



**Cabin Branch Premium Retail and Mixed Use Development**

- MJS* Molline Jackson, Senior Planner Area 3, [molline.jackson@montgomeryplanning.org](mailto:molline.jackson@montgomeryplanning.org), 301.495.4573
- JP* Josh Penn, Senior Planner Area 3, [joshua.penn@montgomeryplanning.org](mailto:joshua.penn@montgomeryplanning.org), 301.495.4546
- RAW* Richard Weaver, Supervisor Area 3, [richard.weaver@montgomeryplanning.org](mailto:richard.weaver@montgomeryplanning.org), 301.495.4544
- JAC* John Carter, Chief Area 3, [john.carter@montgomeryplanning.org](mailto:john.carter@montgomeryplanning.org), 301.495.4575

**Description**

Completed: 11/21/2014

**Cabin Branch Premium Retail and Mixed Use Development**

**\*A. Preliminary Plan Limited Amendment No. 12003110C: Cabin Branch (w/ Preliminary Water Quality).**

Request to revise the adequate public facilities and Preliminary Water Quality Plan, overall property approximately 535 acres, MXP/DMX-1 TDR zone, located in the southwest quadrant of the intersection of I-270 and Clarksburg Road (MD 121) within the Clarksburg Master Plan.

**Staff Recommendation: Approval with Conditions**

**\*B. Site Plan No: 820140160: Cabin Branch Premium Retail and Mixed Use Project (w/ Final Water Quality and Final Forest Conservation Plan).**

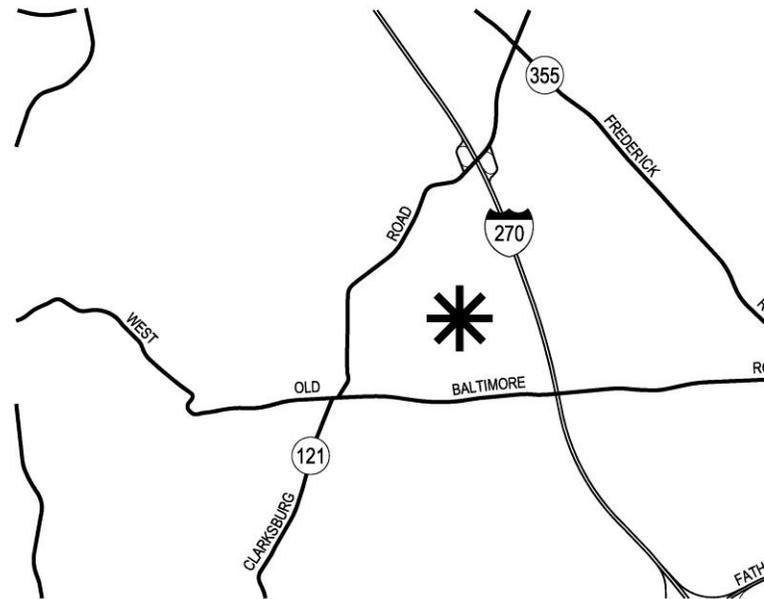
Request to construct phase one of the multi-phased mixed-use development, 450,000 square feet of retail and restaurant uses, parking facilities and public amenities, retail center is located at the intersection of I-270 and Clarksburg Road (MD 121), 84.24 acres, MXP/DMX zone within the Clarksburg Master Plan.

**Staff Recommendation: Approval with Conditions**

**Applicant:** Simon/Clarksburg Development, LLC.

**Submittal Date:** June 30, 2014

**Review Basis:** 59-D-3 Site Plans and Chapter 50



**Summary**

- The Planning Board will take three separate actions. Staff recommends approval of the Preliminary Plan Limited Amendment, the Site Plan (Final Water Quality and Final Forest Conservation Plans) and the WSSC Mandatory Referral for the Water Tower (i.e. Lot 2 on the subject property).
- The Preliminary Plan Limited Amendment requests modifications to the adequate public facilities and water quality only. Other conditions and findings assessed in the previously approved Preliminary Plan No. 12003110B will still apply to this application.
- The Cabin Branch Design Guidelines will be revised to specifically include destination retail. The Applicant has met with the Town Architect and associated Design Review Committee, and per their recommendations Staff supports the proposed changes.
- The reconciliation of a pending forest conservation easement violation (located at 136 West Old Baltimore Road) was also reviewed in conjunction with this project, in order to specifically address an encroachment of the adjacent property owner (i.e. the Kenney property). Upon receiving the Notice of Violation, the property owner has been notified, and the mowing activities have stopped.
- The Applicant proposes to construct Phase I only, which consists of retail/restaurant uses and surface parking facilities. Phase I will serve as a catalyst for future construction of employment and residential uses as approved by the Development Plan.
- Staff has received some community correspondence in favor of this development.

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## **RECOMMENDATIONS & CONDITIONS**

### **Preliminary Plan Limited Amendment No. 120030110C**

The Preliminary Plan Limited Amendment No. 12003110C proposes specific modifications to the adequate public facilities and the Preliminary Water Quality Plan only. In accordance with the previously approved Preliminary Plan No. 12003110B, this application proposed no substantial revisions affecting conformance with the Clarksburg Master Plan and the Subdivision Regulations (Chapter 50) as thoroughly discussed in the approved Resolution (dated October 6, 2008). Therefore; Staff Recommends approval with the following conditions:

- 1) Approval is limited to a maximum of 450,000 gross square feet of retail and restaurant uses, as part of the overall approvals for the MXPDP area.
- 2) The Planning Board accepts the recommendations of the Montgomery County Department of Permitting Service (“MCDPS”) – Water Resources Section in its stormwater management concept letter dated, August 25, 2014 and hereby incorporates them as conditions of the Preliminary Plan approval. The Applicant must comply with each of the recommendations as set forth in the letter, which may be amended by MCDPS – Water Resources Section provided that the amendments do not conflict with other conditions of the Preliminary Plan approval.
- 3) The Applicant must construct the pavement labeled “Premium Outlets Drive” from Clarksburg Road (MD 121) to the southern end of the Subject Property in accordance with the MCDOT Road Code Standard MC-219.01: Commercial/Industrial Dual Road Modified to provide vehicular access to buildings and the amphitheater.
- 4) The Applicant must construct the internal private drive loop road along the northern side of the retail buildings to the MCDOT Road Code Standard 214.02: Commercial/Industrial Road Modified; the 30-foot wide drive aisle shall not have travel lanes that exceed 12-feet in width.
- 5) The Applicant must construct the east-west private drive that connects Cabin Branch Avenue to “Premium Outlet Drive” in accordance with the MCDOT Road Code Standard MC-219.01: Commercial/Industrial Dual Road Modified.
- 6) The Planning Board accepts the recommendations of the Montgomery County Fire and Rescue Services (MCFRS) Fire Code Enforcement Section in its letter dated September 22, 2014, and hereby incorporates them as conditions of approval. The Applicant must comply with each of the recommendations as set forth in the letter, which MCFRS may amend if the amendments do not conflict with other conditions of Site Plan approval.

## Site Plan No. 820140160

Staff recommends approval of Site Plan 820140160, Clarksburg Premium Outlets at Cabin Branch, for the construction of Phase I including 450,000 square feet of retail and restaurant uses, parking facilities and public amenities on 84.24 gross acres. All site development elements shown on the resubmitted plans "Received" by the M-NCPPC on October 1, 2014 are required except as modified by the following conditions.

### Conformance with Previous Approvals

1. Development Plan Conformance

The development must comply with all applicable binding elements, general notes and the development program as shown on the Certified Land Use of the Development Plan (DPA 13-02) approved February 4, 2014.

2. Preliminary Plan Conformance

The development must comply with the conditions of approval for Preliminary Plan Nos. 12003110B (approved October 6, 2008) and 12003110C (as listed in the Planning Board Resolutions), unless amended.

3. Infrastructure and Road Only Site Plan

Applicant must comply with the conditions of approval as set forth in the following applications:

- a. Site Plan No. 820050150, MCPB Resolution No. 07-131,
- b. Site Plan No. 82005015A, MCPB Resolution No. 08-68,
- c. Site Plan No. 82005015B, MCPB Resolution No. 11-124,
- d. Site Plan No. 82005015C (approved November 27, 2012),
- e. Site Plan No. 82005015D, (approved May 2, 2013),
- f. Site Plan No. 82005015E, MCPB 14-36, and
- g. Site Plan No. 82005015F, MCPB 14-79.

4. Incorporation of the Water Storage Tank

Upon acceptance of M-NCPPC Staff recommendations for the associated Mandatory Referral No. 08001-WSSC-1A, the Applicant will meet their on-site obligations as discussed in Washington Suburban Sanitary Commission (WSSC) Amended Phase I Letter (dated July 31, 2014). As described in the letter, the Applicant must comply with the conditions and service requirements prior to the issuance of the use and occupancy permit of the commercial shopping center.

5. Site Plan Amendments

- a. Minor modifications to public plazas, landscaping, lighting, recreation building footprint, site elements, structures (not considered buildings) and paving materials may be approved by M-NCPPC Staff and documented with M-NCPPC

- and MCDPS Site Plan Enforcement.
- b. Other modifications to the Site Plan must follow the procedures outlined in Development Manual.

## **Environment Planning & Water Quality**

### Forest Conservation Plan

6. The agreement, for the release of 1,261 square feet of conservation easement in planting area K, stage I (FFCP Sheet 58), and the new Category I conservation easement increasing planting area K by 1,261 square feet (FFCP Sheet 46) must be recorded by deed in the land records within ninety (90) days of the mailing of the Planning Board Resolution.
7. The record plat must reflect a Category I conservation easement over all areas of stream buffers, forest save, and forest planting.
8. All off-site reforestation must occur within the Clarksburg Special Protection Area unless otherwise specifically approved by the Planning Board.
9. The implementation of the project's forest conservation mitigation requirements, which includes forest retention, onsite and offsite afforestation, reforestation and landscape credit, may be phased. The phasing of the total forest conservation mitigation must be proportionate to either the area proposed for disturbance relative to the total project size or the amount of density for each stage relative to the total project density, whichever is greater. The phasing triggers, forest conservation mitigation type, amount of mitigation, and disturbance areas are to be defined on the Final Forest Conservation Plan and revised as needed with subsequent Site Plans. Conservation easements may be recorded using a metes and bounds description and sketch if preceding recordation of plats, provided the recording information for the conservation easement is referenced and the easement line is shown on subsequent record plats.
10. The development must comply with the conditions of the approved Final Forest Conservation Plan.

### 11. Final Water Quality Plan

The Applicant must conform to the conditions as stated in the Montgomery County Department of Permitting Service (MCDPS) Final Water Quality Plan approval letter dated September 30, 2014 unless otherwise amended.

### 12. Stormwater Management

The Planning Board has accepted the recommendations of the Montgomery County Department of Permitting Service ("MCDPS") – Water Resources Section in its stormwater management concept letter dated September 30, 2014, and hereby incorporates them as conditions of the Site Plan approval. Therefore, the Applicant must comply with each of the recommendations as set forth in the letter, which may be amended by MCDPS – Water Resources Section provided the amendments do not conflict with other conditions of the Site Plan approval.

## Transportation Planning & Circulation

### 13. Transportation

- a. The total future development on this Site Plan is limited to 450,000 gross square feet of retail and restaurant uses and public facilities within the MXPD Zone.
- b. The private drive aisles will be reassessed to determine their adequacy should additional or residential development be proposed with future applications.
- c. The Certified Site Plan must show the internal circulation as private drive aisles.
  - i. The Applicant must construct a private drive aisle (labeled as Premium Outlets Drive) from their main entrance at Clarksburg Road (MD 121) to the southern end of their site that terminates at the proposed amphitheater to applicable Montgomery County MCDOT Road Code Standard MC-219.01: Commercial/Industrial Dual Road Modified. The typical curb to curb dimensions will consist of travel lanes that will be 12-foot wide varying between 1 and 2 lanes in each direction and the median width will vary from 4-30 feet as indicated on the Site Plan. A shared use path will be accommodated within the 80 foot typical section as indicated on the Site Plan from the private drive aisle connecting to Cabin Branch Avenue heading south towards the proposed amphitheater.
  - ii. The Applicant must construct a private drive aisle that traverses the back of the Subject Property and connects at Premium Outlets Drive to applicable Montgomery County MCDOT Road Code Standard 214.02: Commercial/Industrial Road Modified and the 30-foot wide drive aisle shall not have travel lanes that exceed 12-feet in width. Additionally, the Applicant must construct all sidewalks, on the Subject Property within the Site Plan, to applicable ADA standards. The striping plan of the drive aisle will be reviewed prior to Certified Site Plan.
  - iii. The Applicant must construct an east-west drive aisle extension from Cabin Branch Avenue to the Premium Outlets Drive (a private drive aisle) for secondary site access to applicable Montgomery County MCDOT Road Code Standard MC-219.0: Commercial/Industrial Dual Road Modified. The typical curb to curb dimensions will consist of two travel lanes that will be 12-foot wide in each direction and the median width will vary from 2-12 feet as indicated on the Site Plan. A shared use path will be accommodated within the 80 foot typical section as indicated on the Site Plan from Cabin Branch Avenue to the private drive aisle that connects with the proposed amphitheater
  - iv. Before the release of Site Plan Surety, the Applicant must provide MCDPS Zoning & Site Plan Enforcement (Z&SPE) Staff with certification from a licensed engineer that all internal drive aisles and associated sidewalks and shared use paths have been built to the above structure standards and ADA standards.

- d. The Applicant must install twelve (12) pockets of short term public bicycle parking racks, a minimum of eight (8) (“inverted U” racks or similar) and up to four (4) public art oriented bicycle racks, as indicated on the Site Plan.
- e. The Applicant must install four (4) bike shower facilities as indicated on the Site Plan.
- f. The Applicant must install two (2) long term bicycle lockers each with 30 long term, bicycle parking spaces (60 total long-term bicycle parking spaces).

14. Pedestrian Circulation

- a. The Applicant will construct an eight-foot shared-use path on the southern/western side of a private drive aisle (Premium Outlets Drive) from the intersection that provides access to Cabin Branch Avenue to the proposed amphitheater at the southern end of the site.
- b. The Applicant must provide a bicycle connection from the site’s main entrance at Clarksburg Road (MD 121) to the shared path that is being constructed on the south/western side of a private drive aisle (Premium Outlets Drive). A bicycle facility (bicycle lanes) along the private drive aisle that traverses the back of the Outlets is proposed, unless the Applicant can demonstrate an alternative way of accommodating a bicycle connection with the purpose of connecting the shared use path that terminates at the main entrance to the site off of Clarksburg Road to the shared use path on Premium Outlets Drive. The bicycle connection will be reviewed prior to Certified Site Plan.

15. Fire and Rescue

The Planning Board accepts the recommendations of the Montgomery County Fire and Rescue Services (MCFRS) Fire Code Enforcement Section in its letter dated September 22, 2014, and hereby incorporates them as conditions of approval. The Applicant must comply with each of the recommendations as set forth in the letter, which MCFRS may amend if the amendments do not conflict with other conditions of Site Plan approval.

**Miscellaneous**

16. Public facilities and utilities

The Applicant will construct the public facilities and utilities prior to the issuance of the final use and occupancy permit. The development program will include the following facilities:

- a. 24-inch water line that connects into the water storage tank,
- b. 8-foot hike/biker trail along the southern property edge,
- c. Public plaza areas (e.g. north, east and west plazas),
- d. Amphitheater,
- e. Bicycle facilities along the 8-foot hiker/biker trails and within the shopping area, and
- f. Electric charging stations will be provided within the parking facilities, as noted on the Site Plan.

17. Common Open Space Covenant

The record plat must reference the Common Open Space Covenant recorded at Liber 28045 Folio 578 (Covenant).

18. Maintenance of Public Amenities

The Applicant is responsible for maintaining all publicly accessible amenities including, but not limited to the hiker/biker pathways, amphitheater, public plaza areas, electric car charging stations, water features, tot lot, public art and the access to the water storage tank.

**Site Plan**

19. Site Design

- a. The exterior architectural character, proportion, materials, and articulation must be substantially consistent with the schematic elevations as shown on Sheets A-1 to A-43 of the submitted architectural drawings, as determined by M-NCPPC Staff.
- b. Permanent signage must have a permit issued by the MCDPS; which may also approve a variance from any provision from Section 59F-1.3 except Division 59F-7 "Prohibited Signs".
- c. Provide a Signage Package that clearly identifies any placement of signage on the building facades, walls and free-standing signs (i.e. way-finding package).
- d. The Way-Finding Conceptual Plan may include, but is not limited to internal and external signage, banners, lighting and design elements that efficiently direct patrons through the Property. The Way-Finding Concept Plan will be included in the Certified Site Plan and must be substantially consistent with the signage package to be approved by MCDPS.
- e. Locate and include traffic signage and stop bars on the Signage Way-finding and Site Plans.
- f. Provide alternative design solutions for the painted concrete screen walls near the loading dock areas. The screen walls, fronting the surface parking facilities, must be treated as an extension of the building façades.

