots, and finally, at the stream buffer, the largest, highest market rate units with garage front facades. It remains to be seen if the “infill” development expected from the Toll Brothers site can knit these parts together and approach a contextual resolution. Given the environmental constraints, the urgency of sustainable housing design, and future scarcity of building materials, a wise consideration would entertain the a means to achieve greater use of attached housing, or better yet, a new model of detached and semi-detached units that discards the walk-out basement and the front loaded garage.

It is the vision and understanding of the 1994 Clarksburg Master Plan that underpins all, establishing the amenities, environmental goals, transportation, historic preservation of the Bryne Warfield House and setting, with substantial housing needs, which are met, with accuracy in the Development Plan and its subsequent approvals and provide adequate, safe and efficient building locations.

b. Open Spaces

Each of the three mini-neighborhoods designed as Winchester 2 feature dedicated, formal open spaces. The majestic Manor House attached houses shape the North and South Lawns in effective fashion. The proportion of the lawns, expansively reaching across the boulevard, and their orientation allow optimal solar exposure in the morning and evening. The solar roof panels, offered as roof terrace penthouses, fully take advantage of the “window” and at the same time, offer a striking, elevated view of the natural environment. The simplicity of the space creates its grandeur, the broad scale defined in the increments through detailing, lighting and the seat walls that enscribe the space in three planes.

Likewise, Shaw Park, in its character as a “local” park serving its own block, relates to the entire neighborhood in a sweeping visual sense, offering a forced perspective of the entire swath of housing that steps, block by block up the hill, punctuated by significant public spaces.

The Commons, as well, is “confined” within its own neighborhood, too. Framed by detached units that form a gateway at each end, The Commons, like the North and South Lawns are focused internally, contemplating the room in which it is created. Conversely, Shaw Park looks outward, with views to Black Hills Park, and upward, scanning the Cabin Branch development. That this variety of open space is created, all within the larger frame of the natural setting of the stream buffers is a testament to the thoughtful design that emanates from an original vision.



The Commons

c. Landscaping and Lighting

Landscaping and lighting consists of a varying treatment within the public rights-of-way, as outlined in the Cabin Branch Design Guidelines. Staff has recommended that power supplies, lighting and furnishings be provided to support public events, such as concerts and films, for the major public spaces, the North and South Lawns, Shaw Park, and the Commons. The plan proposes a variety of themed lighting fixtures, in larger and smaller heights, scaled to the proportions of the spaces.

d. Recreation Facilities

Recreation facilities provided within Winchester Phase 2 are adequate, safe and efficient. As envisioned in the Master Plan, abundant recreation amenities are offered, both through the extensive natural areas of the stream buffers, and the formal open spaces that offer opportunities for spontaneous activity. The pedestrian and bicycle systems cover the entire site, allowing safe and efficient connective links between the amenities located throughout the development. The trail system, within the stream buffers, ties directly to the sidewalk and bike paths. The location and types of recreational facilities are well dispersed, easily and safely accessible.



e. Vehicular and Pedestrian Circulation

Vehicular and Pedestrian circulation is adequate, safe and efficient throughout the entire site.

The primary access points to the site are from multiple locations along MD 121 (Clarksburg Road), West Old Baltimore Road and future Little Seneca Parkway (A-302). The State Highway Administration proposed intersection improvements at I-270 with Clarksburg Road and Little Seneca Parkway. Two roundabouts are proposed on Clarksburg Road at the first intersection (Street A) that provides primary access to the Gosnell tract to the north, and Adventist Healthcare and the retail section to the south. The second roundabout on Clarksburg Road is located at the intersection with 1st Avenue (Cabin Branch Avenue), which is the primary route to the retail center and a major spine within the community that connects to Little Seneca Parkway. Two internal roundabouts are proposed on Little Seneca Parkway at the intersections with 1st and 2nd Avenues.

Second Avenue is an arterial road and is the primary spine within the community that connects Clarksburg Road and West Old Baltimore Road. Two additional important access points to the site include Street G (Bryne Park Drive), which follows the eastern boundary of the school and park site and Street Z, which is the primary access to the school. Street Z also provides access to a future residential portion of the development that includes primarily one-family detached and attached housing.