20. Landscaping

- a. The surface parking facilities that are directly adjacent to the public roadways (e.g. Clarksburg Road), private drive aisles or driveway will be adequately screened from the major views of vehicular traffic flow with a densely planted hedgerow and a decorative screen wall.
- b. Planted buffers along the parking edges should be at least 10 feet wide.
- c. Shade trees along the 8-foot hiker/biker trails must be professionally pruned ("limbed up") so as not to obstruct the views of vehicles, bikers and pedestrians.

## 21. Lighting

- a. Before issuance of the any building permit, the Applicant must provide certification to M-NCPPC Staff from a qualified professional that the lighting plans conform to the Illuminating Engineering Society of North America (IESNA) standards for commercial/retail developments.
- b. All onsite down-light fixtures must be full cut-off fixtures.
- c. Deflectors will be installed on all fixtures causing potential glare or excess illumination, specifically on the perimeter fixtures abutting the adjacent residential properties.
- d. Illumination levels must not exceed 0.5 footcandles (fc) at any property line abutting county roads.
- e. The height of the light poles must not exceed 30 feet including the mounting base.

## 22. Design Guidelines

- a. The Applicant must comply with the revised Cabin Branch Guidelines for the Site Plan Review (“Design Guidelines”), as amended for Destination Retail specifically associated with but not limited to the site design, architectural design, landscape design, streetscape, sidewalks and pathways, internal crosswalks, street trees, lighting and parking.
- b. The Development Program must adequately address phasing of the design features to be approved by the Cabin Branch Design Committee prior to the certification of the Site Plan. Any significant revisions to the Development Program will be submitted with the building permit and reviewed by M-NCPPC Staff prior to the issuance of the building permit.
- c. The Applicant will submit an approval letter from the Cabin Branch Design Review Committee stating that the buildings and associated design features within their respective phases are in conformance with the Certified Site Plan and the Design Guidelines.
- d. The Applicant must submit to the Planning Director (or designee) architectural plan construction documents for each individual building permit application that demonstrates conformance with the Design Guidelines and the Development Program.
- e. Submissions will be deemed approved after 10-days, unless Applicant is notified that the submission does not comply.

## 23. Site Plan Surety and Maintenance Agreement

Before issuance of the any building permit, the Applicant must enter into a Site Plan Surety and Maintenance Agreement with the Planning Board. The Agreement must include a performance bond(s) or other form of surety as required by Section 59-D-3.5(d) of the Montgomery County Zoning Ordinance with the following provisions:

- a. A cost estimate of the materials and facilities, which, upon Staff approval, will establish the surety amount.

- b. The cost estimate must include applicable Site Plan elements, including, but not limited to plant material, on-site lighting, recreational facilities, water features, site furniture, artwork, trash enclosures, retaining walls, fences, railings, private aisles/drives, specialty pavement patterns, pathways and associated improvements within the relevant phase of development. The surety must be posted before issuance of any building permit within each relevant phase of development and will be tied to the development program.
- c. The bond or surety must be tied to the development program, and completion of all improvements covered by the surety for each phase of development will be followed by inspection and potential reduction of the surety.
- d. The bond or surety for each phase shall be clearly described within the Site Plan Surety & Maintenance Agreement including all relevant conditions and specific Certified Site Plan sheets depicting the limits of each phase.

#### 24. Development Program

The Applicant must construct the development in accordance with a development program that will be reviewed and approved prior to the approval of the Certified Site Plan. The development program must include the following items in its phasing schedule:

- a. Lighting and sidewalks must be installed within six months after construction of the drive aisles. Plantings within the parking facilities may wait until the next growing season.
- a. On-site amenities including, but not limited to, sidewalks, benches, trash receptacles, and bicycle facilities must be installed prior to release of any building occupancy permit.
- b. Public facilities including but not limited to the plaza area, bike facilities, electric car charging stations, tot lot, amphitheater and the 8-foot hiker/biker trail will be included in the development program and constructed prior to the issuance of the final use and occupancy permits.
- c. Clearing and grading must correspond to the construction phasing to minimize soil erosion and must not occur prior to approval of the Final Forest Conservation Plan, Sediment Control Plan, and M-NCPPC inspection and approval of all tree-save areas and protection devices.
- d. The development program must provide phasing for installation of on-site landscaping and lighting.
- e. Landscaping associated with each parking lot and building must be completed as construction of each facility is completed.
- f. Pedestrian pathways and seating areas associated with each facility must be completed as construction of each facility is completed.
- g. Provide each section of the development with necessary roads.
- h. The development program must provide phasing of dedications, stormwater management, sediment and erosion control, afforestation, trip mitigation, and other features.

## 25. Certified Site Plan

Before approval of the Certified Site Plan the following revisions must be made and information provided subject to Staff review and approval:

- a) Include the final forest conservation approval letter, stormwater management concept approval letter, development program, Fire & Rescue approval letter and Site Plan resolution on the approval or cover sheet(s).
- b) Add a note to the Site Plan stating that "M-NCPPC Staff must inspect all tree-save areas and protection devices before clearing and grading."
- c) Modify data table to reflect development standards approved by the Planning Board.
- d) Ensure consistency of all details and layout between Site and Landscape Plans.

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## CONTEXT AND PREVIOUS APPROVALS

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### Vicinity

In accordance with the 1994 Clarksburg Master Plan, the Overall Cabin Branch Neighborhood is approximately 950 acres. The Cabin Branch Development is 535 acres with a mix of uses within the Cabin Branch Neighborhood. The Cabin Branch Development is bordered by Clarksburg Road (MD 121) to the north and west, I-270 to the east and Old Baltimore Road to the south. The property is split zoned MXPB/RMX-1 TDR. Adjacent uses include: existing one-family detached homes, COMSAT and Gateway 270 Business Center (Site Plan No. 820000170), the Gosnell Property (Site Plan No. 80060240), the Winchester Development Phases I and II (Site Plan Nos. 820060290 and 820110080), the Ten Mile Creek property, agricultural farms and the proposed Washington Suburban Sanitary Commission (WSSC) water storage tank. In accordance with the Mandatory Referral (MR08001-WSSC 1A), the water storage tank will be constructed near the Maryland State highway Administration (SHA) salt storage facility on the Subject Property.



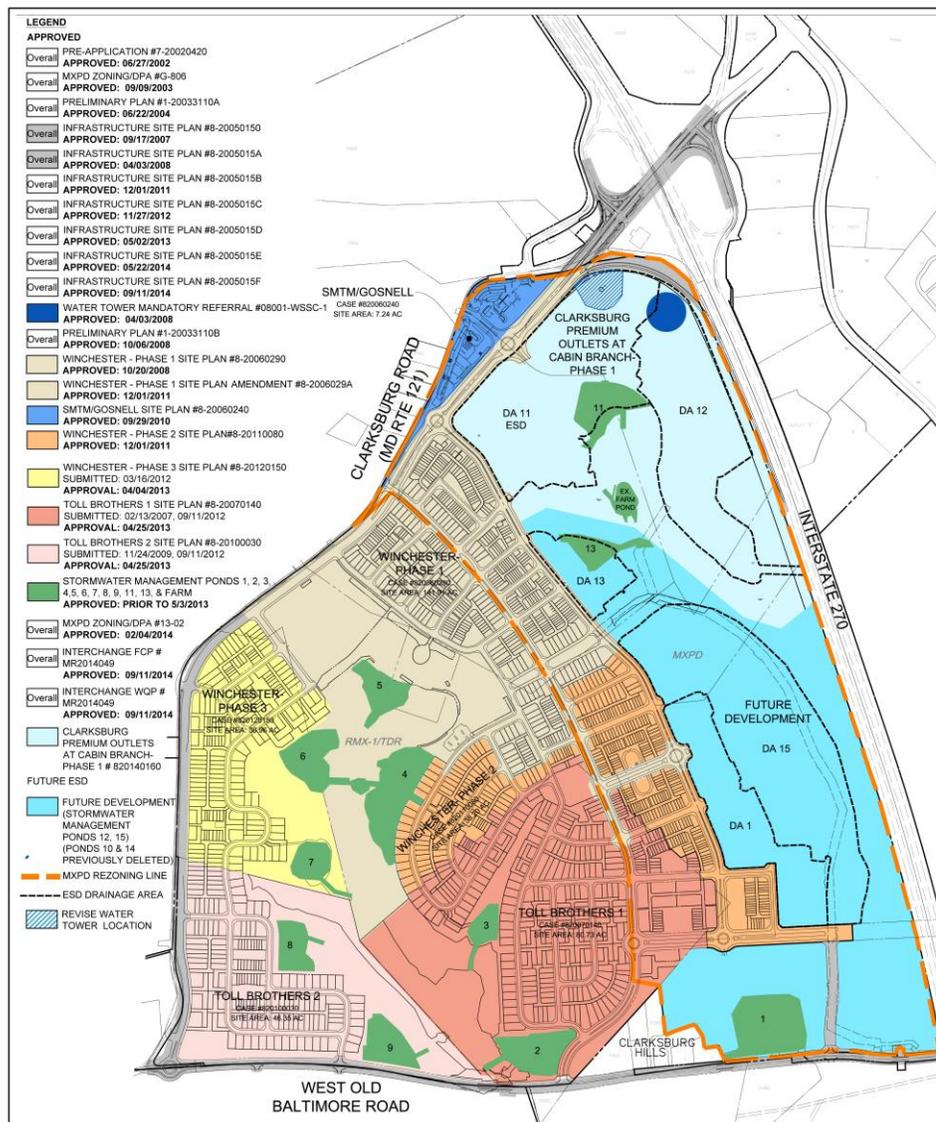
*Aerial Photo*

The Subject Property is a smaller portion (approximately 84.24 acres, highlighted in yellow above) of the Cabin Branch Development, specifically located in the upper northeast corner of the neighborhood and zoned MXPB (“Property” or “Subject Property”).

## Existing Conditions

The Subject Property is comprised of portions of three different unplatted parcels (Parcel 187, 645 and 345, tax maps EV 23 and EV 33), and is served by public water and sewer. There is an approximate 100 foot grade drop from MD 121 to the low point within the stream buffer area. The Property has a perennial stream, 2.69-acres of wetlands and 6.54-acres of floodplain. The stream runs parallel to I-270 and within the boundary of the Subject Property. The existing rolling terrain, forested buffers and stream valley create natural terraced views from the north corner of the Property (high point) out toward the southern boundary.

## Previous Approvals



Approvals Diagram

In accordance with the 1994 Clarksburg Master Plan, on September 9, 2003 the District Council approved a Local Map Amendment (LMA) number G-806 to reclassify the eastern half of the

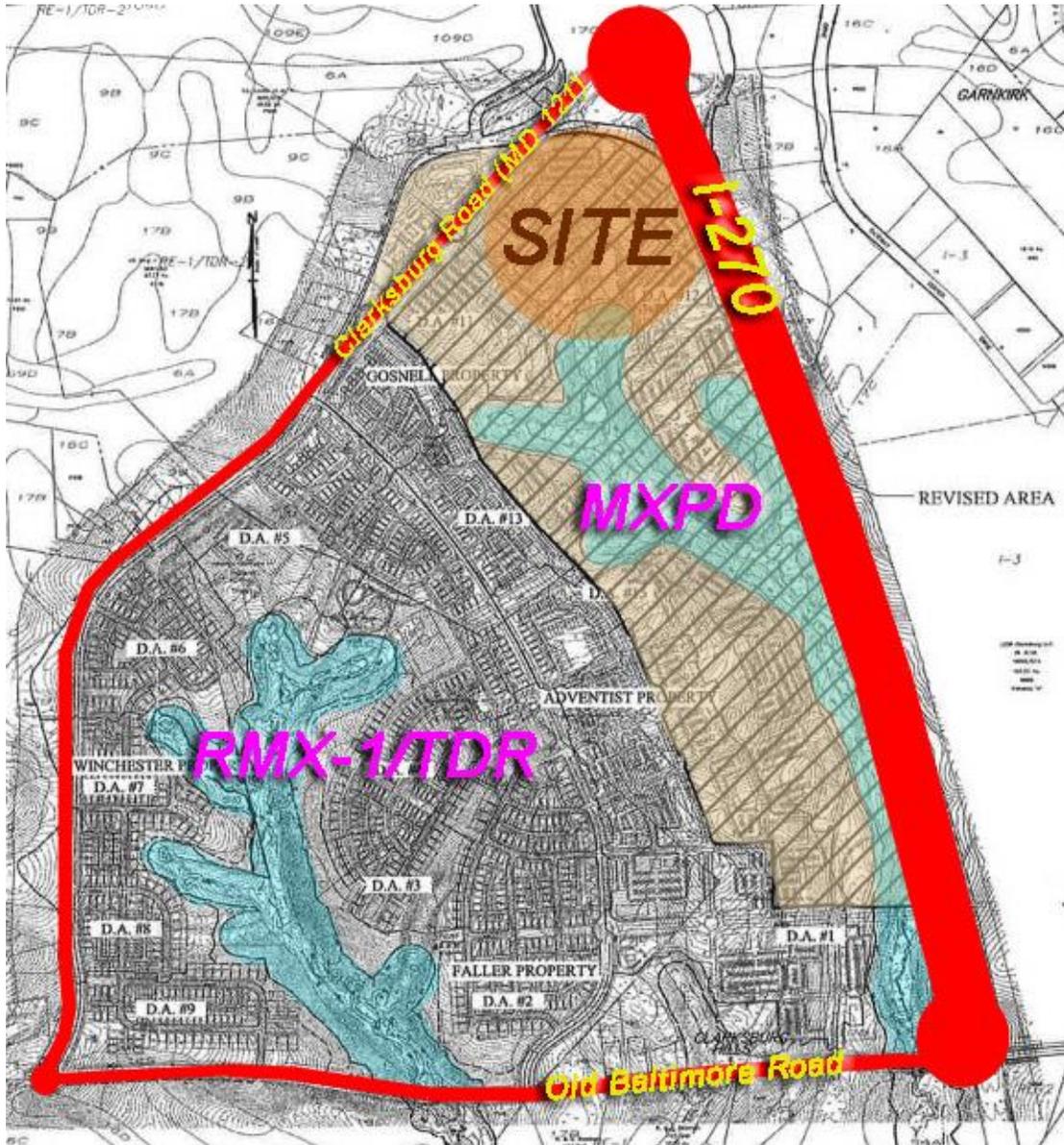
Cabin Branch Neighborhood to the MXPDP zone, while leaving the remaining western half in the RMX-1/TDR zone. Since the approval of the original Development Plan (G-806) in 2003 and the Preliminary Plan 12003110B, several Site Plan approvals have been obtained and properties owners have entered into contractual agreements in order to develop the entire Cabin Branch Development (*see Appendix A*).

As indicated in the Approved Development Plan (DPA 13-02), the Subject Property falls within the North District - Area A. Areas A, B and C are planned for retail, office and residential uses. The dashed orange boundary separates the MXPDP zones from the RMX-1/TDR zones. The DPA 13-02 approved up to 484,000 square feet (sf.) of retail, 1,936,000 sf. for office use, 1,139 dwelling units, 500 senior dwelling units and 75,000 sf. public uses within the MXPDP zone.

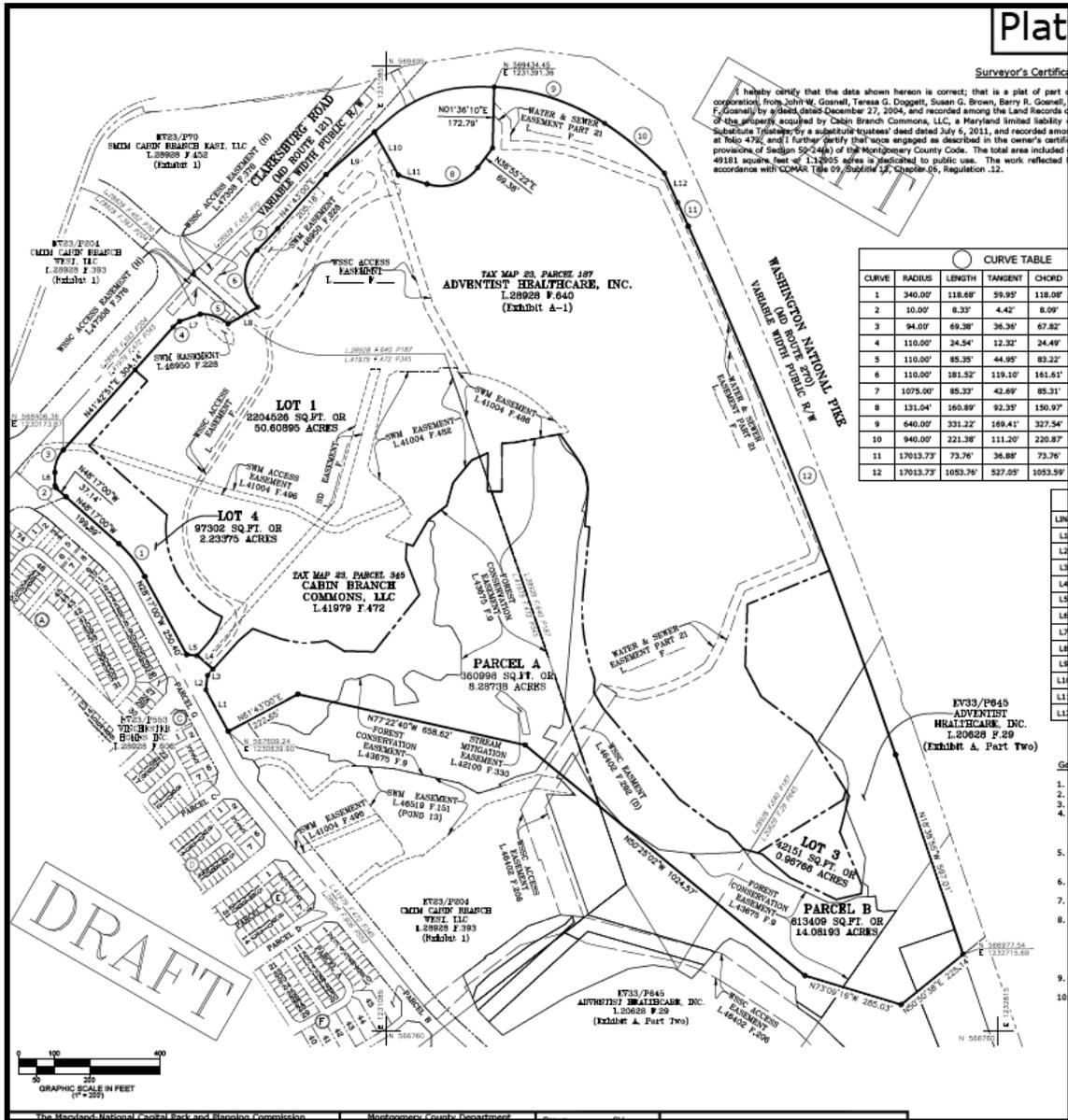
The Planning Board also approved a Preliminary Water Quality Plan (PWQP) and a Preliminary Plan (No. 12003110B) with an adequate public facilities finding valid for up to 12 years. The County Council has since granted extensions of the adequate public facilities that applied to the majority pending projects in the County including the Cabin Branch Development. The validity period for the associated Preliminary Plan does not expire until November 6, 2024.

An Infrastructure Site Plan and Final Water Quality Plan No. 820050150 were approved for the Cabin Branch Development. The purpose of the Infrastructure Plan was to address road phasing, a school, local parks, stormwater management facilities and environmentally sensitive areas. The original Infrastructure Site Plan has since been amended numerous times, and most recently on September 11, 2014 to address changes to the Final Forest Conservation Plan to accommodate the I-270/ MD 121 Interchange improvements.

PRELIMINARY PLAN No. 12003110C: Cabin Branch Development



Cabin Branch Development: Existing Stream Valley and Roads

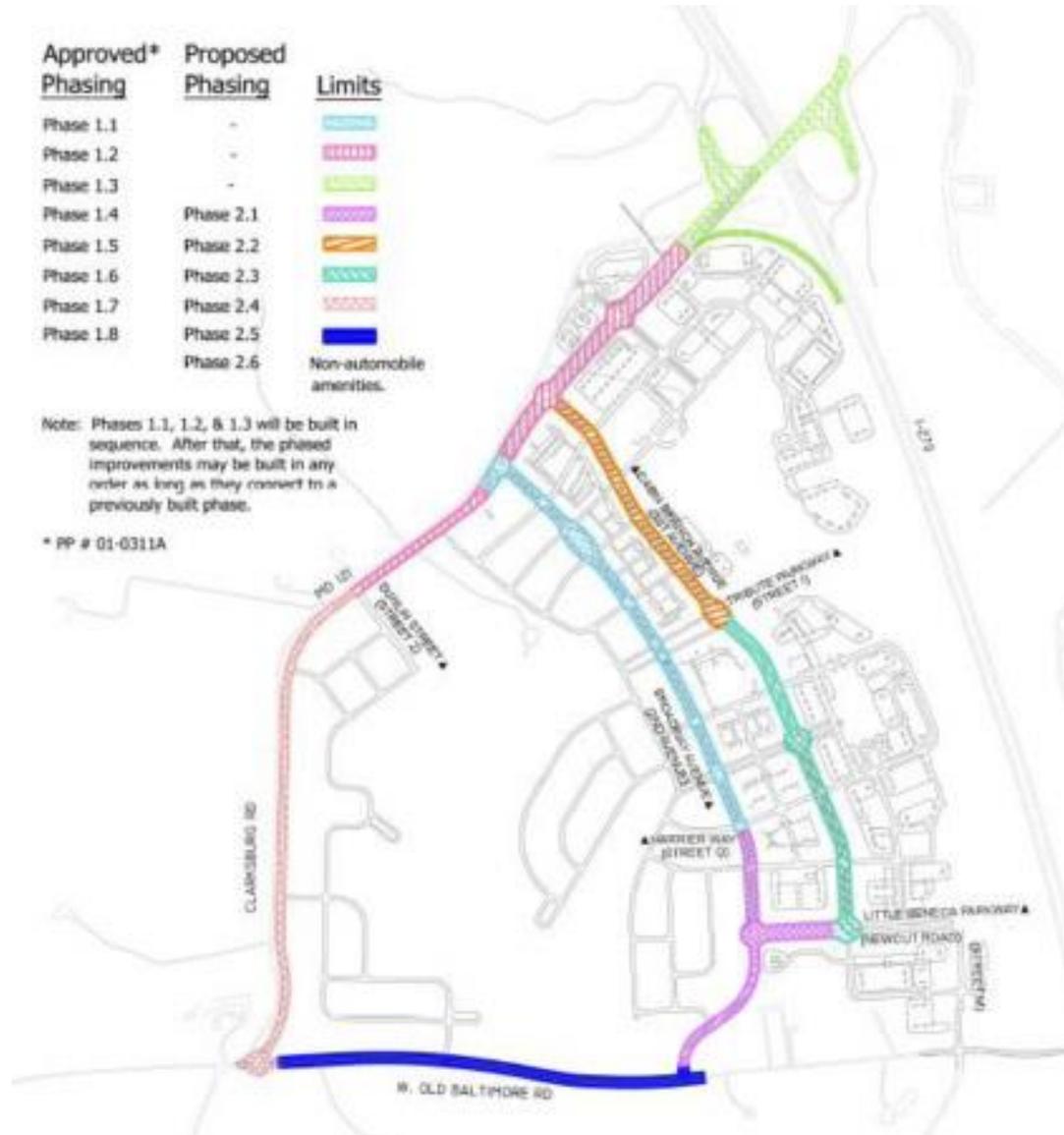


Cabin Branch Development: Proposed Lot Configuration

**ANALYSIS AND FINDINGS: PRELIMINARY PLAN**

The main access to the Property will be located at the first intersection immediately west of the interchange with I-270 on Clarksburg Road (MD 121) via a roundabout. A second vehicular access to the Property will be provided from Cabin Branch Avenue, which intersects Clarksburg Road via a second roundabout. The first roundabout (closest to the I-270/MD 121 interchange) connects directly to a proposed private drive aisle within the Property, currently labeled as "Premium Outlets Drive". Premium Outlets Drive is an internal drive that loops around the development. The second (western) roundabout will connect directly to Cabin Branch Avenue (formerly 2<sup>nd</sup> Avenue as indicated in the Infrastructure Site Plan No. 820050150), which will be a public road. From Cabin Branch Avenue, there is a full movement intersection that provides

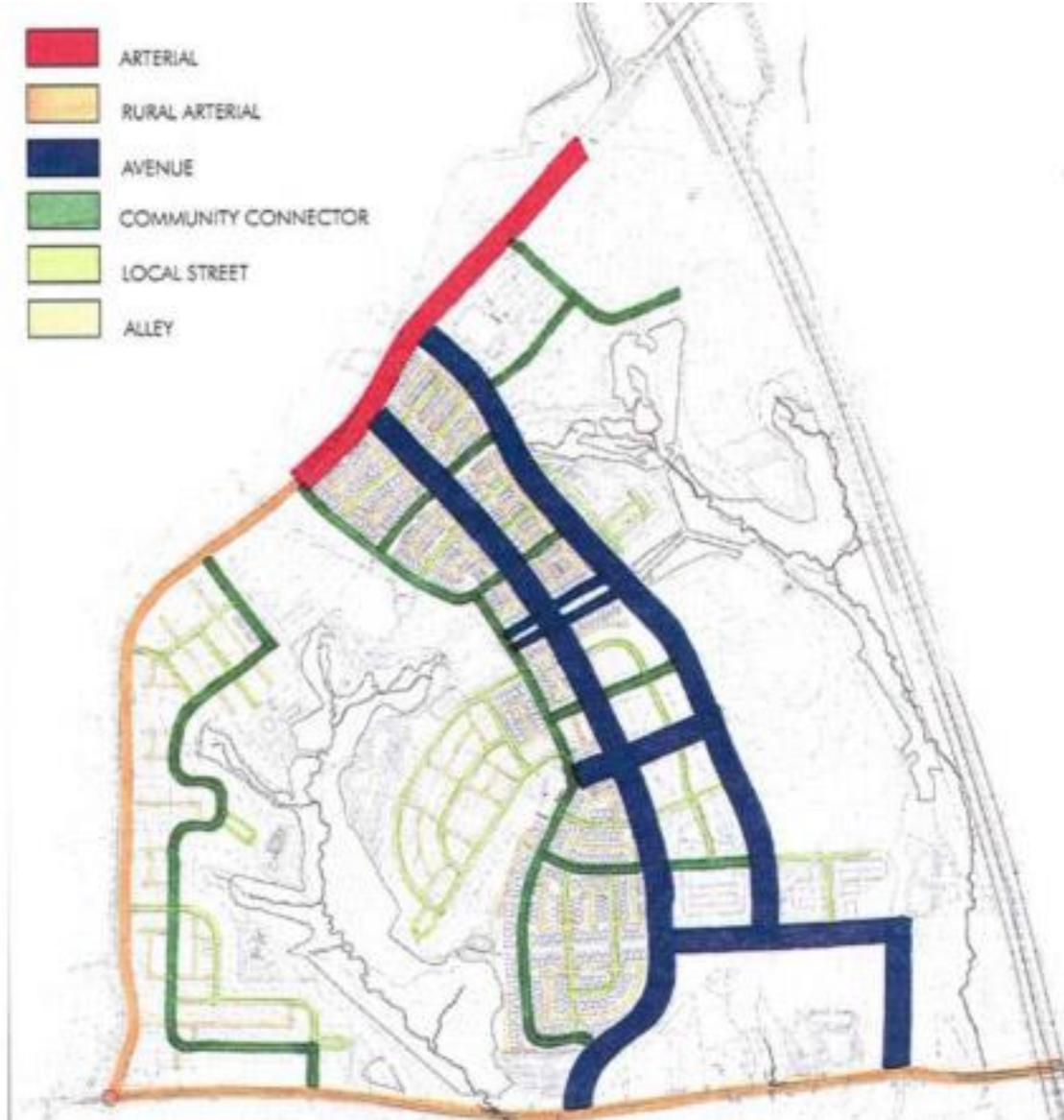
access to the Property and connects into Premium Outlets Drive. Since Premium Outlets Drive is not classified as a public or private road, postal addresses for any buildings shown on the Preliminary and Site Plans will have suite/unit numbers assigned from Clarksburg Road or Cabin Branch Avenue.



*Cabin Branch Development: Approved Roadway Improvements*

Cabin Branch Development, LLC, the developer of Cabin Branch, is responsible for constructing the road improvements as laid out in Preliminary Plan No. 12003110A. As indicated in the approved Preliminary Plan, there are three road improvement phases (1.1, 1.2 and 1.3) and associated development that have to be built first and in sequence. After these improvements are constructed, then the phased improvements and associated development may be built in any order as long as they connect to a previously built phases. In order, the following improvements have to be built prior to the construction of this development:

- 1) Broadway Avenue (formerly 2<sup>nd</sup> Avenue) as a two-lane road,
- 2) Clarksburg Road (partially relocated) transitioning from 2-lanes to 6-lanes from Broadway Avenue to the I-270/MD-121 interchange, and
- 3) Reconstruction of the I-270/MD-121 interchange to carry 6-lanes from the Applicant's main entrance to the on/off ramps located on the east side of the interchange.



*Cabin Branch Development: Approved Roadway Classifications*

Currently, the northern half of Broadway Avenue has been built. The second half from Marrier Way (formerly Street Q) will be constructed with the approved Toll I Site Plan No. 820070140. The northern half of Clarksburg Road from Broadway Avenue to the interchange is waiting on final approval by the State Highway Administration (SHA). The realignment of Clarksburg Road

has been roughly graded, and construction of the roadway and the interchange improvements will begin in 2015. The interchange improvement was presented to the Planning Board for Mandatory Referral (No. 2014049) on September 11, 2011. The northern half of Cabin Branch Avenue from Tribute Parkway (formerly Street I) to Clarksburg Road (not part of the phasing sequence order) is currently under construction and expected to be completed in 2015.

As part of the roadway improvements to Clarksburg Road, dual bikeway facilities are being provided on the road from east of the interchange to Broadway Avenue. The dual bikeway designation is a shared use path and shared roadway designation. The shared use path is on the south side of Clarksburg Road west of the interchange. The Cabin Branch Development, LLC is constructing an extension of the shared use path on the west side of Cabin Branch Avenue from Clarksburg Road to Tribute Parkway (formerly Street I). The Applicant is accommodating an extension of the shared use path on their Property.

This Limited Preliminary Plan Amendment reviewed the adequate public facilities and the Water Quality Plan for the overall Cabin Branch Development (i.e. the Preliminary Water Quality Plan). The 1994 Clarksburg Master Plan identifies the Cabin Branch Neighborhood as the only area west of I-270 to be significantly developed with residential and commercial uses. As a result, a portion of the Cabin Branch Development (283.5 acres) was rezoned from the I-3 Zone to MXPD Zone. Both the Preliminary Plan and the Master Plan allow up to 2,420,000 square feet of non-residential uses within the MXPD Zone.

#### Master Plan Transportation Facilities

The Preliminary Plan Amendment continues to incorporate the Master Plan principles of being a mixed-use transit-oriented and pedestrian-friendly neighborhood. The development provides a street system that connects into existing uses and emphasizes multimodal access to open spaces. The development substantially conforms to the approved Development Plan Amendment (DPA-13-02), the Local Map Amendment (G-806) and the objectives listed below:

- Provide a mix of uses including employment,
- Encourage an employment pattern that is supportive of I-270 as a high-technology corridor,
- Create a transit-oriented land use pattern,
- Maximize access to the proposed open spaces areas,
- Provide a suitable transition to the rural/open space character south of West Old Baltimore,
- Provide an interconnected roadway system,
- Create a strong neighborhood focal point by concentrating public and retail uses in the same general area, and
- Place special emphasis on protection of the west fork of Cabin Branch.

#### Adequate Public Facilities Review

A transportation analysis was submitted by the Applicant using the Staff recommended trip

generation rates for the approved and proposed uses. Additionally, documentation was submitted that showed trip counts for outlet centers similar to the use being proposed with this application. Based on this documentation it was determined that a full revised traffic study did not need be submitted as the estimated vehicle trip generation for the proposed use is below that which was approved use for the Property by the previously approved Preliminary Plan.