The overall Cabin Branch neighborhood will contain an eight-foot-wide shared use path (Class I bikeway) on the external boundaries of the site, including the north side of West Old Baltimore Road, that connects and continues to the east side to connect with the bike paths for the Summerfield Crossing (8-03034) and Martens (8-02039) developments. The bike path on West Old Baltimore Road connects with the bike path on the east side of Clarksburg Road (MD 121). This path terminates at the future interchange for I-270. Internally, the 8-foot-wide bike path follows Street Z on the west side of the proposed school site, and bifurcates the school and park site to connect with the alignment on Street G. The path follows the west side of Street G and H and the north side of Street P to finally connect to the proposed bike path on Little Seneca Parkway.

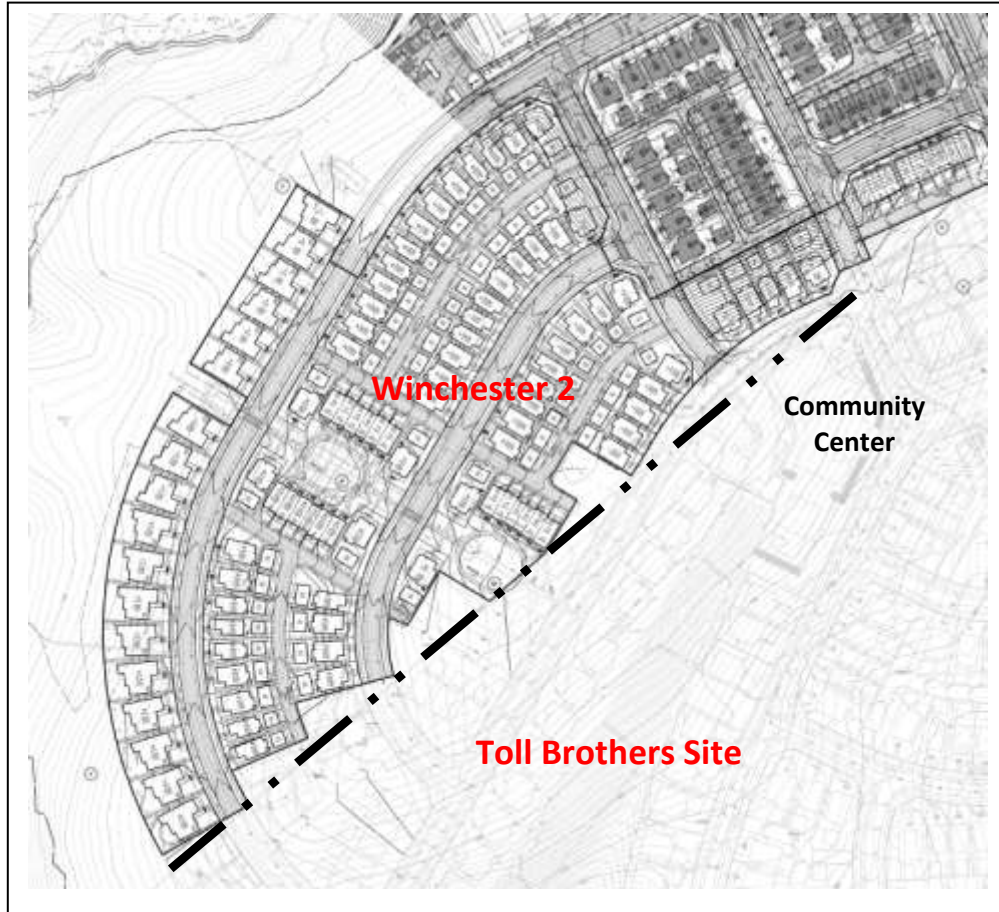
Five-foot-wide internal sidewalks will also be provided for all of the public streets to facilitate pedestrian circulation throughout the development.

The issues surrounding the fragmentary nature of the Winchester 2 site plan area affect the adequate and safe vehicular movement for that portion of the site plan that adjoins or is near the Toll Brothers site. The completion of the Stilt Street, Godwit Street and Stent Street are dependent on the adjacent property owner, which significantly affects the Winchester lots. It is critical that the streets be completed in a timely manner to assure safe, functioning intersections on these local streets. As conditioned by staff, with measures to address this issue, staff finds the vehicular and pedestrian systems to be adequate, safe and efficient.