Trip Generation

The Applicant’s Site Plan application proposes to increase the overall amount of retail from an approved amount of 120,000 SF to 484,000 SF while simultaneously reducing the amount of office space from 2.3 million SF to 1.936 million SF in order to maintain the same overall approved square footage of 2.42 million SF. The table below shows a comparison of the vehicular trips generated by both the approved development program with the Applicant’s current proposal. More detailed trip generation estimation tables (depicting the specific square footages for each use and trip reductions) are referenced in the Appendix E.

*Trip Generation Table – Proposed vs Approved*

Development Program	AM Peak Hour			PM Peak Hour		
	In	Out	Total	In	Out	Total
Previously Approved	2,451	1,220	3,671	1,386	2,577	3,963
Currently Proposed	2,148	1,060	3,208	1,241	2,210	3,451
Net Difference	-303	-160	-463	-145	-367	-512

As shown in the Trip Generation Table above, approval of the Applicant’s Limited Preliminary Plan Amendment would result in a net reduction of traffic during each of the AM and PM peak hours, as compared to the development program in the previously approved Preliminary Plan (No. 12003110B). The traffic forecasts for the overall Cabin Branch Development are referenced in Appendix E: Additional Project Data.

**Local Area Transportation Review**

Based on the traffic generated by the proposed 450,000 SF Outlet Center as well as the approved development on the Gosnell Property (Site Plan No. 820060240) north of MD 121, the Applicant analyzed critical lane volumes (CLV) for nearby intersections. CLVs are shown in the CLV Analysis Table below. The analysis indicates that all four intersections will operate better than the policy area CLV standard of 1,425.

*CLV Analysis Table – 450,000 SF Outlet Center*

Intersection	AM Peak Hour	PM Peak Hour
Clarksburg Rd (MD 121) / I-270 NB Off-Ramp	762	1,118
Clarksburg Rd (MD 121) / I-270 SB Off-Ramp	610	883

Clarksburg Rd (MD 121) / Goldeneye Ave (Whelen Rd)	614	770
Clarksburg Rd (MD 121) / Cabin Branch Ave	483	669

**Transportation Policy Area Review (TPAR)**

The previous Preliminary Plan approval gives the Applicant TPAR credit for 2.42 million SF of development (referred to at the time of that approval as Policy Area Transportation Review – PATR). This Site Plan will not result in an increase of overall building square footage or vehicular traffic (as show in the trip generation table); therefore no additional TPAR/PATR payment is required.

Compliance with the Subdivision Regulations and Zoning Ordinance

This application meets all applicable sections of Chapter 50, the Subdivision Regulations. The lot size, width, shape and orientation are appropriate for the location of the subdivision given the language as discussed in the Clarksburg Master Plan and for the intended use.

The lot sizes were reviewed for compliance with the dimensional requirements for the MXPDP Zone Optional Method of Development as specified in the Zoning Ordinance Section 59-C-7.5. The lots will meet all the dimensional requirements for the area, frontage and width as recommended by the Master Plan and in accordance with the zone. The application provides adequate amounts of Open Space, as required by the MXPDP Zone. A detailed review is included in the Site Plan Project Data Table for the MXPDP Zone located on page 35 of this report. The Preliminary Plan has been reviewed by other applicable county agencies, all of whom have recommended approval of this proposal.

The Applicant is proposing no significant modifications that would adversely impact the conformance to the Clarksburg Master Plan and the Subdivision Regulations – Chapter 50. Therefore, all the other conditions and findings assessed in the previously approved Preliminary Plan No. 12003110B will still apply to this application.

Preliminary Water Quality Plan

The Development Plan Amendment (DPA) increased the non-residential (retail) uses from 120,000 square feet to 484,000 square feet; included in the approved total 2,420,000 square feet of non-residential area). The DPA established a “comprehensively planned and designed employment center” including a variety of employment opportunities, additional retail, hotel, entertainment uses, public uses and a variety of housing types. In accordance with the Development Plan, a Preliminary Water Quality Plan (PWQP) was also approved and required for each subsequent development application.

The revised portion of the PWQP is for Drainage Areas 11, 12, and 15 within the overall 535.4 acres of land known as Cabin Branch. This reflects the full build-out for the Property as approved under DPA 13-02. The purpose of the revised PWQP is to bring all the unapproved portions of the overall Cabin Branch Neighborhood into compliance with the current

stormwater management (SWM) regulations, including application of the Environmental Site Design (ESD) standards. A portion of the Cabin Branch Development was not permitted prior to the expiration date of the grandfathering clause of the old SWM provisions. This ultimately divided the Cabin Branch Development into two categories with regard to the SWM requirements. The portion of the development that was grandfathered was reviewed and approved under the Infrastructure Site Plan. Per the 2004 approvals, the majority of the Subject Property is permitted under the old SWM provisions. The portion of the Subject Property that is considered unapproved (not permitted for development) or not grandfathered, must update the underlying approved PWQP for the overall Cabin Branch Neighborhood in order to meet the new ESD standards.

#### Review for Conformance to the Special Protection Area (SPA) Requirements

As part of the requirements of the SPA Law, a SPA Water Quality Plan should be reviewed in conjunction with a Preliminary Plan of subdivision or Site Plan applications. Under the provision of the law, the MCDPS and the Planning Board have different responsibilities in the review of a Water Quality Plan.

In conjunction with planning staff, MCDPS staff has reviewed and approved the technical elements of the water quantity and quality control facilities including engineering and design. The Planning Board's responsibility is to determine if SPA forest conservation planting requirements, environmental buffer protection and site imperviousness limits have been addressed or satisfied.

#### MCDPS Special Protection Area Review Elements

MCDPS has reviewed and conditionally approved the elements of the SPA and PWQP in a letter dated August 25, 2014 (*Attachment A*). The responsibility of the Planning Board is to determine if the environmental buffer has been adequately protected and SPA forest conservation and planting requirements have been satisfied.

In acting on a Preliminary or Final Water Quality Plan the MCDPS Director has lead agency responsibility for:

- (i) Performance goals or criteria for the approved best management practices;
- (ii) Best management practices monitoring plan;
- (iii) SWM concept plan;
- (iv) Erosion and sediment control concept plan; and
- (v) Any other element of the plan for which the Department has primary lead agency design, review, and approval responsibility.

#### Planning Board Special Protection Area Review Elements

In acting on a Preliminary or Final Water Quality Plan the Planning Board has lead agency responsibility for:

- (i) Compliance with the Environmental Guidelines;
- (ii) Impervious Surfaces; and
- (iii) Forest Conservation;

### Environmental Guidelines

The Subject Property is located within the Clarksburg SPA and the Little Seneca Creek watershed, a Use Class IV-P watershed. The Countywide Stream Protection Strategy rates streams in this watershed as good overall quality. There are streams, floodplains, wetlands, and environmental buffers onsite. The revisions to the PWQP have no inherent impacts on Compliance with the Environmental Guidelines and should be addressed at the time of the Final Water Quality Plan (FWQP) within the Site Plan.

### Imperviousness

Within this section of the Clarksburg SPA there is no impervious surface cap or limit. However, a main goal of all SPA's is to reduce the overall impervious footprint of new development within SPA boundaries (the impervious footprint includes roads, paved surfaces such as driveways, houses, buildings, parking lots, etc.). As part of the original PWQP for the Cabin Branch Development a goal of less than 45% imperviousness was established.

The revisions to the PWQP have no inherent impacts on impervious surfaces or the previously established development a goal of less than 45% imperviousness and will be addressed in more detail at the time of the FWQP.

### Forest Conservation

The revisions to the PWQP have no inherent impacts on forest conservation and should be addressed at the time of the FWQP.

Planning Staff finds that the Limited Preliminary Plan Amendment (No. 12003110C) meets all requirements of Chapter 22A regarding forest conservation and Chapter 19 regarding water resource protection and any other applicable law.

## SITE PLAN

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Rendered Landscape Plan

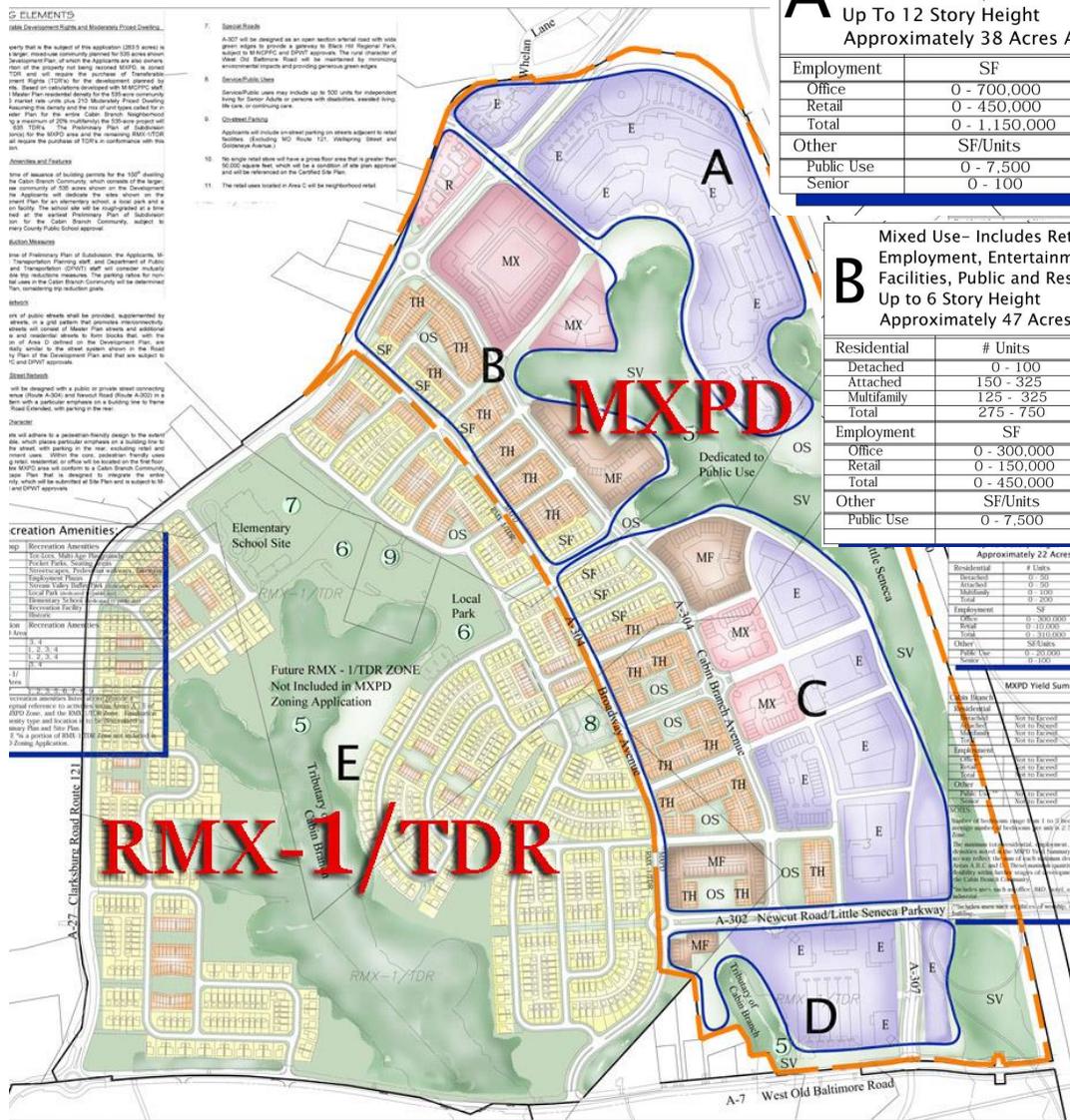
### PROPOSAL

The Applicant is proposing to construct two free standing restaurants (13,000 sf.) and a premium retail shopping center with a food court (437,000 sf. of retail/restaurant uses), surface parking facilities and public facilities (known as Phase I of the Clarksburg Premium Outlets) on 84.24 acres of land zoned MXP. The Subject Property is a smaller component to a larger neighborhood that will be phased over time. This development provides a mix of uses, significant public amenities, improved circulations systems, public art and recreational opportunities within the proposed green space areas.

# ANALYSIS & FINDINGS: SITE PLAN

- The site plan conforms to all non-illustrative elements of a development plan or diagrammatic plan, and all binding elements of a schematic development plan, certified by the Hearing Examiner under Section 59-D-1.64, or is consistent with an approved project plan for the optional method of development, if required, unless the Planning Board expressly modifies any element of the project plan.

This application conforms to the Development Plan Amendment (DPA-13-02) and the respective binding elements (see Appendix A).



**A** Predominantly Employment Includes Retail, Entertainment, Senior Facilities, And Public Uses Up To 12 Story Height Approximately 38 Acres Area

Employment	SF	Parking Spaces
Office	0 - 700,000	
Retail	0 - 450,000	
Total	0 - 1,150,000	0 - 5,050
Other	SF/Units	Parking Spaces
Public Use	0 - 7,500	0 - 25
Senior	0 - 100	0 - 200

**B** Mixed Use- Includes Retail Employment, Entertainment, Senior Facilities, Public and Residential Uses Up To 6 Story Height Approximately 47 Acres area

Residential	# Units	Parking Spaces
Detached	0 - 100	0 - 200
Attached	150 - 325	300 - 650
Multifamily	125 - 325	250 - 650
Total	275 - 750	550 - 1500
Employment	SF	Parking Spaces
Office	0 - 300,000	0 - 1,200
Retail	0 - 150,000	0 - 750
Total	0 - 450,000	0 - 1,950
Other	SF/Units	Parking Spaces
Public Use	0 - 7,500	0 - 50

Approximately 22 Acres in

Residential	# Units
Detached	0 - 50
Attached	0 - 100
Multifamily	0 - 100
Total	0 - 200
Employment	SF
Office	0 - 100,000
Retail	0 - 100,000
Total	0 - 200,000
Other	SF/Units
Public Use	0 - 100,000
Senior	0 - 100

**ELEMENTS**  
 1. **Development Plans and Materials**...  
 2. **Site Plan**...  
 3. **Site Plan**...  
 4. **Site Plan**...  
 5. **Site Plan**...  
 6. **Site Plan**...  
 7. **Site Plan**...  
 8. **Site Plan**...  
 9. **Site Plan**...  
 10. **Site Plan**...  
 11. **Site Plan**...

Approved Development Plan (DPA 13-02): Areas A and B – North District

This development will comply with all of the binding elements associated with the DPA. Binding element No. 10 was specifically added to limit the construction of individual stores to 50,000 sf of gross floor area in order to reduce the possibility of big box retailers (*see Appendix A*).

The DPA limits the maximum density of the different uses by area. The North District (Area A and B) consists of retail, office, public use and residential uses. The Applicant will construct Phase I of the Areas A and B within the property limits and with the intention of creating a vibrant and active commercial shopping center in close proximity of the I-270 interchange. The activation of the streetscape along Clarksburg Road by the proposed uses stimulates economic development within the North District and becomes a destination that draws regional and local traffic to the Cabin Branch Neighborhood. The retail and employment uses constructed also serve the immediate needs of the existing residents, while creating a strong platform for the construction of office and residential uses in the near future.

2. *The site plan meets all of the requirements of the zone in which it is located.*

Commercial and retail uses are allowed in the MXP Zone. This Site Plan fulfills the purposes of the zone by providing multi-use centers located outside the central business districts and transit stations development areas. It is also intended that this zone provide a more flexible approach to the comprehensive design and development of multi-use centers than the procedures and regulations applicable under various conventional zoning categories and other planned development zones. This zone is utilized to implement existing public plans and pertinent county policies in a manner and to a degree more closely compatible with said County plans and policies than may be possible under other zoning categories. The following specific purposes have been adequately addressed in this application.

- a) To establish standards and procedures through which the land use objectives and guidelines of approved and adopted master or sector plans can serve as the basis for evaluating an individual multi-use center development proposal.

Working with the Clarksburg Town Architect and associated Design review Committee, the Guideline provisions will be modified by in order to better conform to destination retail requirements. Staff finds that the revisions adequately address the needs of retail tenants and pedestrians, and are compatible with the surrounding uses. The destination retail component is a necessary enhancement to the Guidelines and the revisions are considered modest changes that will not adversely impact the overall design character of the Cabin Branch Neighborhood.

- b) To encourage orderly, staged development of large-scale, comprehensively planned, multi-use centers by providing procedures for the submission of a concept for an

entire site and subsequent development plans for each stage of development, as identified on the concept plan.

The Applicant has given adequate consideration to future development phases, specifically regarding the construct of additional employment uses (e.g. office and retail uses). Given the necessary parking requirements for the proposed uses, the second phase of development will look for opportunities to convert surface parking into structured parking facilities. The Applicant will need to maintain adequate parking spaces throughout the development process. Therefore, the future building footprints are contingent on the total number of buildable parking spaces. The office/retail buildings (labeled in blue and orange below) typically prefer clear visibility from major roadways (e.g. Clarksburg Road and I-270), while residential units (labeled in green below) should be located within walking distance of to other residential uses and public amenities. The conceptual drawings below depict the full build out of additional employment and residential uses (Options 1 and 2). These drawings are subject to change during the review process of their respective applications.



- a. To provide, where appropriate, higher density residential uses integrated into the overall multi-use center.

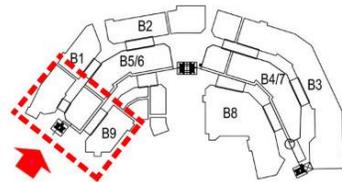
The Applicant proposes to construct Phase I only of the Subject Property. Other respective phases will introduce other employment uses (office/retail) and residential uses that are compatible with existing and surrounding uses. Until such time, the Winchester Phases I and II (toward the south) provide residential uses within in close proximity of the Subject Property.

- b. To ensure internal compatibility of residential and nonresidential uses by providing a suitable residential environment that is enhanced by the commercial, recreational, employment and institutional amenities within commercial and industrial components of the multi-use center.

The North District of the overall Cabin Branch Neighborhood primarily consists of a hotel, banks, and other commercial/retail uses (e.g. the outlet shopping center, restaurants, car wash, etc.). The Gosnell Property (Site Plan No. 820060240) and the retail shopping center are compatible uses that provide connections to the surrounding neighboring properties (i.e. residential uses). The streetscape along Clarksburg Road will be activated with pedestrians and building frontage in close proximity of major intersections and roundabouts; thereby slowing down vehicular traffic. The Clarksburg community has long awaited the opportunity for local retail and will further benefit from enhancements to the public facilities and amenities that will be developed concurrently with this application.

- c. *To assure compatibility of the proposed land uses with surrounding uses by incorporating higher standards of land planning and site design than could not be accomplished under conventional zoning categories.*

The existing grades and the environmentally sensitive features surround the Subject Property provide a unique opportunity to create a more sustainable retail center. The existing topography drops approximately 100 feet from MD 121 to the low point in the stream valley buffer on the southern portion of the Property. This topography allows the shopping center to be tiered as the grades drop. The building footprints and parking facilities were designed to work with the natural grades (i.e. require less disturbance) and be more compact than the typical construction for retail shopping centers. As illustrated below, the middle-tier building(s) are 2-stories with retail frontages on two different elevations.



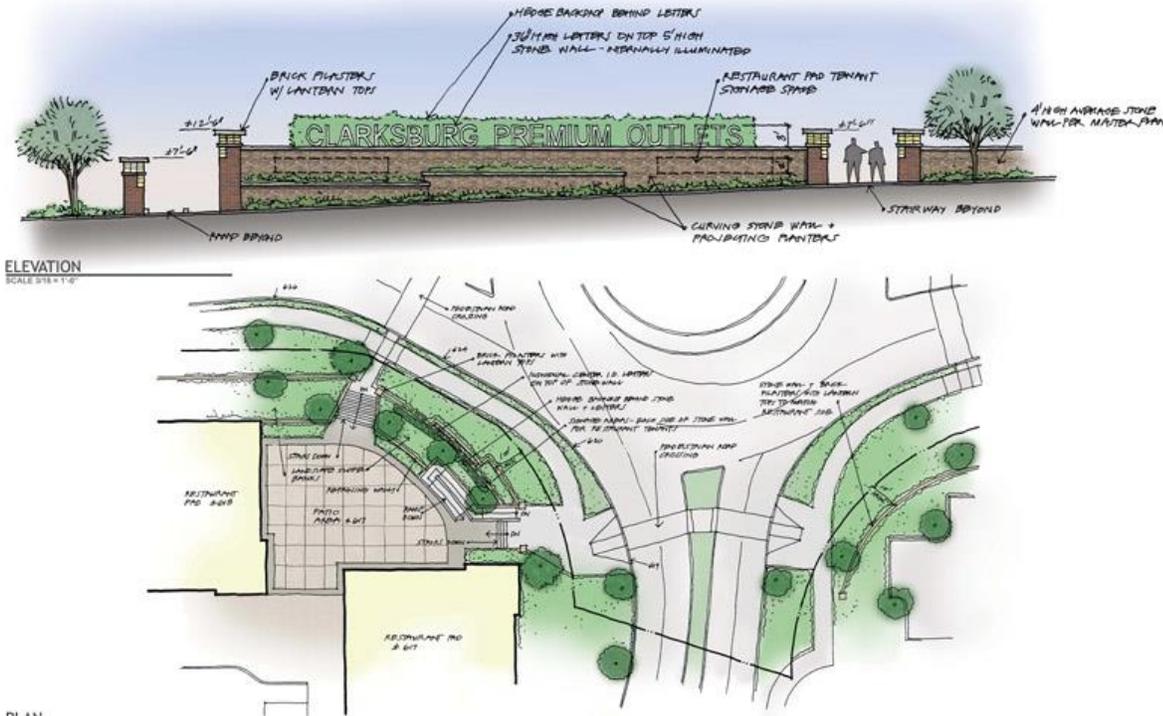
4 - BUILDING 300 NORTH ELEVATION

d. To encourage and provide for open space not only for use as setbacks and yards surrounding structures and related walkways, but also conveniently located with respect to points of residential and commercial/industrial concentration so as to function for the general benefit of the community and public at large as places for relaxation, recreation, and social activity. It also intended that open space and amenities be located so as to achieve the physical and aesthetic integration of the uses and activities within each development. In addition, structured parking within mixed-use planned developments is encouraged to help achieve the open space and amenities objectives of the zone. Where surface parking is necessary, the purposes of this zone may be achieved by the provision of additional landscaping.



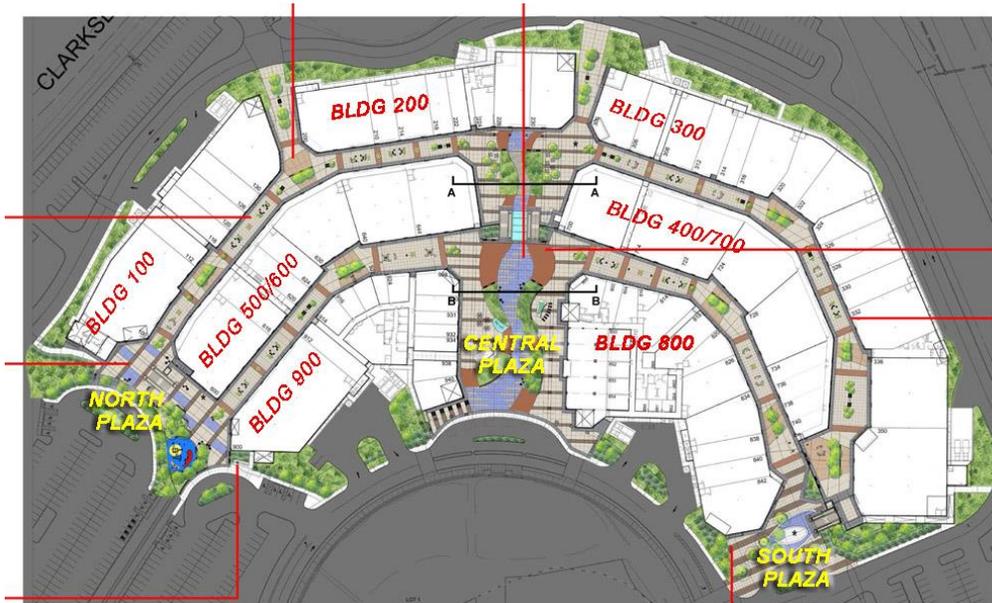
Overall Use Diagram (above) & Amphitheater (below)

Some of the public, open and green space areas are located along the perimeter of the Subject Property. Public plaza areas (4), an 8-foot hiker/biker trail (bike racks included) and amphitheater are all available to the public and linked to the surrounding uses. These public amenities are accessible via the internal sidewalks and pathways. The amphitheater is located in the lower right corner (toward the southeast), accessible via the surface parking facility and adequately buffered from I-270.



Public Amenities: Restaurant Plaza Area

There are four (4) public plaza areas that are designed to function as gateways to the proposed uses. The restaurant plaza area is located directly adjacent to Clarksburg Road near the entrance to the Subject Property. This space will attract pedestrians, activate the streetscape along Clarksburg Road and provide outdoor seating for the adjacent restaurant uses. The main entrance feature to the retail shopping center helps to enclose the space and the pedestrian crosswalks will terminate at the access points.



*Public Amenities: Retail Plaza Areas*

The central public plaza area is the largest plaza. This space was designed to be the “heart” of the retail shopping center; an open gathering that reinforces the sense of place with framed major views toward the stream valley buffer (i.e. forested edge). This space provides east/west pedestrian connections on two different levels and a variety of seating areas enclosed by planters and activated by the food court.



*Public Amenities: North Plaza Areas*

The north plaza area is located in close proximity to the entrance off of Clarksburg Road. This space includes a drop-off entrance point (with a flush curb), artistic bicycle racks, bike shower facilities, seating areas and a playground.



EAST ELEVATION BLDG. 300 & 800

*Public Amenities: South Plaza Area*

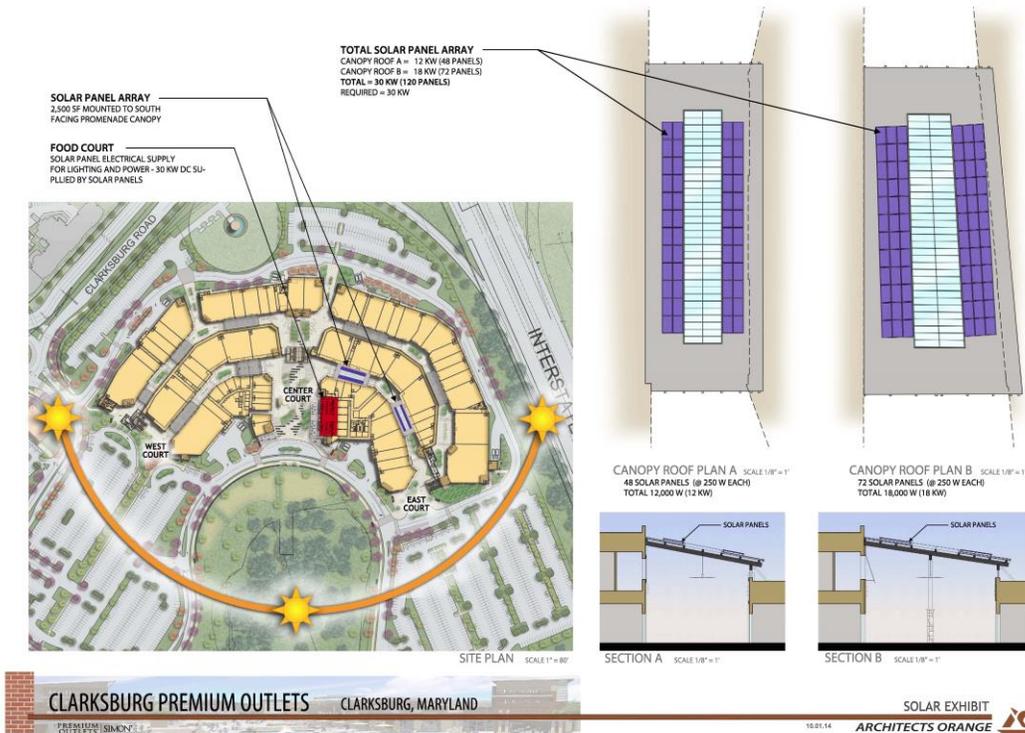
The south plaza area is located in close proximity to the southeast parking facility and I-270. This space provides a drop-off entrance point (with a flush curb), seating areas, artistic bicycle racks and bike shower facilities.

Structured parking facilities are anticipated with the future construction of office and residential phases. The surface parking facilities are heavily planted to exceed the internal landscape requirement by 18 percent (5% required, 23% proposed).

- e. *To encourage and provide for the development of comprehensive non-vehicular circulation networks, separated from vehicular roadways, which constitute a system of linkages among residential areas open spaces, recreational areas, commercial and industrial area and public facilities.*

As illustrated in the Overall Use Diagram (above) and within the Circulation Diagram (below, page 38 of this report), an 8-foot hiker biker trail and 5-foot sidewalks are proposed throughout the Property. These circulation systems provide walkable connections to the surrounding uses. The hiker/biker trail is primarily located along the internal drive aisles and stream valley buffer. The majority of the trail is separated from the 5-foot pedestrian sidewalk; while the sidewalks and pathways generally circulate through the center the surface parking facilities and provide a direct connection to the shopping center and amphitheater.

- f. *To encourage and provide for efficient use of energy resources through shared facilities or other economies of scale or technology, including innovative fuels and district heating,*



The retail shopping center is designed to exceed the Green Building environmental requirements and intends to be LEED Certified. Electric car charging stations (24 total stations) are planned to be phased into the surface parking facilities; four (4) stations will be constructed initially and twenty (20) are planned as needed. Solar panels will be installed on the structural canopies between the proposed buildings and the energy generated from the solar panels feeds directly into the food court.

The buildings and public spaces are designed to maximize solar exposure and reduce overall energy consumption (i.e. decrease energy dependency during peak hours of the day).

- g. To preserve and take the greatest possible aesthetic advantage of existing trees and to minimize the amount of grading necessary for construction of the development.*

The natural topography and existing environmental features (i.e. the stream valley buffer) provide an opportunity to terrace the construction of the proposed buildings and parking facilities. The overall layout embraces the southern perimeter of the Property and frames major views towards the forested edges. As illustrated below (dark green), the planted edges will be increased around the stream valley buffer, amphitheater and the SWM facilities.



*Forested Areas and Green Space Diagram*

The Site Plan meets all of the development standards of the MXPB Zone. The building heights, setbacks, and the density of this development do not exceed the maximum standards allowed. Regarding the parking requirements, the Applicant is requesting a slight reduction (2.8%) in accordance with the new Zoning Ordinance (Division 7.7 (b)(1)). This application was accepted by M-NCPPC Staff on June 30, 2014 and was considered in progress before the October 30, 2014 deadline. Therefore, the Applicant may choose to have the parking calculations be reviewed with the new parking provisions. The proposed circulation systems (e.g. sidewalks, hiker/biker trails and pathways) are in close proximity to local residential and retail uses, which enables walkable linkages to the Subject Property and have the potential to reduce dependency on vehicles.

**Development Standards**

The following data table indicates the development’s compliance with the Zoning Ordinance.

*Project Data Table for the MXPB Zone*

<b>Development Standard</b>	<b>Permitted/Required</b>	<b>Site Plan No. 820140160</b>
<b>MXPB Lot Tract Area</b>		
Gross Tract Area	283.50 acres	84.24 acres
Dedication - Public Roads (acres)	N/A	6.45 acres
Dedication – HOA (acres)	N/A	22.37 acres
WSSC Water Tower (Lot 2)	N/A	1.61 acres
<b>Net Area (acres)</b>	<b>N/A</b>	<b>53.81 acres</b>
<b>Max. Floor Area Ratio (FAR)</b>	<b>0.30 FAR</b>	<b>0.012 FAR</b>
<b>Max. Gross Commercial/Retail</b>	<b>484,000 sf.</b>	<b>450,000 sf.</b>
<b>Max. Building Height (feet)</b>	<b>N/A</b>	<b>70 feet</b>
<b>Min. Building Setbacks (feet)</b>		
From Residential Zones, other than one-family detached buildings	100 feet	100 feet (plus)
Front	10-feet	10 feet
Rear	0 feet	0 feet
Side (Street Side)	10 feet	10 feet
Side (Interior Lot)	3 feet	3 feet
Side (Alley)	3 feet	3 feet
R/W Truncation	0 feet	0 feet
<b>Min. Green Area for MXPB zone</b>		
Residential Uses	50% (34.90 acres)	23% (16.50 acres) <sup>1</sup>

<sup>1</sup> The residential green space calculations reflect approved and proposed Site Plans. This figure is an estimation of green area within each plan thus far. As final designs are completed, this table is subject to change and will be updated with more precise

Commercial Uses	40% (85.48 acres)	68% (144.70 acres)
<b>Min. Internal Landscaping within Surface Parking Facilities (%)</b>	5% (51,422 sf.) <sup>2</sup>	23 % (236,966 sf.)
<b>Tree Canopy (%)</b>	25%	50% <sup>3</sup>
<b>Min. Parking Spaces<sup>4</sup></b>		
Commercial/ Retail Uses	2,185 spaces	2,123 spaces <sup>5</sup>
Restaurant Uses	162 spaces	170 spaces
<b>Total</b>	<b>2,347 spaces</b>	<b>2,293 spaces</b>
Handicap Parking	46 spaces	<i>± 53 spaces</i>
Electric Charging Stations	N/A	24 spaces <sup>6</sup>
Bicycle Spaces	115 spaces	218 spaces
Motorcycle Spaces	10 spaces	10 spaces

3. *The locations of buildings and structures, open spaces, landscaping, recreation facilities, and pedestrian and vehicular circulation systems are adequate, safe and efficient.*

#### Building and Structures

The buildings and structures of the Subject Property are located south of Clarksburg Road (directly adjacent to the street edge) and west of I-270. The main entrance of the development is clearly visible from the street edge, and major views toward the Property from I-270 will outwardly project a sense of place to potential visitors. At this location, the proposed uses are appropriate for the character envisioned by the Master Plan and compatible with the surrounding uses. These locations provide access to the buildings from adjoining sidewalks and parking facilities, while also creating adequate distance between neighboring residential properties. The building frontages provide a strong retail presence in conjunction with the Gosnell Property. The streetscape and slows vehicular traffic entering the traffic circle. Staff finds the locations of the buildings and structures to be adequate and efficient. The buildings and structures do not pose any safety concerns for the development.