4. *Each structure and use is compatible with other uses and other site plans and with existing and proposed adjacent development.*

The Winchester 2 plan proposed by this application represents the first multi-phased, sequential residential property development within the Cabin Branch Neighborhood. The fragmented nature of the acreage included in the site plan raises concerns about the future timing and feasibility of completing the larger areas in which these fragments lie. The fulfillment of the development initiated with Winchester 1 relies on the adjacent property owner—requiring dedicated coordination and design to surmount the risks with such conflicts inherent in development crossing property lines. The commitment to the context established is a great one, all the more difficult in a challenging economy. The future Community Center represents the most critical point in this process—the site itself forming the center of the neighborhood constellation – one that knits together east and west, north and south, stream buffer to stream buffer through its visual prominence, functional importance and social service. The realization of a harmonious, aesthetic, and working neighborhood in Winchester 2 site plan will rest almost entirely with adjacent development and the owner’s commitment to such a vision.

Issues outstanding with respect to context and compatibility concern the future resolution of the residential unit mix and providing greater than 50% green space in future phases to meet the requirements of the zoning ordinance.



5. *The Site Plan meets all applicable requirements of Chapter 22A regarding forest conservation, Chapter 19 regarding water resource protection, and any other applicable law.*

As part of the requirements of the Special Protection Area Law, a SPA Water Quality Plan should be reviewed in conjunction with a preliminary plan of subdivision or site plan. Under the provision of the law, the Montgomery County Department of Permitting Services (MCDPS) and the Planning Board have different responsibilities in the review of a water quality plan.

The Planning Board's responsibility is to determine if environmental buffer protection, SPA forest conservation and planting requirements, and site imperviousness limits have been satisfied.

Planning staff has reviewed the elements of the SPA water quality plan and recommends that the Board approve the plan with conditions

Forest Conservation

The entire 540-acre Cabin Branch project has an overall Final Forest Conservation Plan (FCP) which was approved with the Cabin Branch Infrastructure Site Plan (820050150). The intent of having one overall FCP was to ensure that as this multi-year project developed, portions would not be left uncovered by a FCP and the whole project would be in compliance with Chapter 22A the Montgomery County Forest Conservation Law. The original Final FCP was designed to allow for the mass grading and installation of roads, utilities, and public amenities such as school and park sites.

The overall FCP includes six worksheets separated by zone or land use, MXPB-Employment, RMX-1/TDR, MXPB-Residential, Water Tower Storage Facility, Linthicum West, and offsite Area A. The intention was that as each individual site plan with final grading and design came in for review, the overall FCP and the worksheets associated with that site plan would be updated to reflect final designs and grading. The overall FCP indicates that the applicant will meet the forest conservation worksheet requirements through a combination of on-site forest retention, on-site planting of unforested stream buffers, landscape credit, and off-site planting within the Clarksburg SPA.

Under the M-NCPPC implementation of the special protection regulations, the Environmental Guidelines require accelerated reforestation of stream buffers within SPAs. Since the property includes land both in and out of the SPA and the tributaries drain to a common water body, Staff believes it is appropriate to treat the planting requirements as if the entire site is located within the Clarksburg SPA. Therefore, Staff recommends conditions on the forest conservation plan that require the applicant to plant the stream buffers in accordance with the final forest conservation plan planting phasing plan, shown on sheet 63A of that plan, and that the applicant provides a five-year maintenance period for all planting areas credited toward the forest conservation plan.

The Planting Phasing Plan was designed to establish easements and forest planting for the entire Cabin Branch project, while taking into consideration that it will be a multi-year phased development with multiple site plans for individual sections. The Planting Phasing Plan looked at the overall project and each individual site plan in terms of both size (proportion) of the overall project and the percentage of total units for each phase.

Environmental Guidelines

The various approved NRI/FSDs which cover the entire Cabin Branch neighborhood, are included in the Final FCP and were used to identify the environmental buffers. Environmental buffers include wetlands and wetland buffers, floodplains, streams and stream buffers. As part of the Environmental Guidelines, the unforested stream buffer within the SPA must be reforested. The applicant will place forest conservation easements on the environmental buffers and all forest retention areas. The specific Win-2 site plan area does not contain any environmental buffers but is part of the overall Cabin Branch Neighborhood and overall water quality plan. The Win-2 site contains 9.71-acres of forest, no wetlands, no floodplain, and no stream buffers. There are steep slopes (> 25%) on the property and highly erodible soils.

Stream Buffer Encroachments

This application is in conformance with the Environmental Guidelines. There are no environmental buffers or environmental buffer encroachments within the Win-2 site plan area.