#### Open Space, Landscape and Recreation Facilities

The open space and forested buffers are primarily located along the edges of the Property, between the residential uses (toward the west and south) and I-270 (toward the east). The existing stream valley buffer is located at the center of the Property and serves as an important organizational feature. The forested edges and natural landscape provide a screen for the parking facilities. Open space areas include, but are

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calculations for the overall MXPD Zone. This table does not reflect the future construction of residential uses for this development.

<sup>2</sup> Total parking area is 23.61 acres.

<sup>3</sup> The percentage is based of the total acreage.

<sup>4</sup> Per the requirements of the New Zoning Ordinance, Division 7.7.1 Exemptions B (1).

<sup>5</sup> Per the New Zoning Ordinance, Divisions 6.8.1 Alternative Compliance and 6.2.3G (2a) Calculations of Required Parking, the Applicant is requesting a parking reduction of approximately 62 spaces (2.8%) due to site constraints and existing steep grades.

<sup>6</sup> The electric car charging stations will be phased over time, as deemed necessary with future development phases. Four (4) stations will be constructed in Phase 1, and twenty (20) stations are planned for the future.

not are not limited to, the stream valley buffer, the tot lot and the public plazas. Foundation plantings are located near the building edges and are integrated into the design of internal pathways throughout the shopping center. The foundation plantings, building's facades, pavement patterns and public artworks enrich the pedestrian environment and encourage a colorful pattern that supports the proposed Way-Finding system. Shade trees and lighting are provided along the drive aisles and within the parking facilities to further enhance the pedestrian environment. Interior lighting will create enough visibility to provide safety, but not so much as to cause glare on the adjacent roads or neighboring properties. Recreational facilities are not required for this Site Plan application, but a tot lot, an amphitheater, seating areas, a hiker/biker trail and bicycle facilities are provided. The open spaces, landscaping and site details adequately and efficiently address the needs of the retail tenants and consumers; while also providing a safe and comfortable environment for pedestrians.

#### Vehicular and Pedestrian Circulations Systems

The main entrance to the Property is located off of Clarksburg Road (near the traffic circle). The 30-foot internal road network was designed to bring vehicular traffic from the traffic circle into the Property; while providing adequate stacking distance off of the main roadway. The driveway (i.e. Premium Outlets Drive) has a planted median and will be designed as a private street. The internal roadway loops the entire Property and connects into a secondary access point off of Cabin Branch Avenue. A shared use path will run along the south side until it reaches the proposed amphitheater (at the southern terminus).

Parking facilities are provided on surface lots (during phase one), and will be accessed off of Premium Outlets Drive. The internal loop continues throughout the Property and connects into the backside of the retail buildings (directly adjacent to I-270). The surface parking facilities along the backside of the buildings will be primarily used by employees. In future development phases, it is anticipated that surface parking facilities will be converted into structured parking and new buildings will be constructed on surface lots. Per the requirement of the new Zoning Ordinance (Division 6.2.3G (2a): Calculations of Required Parking), the minimum required parking for commercial/retail uses is 2,347 spaces; however, the Applicant is requesting a reduction to the parking requirement for Phase one. The total number of proposed parking spaces is 2,293 spaces (including handicap, motorcycle and electric charging stations).

The RideOn mass transit system (i.e. public facilities) is another sustainable alternative circulation system that could be provided in the near future. This circulation system would not only offer commuters and residents additional transportation, but could also further reduce the need and dependency on surface parking facilities. Routes 75 and 79 currently operate in close vicinity of the Subject Property. These routes provide service to Stringtown Road and Gateway Center Drive with direct access to Shady Grove Metro Station. While RideOn services are not currently available to the Subject Property, the

County should consider an additional stop along Clarksburg Road (near the retail and other employment uses) as the overall neighborhood continues to develop. Phase I of this development anticipates the need for public transit facilities by providing parking spaces for bus/shuttle services and three (3) different drop-off areas in close proximity to the public amenities.

Bicycle and pedestrian connectivity between Clarksburg Road (the first roundabout), Cabin Branch Avenue (second roundabout) and the southernmost portion of the Property provide another sustainable circulation system for local residents and visitors. An 8-foot hiker/biker trail and pedestrian pathways are separate from vehicular systems, and integrated along the perimeter of the stream valley buffer and Premium Outlets Drive. Pedestrian sidewalks are oriented parallel to the parking stalls and provide an efficient connection to the open space areas, public facilities and commercial/retail spaces. As illustrated in the diagram below, the vehicular, bicycle and pedestrian systems were all designed to connect into the overall neighborhood.



*Circulation Diagram*

Pedestrian access from adjacent sidewalks adequately and efficiently integrates the Property into the surrounding neighborhood. Pedestrian safety is further enhanced by a

Way-Finding signage package, the road alignment (i.e. continuous curved loop) and the landscape plantings. The vehicular circulation design efficiently directs traffic into and through the Property with minimal impacts to pedestrian circulation. Balancing the overall design of the Property, considering the recommendations of the Master Plan and focusing on the specific needs of the users, this development adequately provides a safe atmosphere for pedestrians, cyclists, and vehicles.

4. *Each structure and use is compatible with other uses and other site plans and with existing and proposed adjacent development.*

The Clarksburg Master Plan envisioned the establishment of highly desirable employment areas, without limiting specific employment uses. Based on market demands, the days of “office-only” business centers are a thing of the past. Office developments are considered more successful when the placemaking characteristics include venues for retail, public art and culture, aesthetic appeal, recreational opportunities and connections to housing resources. With that in mind, the development is compatible with other uses and Site Plans (existing and proposed) adjacent to this development. It has been designed with a strong emphasis on four planning principles: 1) Neighborhood Location and Placemaking, 2) Linkage and Pedestrian Orientation, 3) Compatibility and Energy Conservation, and 4) Environmental Protection and Conservation.

Neighborhood Location and Placemaking:

The prime location of the retail shopping center serves as a catalyst for the development of additional employment and residential uses in the near future. The Subject Property is naturally buffered from the surrounding residential uses (toward the south) by the stream valley buffer and existing topography. It is clearly visible from the Clarksburg Road and I-270. Local residents will have an adequate buffer zone and alternative circulation systems to access the Property from Cabin Branch Avenue. Major views from I-270 will also attract local and regional visitors. The retail shopping center are compatible with the Gosnell Property (i.e. other commercial and employment uses), and both properties will activate Clarksburg Road, which ultimately support the network of commercial uses throughout the Cabin Branch Neighborhood. Establishing a strong commercial/retail core, while building other public facilities (e.g. schools and parks), will attract additional employment uses and create a local and regional destination point.

Linkage and Pedestrian Orientation:

The Subject Property will have primary access from Clarksburg Road. A secondary access point is also provided within the community along Cabin Branch Avenue. An internal roadway loop (i.e. Premium Outlets Drive) provides access to vehicular and bicycle traffic via a shared-use pathway. Internal sidewalks are efficiently located throughout the development and connect into existing circulations systems. The pedestrian orientation and heat Island effects will be improved with the use of

landscape enhancements (e.g. installation of shade trees and ornamental trees).

Compatibility and Energy Conservation:

The base building(s) will be designed to meet LEED Certification, and should exceed the Green Building Requirements for the County. The building massing and public spaces have been oriented to maximize solar exposure. This development features solar panels on the structural canopies (between the buildings). The energy generated from the solar panels will be used to power the food court. Electric car charging stations (maximum 24 stations phased over time) are located in the surface parking facilities. The surface parking facilities will be converted into residential and office uses upon generating higher demand in the near future. The total tree canopy is approximately 50% of the total acreage, internal landscaping is approximately 23% of the total parking area and the overall imperviousness is 42.7% of the total acreage. The retail shopping center will also serve as an anchor for other surrounding commercial uses within the Neighborhood. This development is designed to attract long-term office (additional employment uses) in future development phases.

Environmental Protection and Conservation:

The buildings, structures and parking facilities work with the existing topography to reduce the overall disturbance on the natural environment. The edge of the stream valley buffer (toward the south) is a focal point and organizational feature for the center. All of the design features are organized around the forested edges and green space areas to create framed major views. The surface parking facilities include planted islands heavily planted in order to treat SWM runoff and reduce heat island effects. The surface parking facilities will also be converted into structural facilities with the development of future phases.

5. *The site plan meets all applicable requirements of Chapter 22A regarding forest conservation, Chapter 19 regarding water resource protection, and any other applicable law.*

As part of the requirements of the SPA Law, a SPA Water Quality Plan should be reviewed in conjunction with a Preliminary Plan of subdivision or Site Plan. Under the provision of the law, the MCDPS and the Planning Board have different responsibilities in the review of a water quality plan.

In conjunction with planning staff, MCDPS has reviewed and approved the technical elements of the water quantity and quality control facilities including engineering and design. The Planning Board's responsibility is to determine if SPA forest conservation planting requirements, environmental buffer protection, and site imperviousness limits have been addressed or satisfied.

## **MCDPS Special Protection Area Review Elements**

MCDPS has reviewed and conditionally approved the elements of the Final Water Quality Plan (FWQP) under its purview. The Planning Board responsibility is to determine if environmental buffer protection, SPA forest conservation and planting requirements have been satisfied.

In acting on a final water quality plan the MCDPS Director has lead agency responsibility for:

- (i) Performance goals or criteria for the approved best management practices;
- (ii) Best management practices monitoring plan;
- (iii) Stormwater management concept plan;
- (iv) Erosion and sediment control concept plan; and
- (v) Any other element of the plan for which the Department has primary lead agency design, review, and approval responsibility.

### **Performance goals or criteria for the approved best management practices**

As part of the final water quality plan, several site performance goals have been achieved to date that were established for the Cabin Branch Development:

- 1) Protect the streams and aquatic habitat.
- 2) Maintain the natural onsite stream channels.
- 3) Minimize storm flow run off increases.
- 4) Identify and protect stream banks prone to erosion and slumping.
- 5) Minimize increases in ambient water temperatures.
- 6) Minimize sediment loading.
- 7) Maintain stream base flows.
- 8) Protect springs, seeps, and wetlands.
- 9) Minimize nutrient loadings.
- 10) Control insecticides, pesticides, and toxic substances.

### **Stormwater Management and Erosion and Sediment Control**

The Final Water Quality Plan proposes to meet required stormwater management goals via permeable pavement, bioswales and micro-bioretenion. All of the previously required goals and conditions noted on the Preliminary Water Quality Plan still apply. A BMP monitoring fee will be required for the area of this FWQP.

MCDPS has reviewed and approved the elements of the SPA FWQP plan under its purview in a letter dated September 30, 2014 (*Appendix C*).

### **Planning Board Special Protection Area Review Elements**

In acting on a preliminary or final water quality plan the Planning Board has lead agency responsibility for:

- (i) Compliance with the Environmental Guidelines;
- (ii) Impervious Surfaces;
- (iii) Forest Conservation;

#### **Environmental Guidelines**

The site is located within the Clarksburg SPA and the Little Seneca Creek watershed, a Use Class IV-P watershed. The Countywide Stream Protection Strategy rates streams in this watershed as good overall quality. There are streams, floodplains, wetlands, and environmental buffers on site.

The overall project is in compliance with the Environmental Guidelines and the previously approved FFCP with two exceptions. There are two areas of minor grading (a 0.1 acre (Area A) and 0.2 acre (Area B)) that are comprised of 3:1 grading tie-outs in an unforested and tilled area of the SVB. The grading is a result of the enhanced green space corridors and ESD features near these two locations. Avoiding these grading tie-outs would result in less internal green space in the surface parking area. The grading tie-outs do not impact forested areas and will be reforested and placed in a category 1 conservation easement.

Staff believes the temporary impacts to unforested SVB combined with the additional internal open space provided by the minor encroachments provides for a better overall condition.



*Forested Areas and Green Space Diagram*

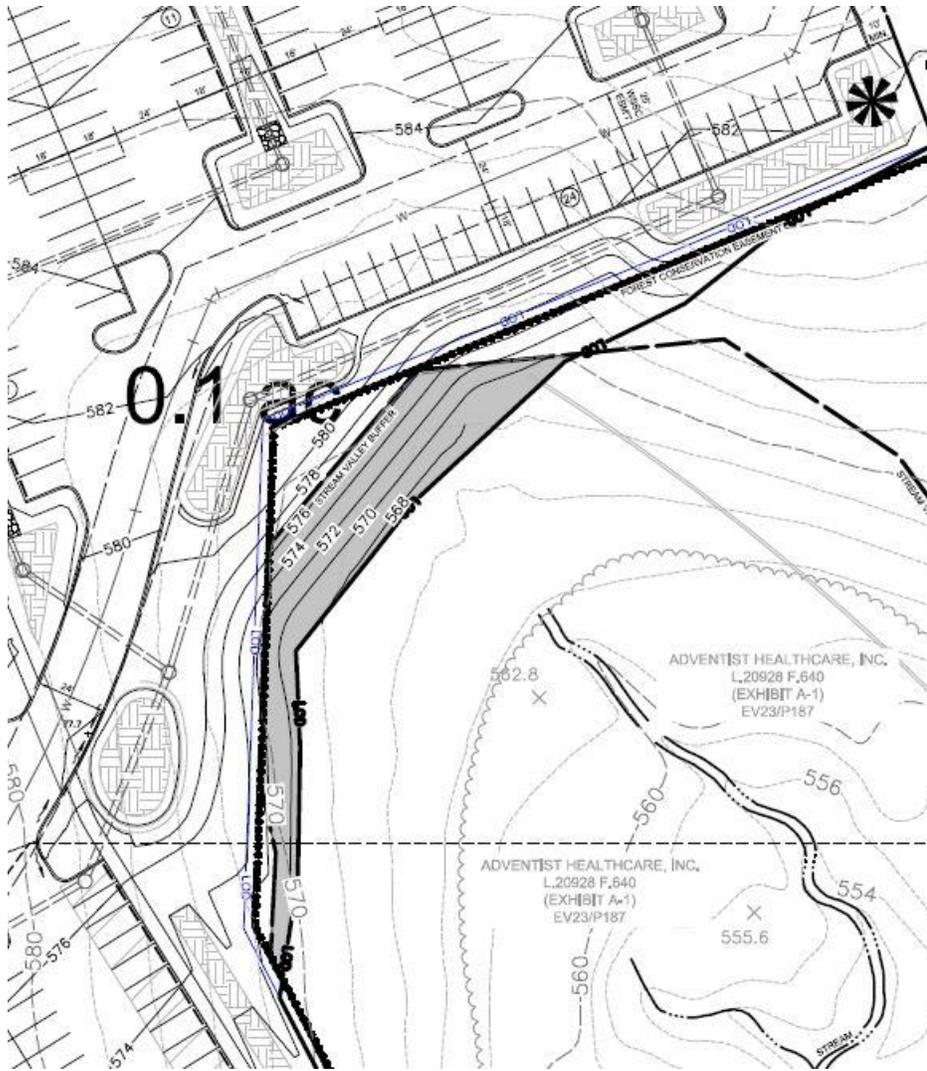


Figure 1: SVB Impact Area A

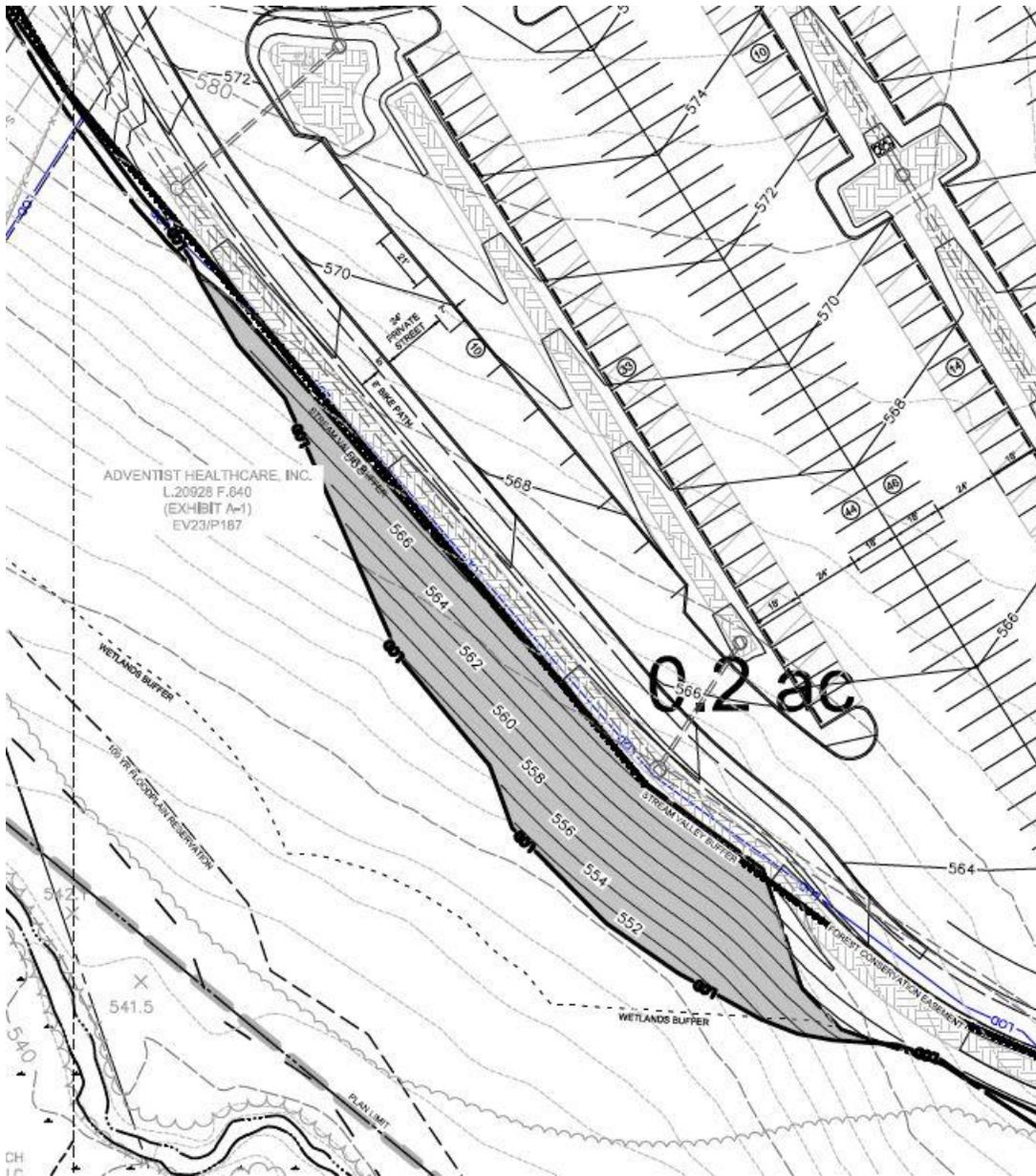


Figure 2: SVB Impact Area B

### Imperviousness

Within this section of the Clarksburg SPA there is no impervious surface cap or limit. A main goal of all SPA's is to reduce the overall impervious footprint of new development within SPA boundaries (the impervious footprint includes roads, paved surfaces such as driveways, houses, buildings, parking lots, etc.). As part of the original PWQP, and the revised PWQP, for the Cabin Branch Development, a goal of less than 45% imperviousness was established.

This Site Plan indicates an impervious level of approximately 42.7% with approximately 35.99 acres of impervious surface (out of 84.24 acres) and is therefore consistent with the approved and Revised PWQP. The Cabin Branch Development is on course to remain below an aggregate level of 45% imperviousness for the entire 535-acre development.

#### Final Forest Conservation Plan

Consistent with conditions 1 and 14(c) of the Infrastructure Site Plan (82005015B), the Final Forest Conservation Plan (FCP) for the overall Cabin Branch Development is being amended through this Site Plan. The Infrastructure Site Plan is amended with each individual site plan as stated in conditions 1 and 14(c).

The entire 535-acre Cabin Branch Development has a FCP which was approved with the Infrastructure Site Plan. The FCP was designed to allow for the grading and installation of roads, utilities, and public amenities such as school and park sites. The intent of having one FCP was to ensure that as this multi-year project developed, portions would not be left uncovered by an individual FCP and the whole project would remain in compliance with the Montgomery County Forest Conservation Law.

The FCP includes seven worksheets separated by zones or land uses: MXPB-Employment, RMX-1/TDR, MXPB-Residential, Water Tower Storage Facility, Linthicum West, "offsite Area A", and West Old Baltimore Road- MDR Category (Limited to Culvert #2 improvements). Each individual site plan application is submitted with final grading and design, the FCP and the worksheets associated with that particular site plan will be updated to reflect final design and grading details. The FCP indicates that the individual applicants for each site plan area must meet the forest conservation worksheet requirements through a combination of on-site forest retention, on-site planting of unforested stream buffers, landscape credit, and off-site planting within the Clarksburg SPA.

Under the M-NCPPC implementation of the SPA regulations, the Environmental Guidelines require accelerated reforestation of the SPA stream buffers and that any unforested portions of the stream buffer be afforested above and beyond the standard forest conservation requirements. Since the Cabin Branch Development includes land both in and out of the SPA and the tributaries drain to a common water body, Staff recommends the treatment of the planting requirements as if the entire development is located within the Clarksburg SPA. Conditions 1 and 14 of the Infrastructure Site Plan (82005015B) require the Applicant to plant the stream buffers in accordance with the FCP including the Planting Phasing Plan. The Applicant must provide a five-year maintenance period for all planting areas credited toward meeting the forest conservation plan worksheet requirements.

The required Phasing Plan was designed to establish easements and forest planting for the entire Cabin Branch project, while taking into consideration that it will be a multi-

year phased development with multiple site plans for individual portions of the development. The Phasing Plan considered how each individual site plan area would address forest conservation in terms of the size (proportion) of the site plan project and the percentage of total units to the entire development.

Clarksburg Premium Outlets at Cabin Branch

To reflect the new design for the proposed retail shopping center, this Site Plan will require revisions to 21 sheets (1-11, 13-16, 46, 57, 58, 62, 63, and 63A) of the approved Final Forest Conservation Plan. These pages are attached to this Staff Report.

The revision to the FFCP not only update the plans to show the new layout and design of the retail shopping center portion of the Cabin Branch Neighborhood, but also updates the plans to show additional areas of forest retention and planting that became available based on the final design.

The final design of the retail shopping center portion of the Cabin Branch Neighborhood FFCP managed to retain an additional 0.30 acres of existing forest and provided and additional 1.02 acres of onsite afforestation.

<b>Clarksburg Premium Outlets at Cabin Branch</b>			
<b>Final Forest Conservation Plan Comparison</b>			
<b>Retention Area Key</b>	<b>Previous Submission (Acres)</b>	<b>Current Submission (Acres)</b>	<b>Increased Retention / Planting (Acres)</b>
A (Retain)	4.68	4.68	0.00
A1 (Retain)	0.38	0.38	0.00
B1 (Retain)	3.29	3.42	0.13
B2 (Retain)	14.17	14.34	0.17
<b>Subtotal</b>	<b>22.52</b>	<b>22.82</b>	<b>0.30</b>

*Figure 3: Forest Retention Area Changes*

Clarksburg Premium Outlets at Cabin Branch			
Final Forest Conservation Plan Comparison			
Planting Area Key	Previous Submission (Acres)	Current Submission (Acres)	Increased Retention / Planting (Acres)
M1 (Plant)	0.57	0.61	0.04
M1A (Plant)	0.18	0.16	-0.02
M2 (Plant)	0.25	0.00	-0.25
N (Plant)	4.37	5.36	0.99
Q1 (Plant)	0.58	0.59	0.01
Q2 (Plant)	1.70	1.64	-0.06
U (Plant)	1.45	1.46	0.01
A-A (Plant)	0.30	0.30	0.00
<b>Subtotal</b>	<b>9.40</b>	<b>10.12</b>	<b>0.72</b>
<b>Total</b>			<b>1.02</b>

*Figure 4: Forest Planting Area Changes*

Additional Forest Conservation Issues

This FFCP amendment includes a resolution for the reconciliation of a pending forest conservation easement violation related to the adjacent Kenney residence, located at 13621 West Old Baltimore Road.

On Nov. 28, 2014, Cabin Branch Management, LLC received a Notice of Violation related to an encroachment by an adjacent property owner, referred to as the Kenney Property, located at 13621 West Old Baltimore Road. The encroachment consisted of a concrete driveway extending from the Kenney property onto part of the Cabin Branch property in an area encumbered by a category 1 conservation easement. The encroachment was

installed after the survey of the phase 3 category 1 conservation easement.

Upon receipt of the Notice of Violation, Cabin Branch Management, LLC notified the owners of the Kenney property of the receipt of the Notice of Violation, notified them to cease mowing activity, and informed them that Cabin Branch Management, LLC would be required to resolve the violation with M-NCPPC immediately. The Kenney's acknowledged that they were responsible for installing the concrete driveway and that it was installed beyond their property line and onto the Cabin Branch property.

In order to resolve this matter, the Applicant met with M-NCPPC Staff and agreed upon a course of action to resolve the violation. Cabin Branch Management, LLC and Adventist HealthCare (the property owner) agreed to have a split rail fence installed along the Kenney property and encroachment area to stop any further encroachment and clarify the limits of the easement. The agreement further consisted of Cabin Branch Management, LLC agreeing that the next FFCP amendment for Cabin Branch would include revisions to remove the easement and associated planting within the encroachment area and replacement of the easement and planting area elsewhere on the Cabin Branch property. The future record plat that covers this area will also reflect the revised easement delineation, consistent with and as permitted by section 17 of the Cabin Branch category 1 easement agreement.

The FFCP amendment on Sheet 58 shows the proposed release of 1,261 sf of conservation easement where the encroachment occurs, (planting area K, stage I) and on Sheet 46 shows the expansion of planting area K by 1,261 sf. A release agreement for the 1,261 sf will be submitted, processed and recorded concurrent with an additional category 1 easement for the replacement area.

## **COMMUNITY OUTREACH**

The Applicant has met all proper signage, noticing, and submission meeting requirements. Staff has received community correspondence in favor of this application. The community has expressed the need for local retail, infrastructure improvements and increased property values. The community fully supports the development of destination retail and welcomes the opportunity to have the retail shopping center become a part of the overall Cabin Branch Neighborhood.

## **CONCLUSIONS AND RECOMMENDATIONS**

Staff finds that the Preliminary Plan's limited modifications to the adequate public facilities and Preliminary Water Quality Plan have been adequately addressed and will not adversely impact the intent and purpose of previous approvals. The Preliminary Plan and Site Plan substantially conform to the Development Plan and associated binding elements. Staff supports the minor modifications to the Clarksburg Design Guidelines, and finds that the revisions adequately

addresses the site design details directly associated with the destination retail component. The retail shopping center and restaurants are compatible uses permitted within the MXPB Zone, and do not exceed the development standards. The location and design features of proposed buildings, structures, open space, recreational facilities and circulation systems are safe, adequate and efficient. Planning Staff also finds that the Final Forest Conservation Plan and SWM facilities meet all requirements of Chapter 22A regarding forest conservation and Chapter 19 regarding water resource protection and any other applicable law.

Staff also recommends approval of the SPA Final Water Quality Plan for retail shopping center, conforms to the Environmental Guidelines, satisfies forest conservation, and maintains impervious levels consistent with the original approvals.

## **APPENDICES**

- A. Previous Approvals Chart & Binding Elements
- B. Letter of Justification
- C. Agency Approval Letters
- D. Community Correspondence
- E. Additional Project Data, Design Guideline Revisions and Submitted Plans

**APPENDIX A**

*(Previous Approvals Chart & Binding Elements)*

<b>Case No.</b>	<b>Opinion</b>	<b>Type of Plan</b>	<b>Uses Approved</b>	<b>Status</b>
G-806	9/9/2003	Local Map Amendment w/ PWQP	Rezoned 283.5 acres to MXPDP (eastern portion of the tract). Development Plan 535 acres to allow 1,886 dwelling units, 2,240,000 sf. of commercial and 500 senior housing units.	Approved
<b>Preliminary Plans</b>				
120031100 & 12003110A	6/22/2004	Preliminary Plan w/ PWQP	1,600 dwelling units, 500 senior housing units, 1,538,000 sf. commercial space, transportation improvements, LATR review, road construction phasing on 535 acres of land.	Approved
120030110B	10/6/2008	Preliminary Plan w/ PWQP (based on previous approvals)	1,886 dwelling units (including MPDUs), 2,420,000 sf. commercial space, 500 senior housing units.	Approved
<b>Associated Mandatory Referrals</b>				
08001-WSSC-1	3/24/2008	Mandatory Referral – Water Storage Tank	750,000 gallon storage tank (24-hour operation), intended to serve approximately 15,000 people in the 760A Zone by year 2020.	Approved
MR2014049	6/2/2014	Mandatory Referral	Preliminary/Final Water Quality Plan and Forest Conservation Plan associated with the construction of MD 121 & I-270.	Approved
<b>SITE PLANS</b>				
820050150	09/19/2007	Infrastructure Site Plan w/ FWQP	Roads only. Density per 12003110B, maximum 635 TDRs, 236 MPDUs, 28.32 acres green space, public water and sewer (W-3), Master Plan Unit Mix, 2,436,000 sf. retail and employment.	Approved

82005015A	6/9/2008	Infrastructure Site Plan Amendment	Modifications to the conditions of approval and roadway cross-section of Old Baltimore Road.	Approved
82005015B	3/7/2012	Infrastructure Site Plan Amendment	Revisions to FCP, SMW and FWQP.	Approved
82005015C	11/27/2012	Infrastructure Site Plan Amendment	Revisions to FCP, SMW and FWQP.	Approved
82005015D	5/2/2013	Infrastructure Site Plan Amendment	Modifications to Grading Plan, additional turn lane and additional pavement removed.	Approved
82005015E	6/2/2014	Infrastructure Site Plan Amendment	Revisions to the FCP (category I easements and mitigation project).	Approved
82005015F	9/23/2014	Infrastructure Site Plan Amendment	Revisions to the FCP including adjustments to the LOD.	Approved
820060290	10/20/2008	Winchester I Site Plan w/ FWQP	428 dwelling units, including multi-family, attached and detached one-family homes, 64 MPDUs, 128 TDRs, 62.55 green space.	Approved
82006029A	7/11/2012	Winchester I & II Site Plan Amendment w/ FWQP	400 dwelling units in the MXPd and RMX-1/TDR Zones, 341 dwelling units in the MXPd and RMX-1/TDR Zones	Approved
820120150	4/4/2013	Winchester III Site Plan Amendment w/ FWQP	RMX-1/TDR zone w/ 185 dwelling units (including 10 MPDUs), consisting of 128 one-family detached and 57 one-family attached units using 56 TDRs.	Approved

820060240	9/29/2010	Gosnell Property Site Plan w/ FWQP	Hotel, banks (employment), 87,500 sf (hotel) and 8,600 sf. (retail) auto-related uses w/ parking waiver.	Approved
820070140	5/2/2013	Toll Brothers I & Site Plan w/ FWQP	435 dwelling units, including 239 one-family detached, 68 one- family attached and 128 multi- family units.	Approved
820100030	5/2/2013	Toll Brothers II Site Plan w/ FWQP	RMX-1/TDR-3 Zone w/ 168 dwelling units (including 128 one-family detached and 40 one-family attached units) with 10 MPDUs using 63 TDRs on 46.35 acres.	Approved

#### DPA 13-02 Binding Elements

**1. Transferable Development Rights and Moderately Priced Dwelling Units (TDRs and MPDUs)**

The Cabin Branch Neighborhood is a mixed-use community planned on 535-acres of land zoned MXPd and RMX-1/TDR. The MXPd zone (283.5-acres) is the portion of the property will not be rezoned. The RMX-1/TDR portion requires the purchase of TDRs, and based on the maximum residential density for the entire property MPDUs are also required. Assuming the density and the mix of unit types as called for in the Master Plan for the entire Cabin Branch Neighborhood (including a maximum of 20 percent multi-family) the 535-acre project will require 635 TDRs. The Preliminary plan of Subdivision applications for the MXPd area and the remaining RMX-1/TDR area shall require the purchase of TDRs in conformance with this calculation.