Protection of Seeps, Springs and Wetlands

One of the performance goals for the final Water Quality Plan for this site is to protect springs, seeps, and wetlands. It is M-NCPPC’s responsibility to protect these environmental elements through the delineation of stream and wetland buffers. Buffers are recommended over all streams and wetlands identified on the appropriate approved NRI/FSD. There are no environmental buffers within the Win-2 site plan area and this phase is in compliance with the Environmental Guidelines

Site Imperviousness

There are no impervious limitations within the Clarksburg SPA; however, the Special Protection Area regulations allow M-NCPPC to review imperviousness and to work with the applicant to reduce imperviousness. The estimated impervious amount proposed for the entire Cabin Branch project is less than 45 percent. The impervious amount will be refined with the submission and approval of individual site plans. Impervious data is not available for sites of this size and mixture of units and square foot of commercial/office space. However, the table below shows the approximate impervious surface percentages for Win-2 for the overall plan as well as broken down by zone.

Table 1: Winchester Phase II Imperviousness

Winchester Phase 2	Overall FWQP	RMX/TDR-I Area	MXPB Area
	38.2 acres	13.1 acres	25.1 acres
	30.3 acres impervious	11.1 acres impervious	19.2 acres impervious
	79.3 % impervious	84.7 % impervious	76.5 % impervious

County DPS Special Protection Area Review Elements

MCDPS conditionally approved the elements of the final water quality plan under their purview on September 15, 2011.

Site Performance Goals

As part of the final water quality plan, several site performance goals were established:

1. Protect the streams and aquatic habitat.
2. Maintain the natural onsite stream channels.
3. Minimize storm flow run off increases.
4. Identify and protect stream banks prone to erosion and slumping.

5. Minimize increases in ambient water temperatures.
6. Minimize sediment loading.
7. Maintain stream base flows.
8. Protect springs, seeps, and wetlands.
9. Minimize nutrient loadings.
10. Control insecticides, pesticides, and toxic substances.

Stormwater Management

To help meet these performance goals, the stormwater management plan requires water quality control and quantity control to be provided through an extensive system of linked best management practices (BMPs). Six extended detention dry ponds will provide stormwater quantity control. These facilities are designed with a maximum retention time of 12 hours for the one-year storm. Stormwater quality control is provided via a treatment train consisting of recharge structures, surface sand filters in series, structural water quality inlets and vegetated buffer filtering. Since open section roads will not be feasible for the majority of the site additional water quality volume treatment will be provided in surface sand filters. This will be accomplished by sizing these structures to treat the entire drainage area regardless of the treatment being provided for the upland areas. Areas intended for vehicular use will be pretreated prior to entering any water quality filtering structures. Recharge is to be provided below the outlet pipe of all proposed (non-structural) water quality structures where groundwater or bedrock is not encountered. Redundant sediment control facilities are required during construction of the site. This means upland sediment traps will drain to secondary traps downgrade. MCDPS will not allow silt fences as only perimeter control.

COMMUNITY OUTREACH

The Applicant has met all proper signage, noticing, and submission meeting requirements. The Cabin Branch Development Team, with its engineering and design consultants, has held outreach meetings with project presentations as follows:

January 18, 2011:	[Winchester 1, Winchester 2] Community Outreach meeting, for MNCPPC requirement and DPS SPA requirement at Clarksburg
May 1, 2011:	Development Review Committee at MNCPPC [Winchester 2]
June 12, 2011:	Development Review Committee at MNCPPC [Winchester 1]
August 15, 2011:	Clarksburg Civic Association Planning Committee Meeting, [Winchester 1, Winchester 2] with MNCPPC Staff
September 26, 2011:	Clarksburg Civic Association General Meeting [Winchester 1, Winchester 2] with MNCPPC Staff
October 24, 2011:	Clarksburg Civic Association General Meeting [Winchester 1, Winchester 2 Update] with MNCPPC Staff
November 28, 2011	Clarksburg Civic Association Scheduled Update

CONCLUSION

Staff concludes that development of the Winchester I Site Plan, the land use plan, the site and landscape plans, the public facilities and amenities and recreational facilities proposed, are in substantial conformance with the recommendations of the Clarksburg Master Plan, the Development Plan and the Montgomery County Zoning Ordinance.

Attachments: See attachments for part B: Limited Site Plan Amendment No. 82006029A:
Winchester I Phase of the Cabin Branch Neighborhood