**2. Off-site Amenities and Facilities**

By the time of issuance of building permits for the 100<sup>th</sup> dwelling unit in the Cabin Branch Neighborhood, which consists of the MXPd and RMX-1/TDR zones (535-acres as shown on the Development Plan, the owner will dedicate the sites shown on the Development Plan for an elementary school, a local park and a recreation facility. The school site will be rough-graded at a time determined at the earliest Preliminary plan of Subdivision application for the Cabin Branch Community, subject to Montgomery County Public School approval.

**3. Trip Reduction Measures**

At the time of Preliminary Plan of Subdivision, the owner, M-NCPPC Transportation Planning Staff and Department of Public Works and Transportation (DPWT) Staff will consider mutually acceptable trip reductions measures. The parking ratios for non-

residential uses in the Cabin Branch Community will be determined at Site Plan, considering trip reduction goals.

4. Street Network

A network of public streets shall be provided, supplemented by private streets, in a grid pattern that promotes interconnectivity. Public streets will consist of Master Plan streets and additional business and residential street to form blocks that with the exception of Area D defined on the Development Plan, are substantially similar to the street system shown in the Road Hierarchy Plan of the Development Plan and that are subject to M-NCPPC and DPWT approvals.

5. Area D Street Network

Area D will be designed with a public or private street connecting First Avenue (Route A-304) and Newcut Road (Route A-302) in a grid pattern with a particular emphasis on a building line to from Newcut Road Extended, with parking in the rear.

6. Street Character

All streets will adhere to a pedestrian-friendly design to the extent practicable, which places emphasis on a building line to frame the street with parking in the rear, excluding retail and entertainment uses. Within the core, pedestrian friendly uses including retail, residential or office will be located on the first floor. The entire MXPDP area will conform to a Cabin Branch Community Streetscape Plan that is designed to integrate the entire community, which will be submitted at Site Plan and is subject to M-NCPPC and DPWT approvals.

7. Special Roads:

A-307 will be designed as an open arterial road with wide green edges to provide a gateway to Black Hill Regional Park, subject to M-NCPPC and DPWT approvals. The rural character of West Old Baltimore Road will be maintained by minimizing environmental impacts and providing generous green edges.

8. Service Public Uses

Service/Public uses may include up to 500 units for independent living for Senior Adults or persons with disabilities, assisted –living, life care or continuing care.

9. On-street Parking

The owner will include on-street parking on streets adjacent to retail facilities, excluding MD 121, Wellspring Street and Goldeneye Avenue.

10. No single retail store will have a gross floor area that is greater than 50,000 sf, which will be a condition of the Site Plan approval and will be referenced on the Certified Site Plan.

11. The retail uses located in Area C will be neighborhood retail.

**STATEMENT OF JUSTIFICATION****CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH  
NORTH DISTRICT, PHASE 1****Southwest Corner of Clarksburg Road and Interstate-270  
Clarksburg, Maryland****Application for Site Plan #820140160  
Limited Plan Amendment, Preliminary Plan #12003110C****October 1, 2014****I. Introduction**

This application was filed by Simon/ Clarksburg Development, LLC (the "Applicant") on May 7, 2014. It seeks Site Plan approval and a Limited Preliminary Plan Amendment for property located in the southwest quadrant of the intersection of Interstate 270 and Clarksburg Road (MD 121) in Clarksburg, Maryland (the "Property"). The Site Plan proposes retail/restaurant use and the Limited Preliminary Plan Amendment is to revise the approved Water Quality Plan as required by new County Stormwater Management regulations enacted subsequent to the Preliminary Plan approval.

**II. Property Description****a. Site Location and Characteristics**

The Property is an 84.24 acre portion of a larger tract of land zoned MXPDP, totaling 283.5 acres, including prior road dedications, and is bounded by I-270 to the east, Clarksburg Road to the north, a planned roadway to be known as "Cabin Branch Ave" to the west and Old Baltimore Road to the south. The MXPDP tract, in turn, is part of an even larger 540 acre tract known as Cabin Branch which is under development as a major, mixed-use office/retail/hotel/residential project.

a. Zoning and Permitted Uses

The Property was zoned MXPDP (Mixed Use Planned Development) in 2003 consistent with the 1994 Clarksburg Master Plan. Under the new Zoning Ordinance and District Map Amendment (DMA G-956), the property has been rezoned to CRT 0.5 (C-0.25, R-0.25, H-130T). This application was filed under the provisions of Section 7.7.1B, providing for its review under the preexisting MXPDP zone standards. Section 59-C-7.13 of the Montgomery County Zoning Ordinance indicates that the MXPDP zone allows all types of residential uses, as well as various commercial and service uses by-right, which allows the proposed use.

b. Existing Conditions

The Property is currently undeveloped and up until recently, was being used for farming activities. The Property is served by public water and sewer. Roads, and utilities to serve the Property either have been constructed or are under construction at this time.

b. Surrounding Zoning and Land Uses

The surrounding neighborhood is most appropriately characterized as mixed-use, including: residential, office, hotel and retail uses.<sup>1</sup> Specifically, zoning and land uses in the immediate vicinity include:

North: Directly adjacent to the Subject Property, located north of relocated Clarksburg Road, is the Gosnell Property. The Gosnell Property also is zoned MXPDP and is approved for a

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<sup>1</sup> Although the surrounding area is predominately undeveloped farmland, the Zoning Hearing Examiner appropriately determined that the surrounding neighborhood should be characterized by the approved uses, rather than the existing conditions. As such, the Zoning Hearing Examiner concluded that the neighborhood is characterized by a mix of residential uses, limited farming activity and employment uses (Zoning Hearing Examiner Report, DPA 13-02, page 11-12).

hotel and small “highway commercial” development, including: a bank, gas station, car wash and convenience store.

South: South, across Old Baltimore Road, are single-family residentially zoned properties (RE-1 and RE-2). The Black Hill Regional Park also confronts the Property.

East: The COMSAT and Gateway I-270 Business Center are located across I-270, in the I-3 Zone.

West: The western portion of the Cabin Branch Neighborhood is zoned RMX-1/TDR and is residential in character. All properties within this area have received site plan approval for construction of single-family detached homes and townhouses and homes are now under construction. Additionally, the western portion of the Cabin Branch Neighborhood will include an elementary school and additional recreational areas.

### **III. Prior Approvals**

On September 9, 2003, by Resolution Number 15-326, the Montgomery County Council, sitting as the District Council (the “District Council”) approved Local Map Amendment (“LMA”) Number G-806 to reclassify the eastern half of the Cabin Branch Neighborhood to the MXPD Zone, while leaving the remaining western half of the Cabin Branch Neighborhood (approximately 257 acres) in the RMX/TDR zone. Subsequently, on April 3, 2008, the Montgomery County Planning Board (the “Planning Board”) approved Preliminary Plan Number 12003110B for a total of 1,886 residential dwelling units, 2,420,000 square feet of unspecified commercial space and 500 dwelling units for senior housing for Cabin Branch. An Infrastructure

Site Plan (No. 820050150) and Final Water Quality Plan (No. 820050150) also were approved on June 14, 2007 for the entire Cabin Branch neighborhood.

On February 4, 2014, with the support of the Clarksburg community, M-NCPPC Staff, the Planning Board and the Zoning Hearing Examiner, the District Council approved Development Plan Amendment No. 13-02 ("DPA") to allow for a multi-phase mixed-use development within the subject MXPB zoned area of Cabin Branch. This DPA included the uses proposed in this Site Plan. Specifically, DPA 13-02 approved up to 484,000 square feet of retail (for an outlet retail center and neighborhood retail) as well as 1,936,000 square feet of office use, 1,139 dwelling units, 500 senior dwelling units, and 75,000 square feet of public uses. In furtherance of the Prior Approvals, the Applicant has submitted this Site Plan application for the retail/restaurant uses approved in that DPA.

The Applicant worked with Staff extensively during the approval of the referenced DPA. Immediately upon approval of the DPA, Applicant and Staff began working together to implement the DPA and to address all anticipated Site Plan issues. Even before filing the application, Applicant attended six meetings with Staff and then participated in eight additional meetings to refine every aspect of the Plan. The subject Application reflects that highly collaborative process that has enabled a major Site Plan to be brought to the Planning Board only six months after its filing.

#### **IV. Project Description**

The Applicant is seeking Site Plan approval for the first phase of a multi-phased mixed-use development, which will implement a portion of the overall development approved by DPA 13-02, also known as part of Area A and Area B. The proposed Phase I development on the

Property will include 13,000 square feet of restaurant, and 437,000 square feet of retail space and other commercial uses (now known as “Clarksburg Premium Outlets”). This development, combined with the overall Cabin Branch neighborhood, will create a vibrant and successful mixed-use planned community and will provide: significant public amenities and green area; pedestrian and bicycle paths; arts and culture venues; recreational opportunities; a mix of employment, retail and restaurant uses; and diverse housing opportunities.

This Site Plan application will cover Phase I of the proposed Project, and the remaining phases will be filed as a separate Site Plan in the future when that development is ready to proceed. Phase I will provide a significant amount of public open space and amenities. An outdoor amphitheater will be provided near the southern portion of the Property, adjacent to the stream valley buffer and forest conservation area. The stream valley buffer will provide extensive open space and will serve as an important organizational feature of the entire Project. Additionally, the proposed outlet center has been designed to include three large public courtyards that will provide attractive gathering locations. These courtyards will contribute to the sense of place and provide meaningful space for social interaction among residents and visitors.

#### **a. Architecture and Design**

Clarksburg Premium Outlets at Cabin Branch takes advantage of the existing topography to create a unique two level outlet shopping center, which is unprecedented in an outlet center in the United States. That topography drops approximately 100 feet from MD Route 121, to the low point in the stream buffer area at the southern side of the site. This topography allows the retail center to be “benched” into the dropping grade and for the parking areas to be largely

screened from views along I-270 and MD Route 121. Using this grade change, starting in the northernmost part of the Cabin Branch site, two double sided retail pedestrian promenades step down the hill toward the extensive stream valley buffer and forest conservation area. The buildings emphasize the pedestrian experience by creating three pedestrian plazas at the southern edge of the shopping area that serve as the pedestrian entry points from the main parking areas. Thus they are shielded from traffic on I-270 and enjoy views to the south into and over the extensive stream valley system farther to the south.

The shopping area features gently curving, double sided pedestrian shopping promenades punctuated by attractive landscape and hardscape features. The pedestrian ways will create a European village feel with wide promenades, varied store facades on both sides, and shade canopies. Courtyards will be designed as enjoyable, urbane gathering spaces with benches, planters, water features, children's play structures, and views to the conservation area.

The architecture of the Clarksburg Premium Outlets at Cabin Branch is distinctive but also designed to complement the character of the residential portion of Cabin Branch. The facades and exterior walls feature multiple horizontal and vertical elements. In addition, the elevations are comprised of varied materials, colors and textures, responding to the overall context and well as adjacent uses. The base building will be designed to be LEED Certified rated, meeting or exceeding County green building requirements. Though not required by the LEED criteria, the development will feature solar panels to supply electricity to the food court. A variety of roof and cornice lines add to the distinctive architectural expression to create a lively, welcoming and fun environment.

## **b. Parking and Loading**

The Phase I parking has been designed with a maximum amount of green area. Whereas the Zoning Ordinance requires 5% green area for parking lots, the two main surface parking lots at Cabin Branch will have over 25%. The green area will consist of perimeter plantings and extensive internal islands. The internal islands will include Environmental Site Design features to manage stormwater. Both lots will be well screened from external view by the retail building itself, the sloping grade which runs down and away from MD Route 121, a decorative stone wall along MD Route 121, perimeter plantings and other features. Additionally, the Western lot will be terraced along with the grade dividing it into two smaller components. Each lot will have pedestrian connections through the green islands in order to provide convenient pedestrian access. As subsequent phases of the project are developed, the Phasing Plan provides for replacement of these surface parking areas with office, commercial and residential buildings served by structured parking facilities.

The Applicant is requesting a waiver of the standard parking requirements in order to provide for a reduced number of parking spaces. Section 59-E-3.7 requires regional shopping centers to provide 5.5 spaces per 1,000 square feet of gross leasable area and restaurants to provide 25 spaces per 1,000 square feet of patron area, and 15 parking spaces per 1,000 square feet of outside patron area. (Note, the new Zoning Ordinance reduces the parking requirement for restaurants). Under the current Ordinance, 2,269 parking spaces are required for the Phase I regional shopping center and 162 spaces for the restaurants. In connection with this Site Plan, pursuant to the waiver provisions of Section 59-E-4.5, the Applicant is seeking a modest parking reduction from the required parking by approximately 5%. The parking reduction will ensure an appropriate amount of parking is provided on site while, at the same time, avoiding any adverse

visual and environmental impact of excess parking. With this reduction, the proposed project will provide a total of approximately 2292 parking spaces.

### **c. Vehicular and Pedestrian Connections**

The overall development will include an extensive transportation network, designed to accommodate various modes of transportation. The Plan itself, and the surrounding Vicinity Plan, make it clear that excellent road access will be provided to the Project. As noted, the site is immediately adjacent to the I-270/MD Route 121 interchange. As part of the Cabin Branch project, that entire interchange is being rebuilt to add capacity, and MD Route 121 is being relocated and expanded along the frontage of the Property. Additionally, the Project is providing for a bus pick-up and drop-off immediately at the “front door” of the center. This will include not only suitable roadway access and turning areas, but also a bus shelter. Initially, this will provide for four buses and eventually can serve as a Ride-On bus location once there is sufficient ridership in the area to support Ride-On service. Additionally, the Project will include connections to the extensive bicycle and pedestrian network being built in the Cabin Branch project and will also connect to a bike trail being constructed along MD Route 121, across the new interchange bridge, connecting to the Clarksburg communities on the east side of I-270. Working with Staff, Applicant has agreed to provide bicycle parking for 218 bicycles, including bike lockers, a bike repair station and showers for bicyclists. The Project also will include charging stations for electric vehicles

The Comprehensive Transportation Review performed originally when the Project was approved has been updated with plans for the outlet center and the calculations show that all

County standards will be met and that the transportation system will appropriately serve the Project. (Documentation of the traffic analysis both on and off-site has been provided).

## **V. Master Plan Conformance**

The Property is within the boundaries of the Approved and Adopted *1994 Clarksburg Master Plan and Hyattstown Special Study Area* and the *2011 Limited Amendment to the 1994 Clarksburg Master Plan and Hyattstown Special Study Area*. (The recent Ten Mile Creek Amendment made no changes for this property). An overarching goal of the Master Plan is to create pedestrian-oriented neighborhoods, surrounded by open space. The Master Plan contains ten planning policies that guide the vision for Clarksburg: (1) Town Scale of Development; (2) Natural Environment; (3) Greenway Network; (4) Transit System; (5) Hierarchy of Roads and Streets; (6) Town Center; (7) Transit- and Pedestrian-Oriented Neighborhood; (8) Employment; (9) Farmland Preservation; and (10) Staging. In recommending approval of the DPA, Park and Planning Staff (Report dated July 18, 2013), the Planning Board, the Zoning Hearing Examiner (Report dated December 9, 2013) and the District Council (Resolution dated February 4, 2014) all found the proposed development to be in compliance with the Master Plan. This Site Plan reflects that compliance as follows:

### **a. Town Scale of Development**

The overall development proposed on the Property is within with the density recommended in the Master Plan (i.e. 2,420,000 square feet of commercial use). The retail and employment uses proposed in connection with Phase I will serve the needs of both local residents and those attracted to the area. To ensure compatibility, the more intense uses will be

located along I-270, away from the less intense, residential uses on the western portion of the Property.

b. Natural Environment

The proposed Project has been specifically designed to incorporate and protect the surrounding natural environment, consistent with the recommendations of the Master Plan. A large stream buffer exists on the Property, and runs parallel to I-270 along the eastern portion of the Property. The Project will provide a minimum stream buffer of 175 feet. The Applicant recognizes the environmental significance of the stream valley buffer and accordingly, has incorporated the stream valley as a central organizing feature of the Project. (The forest conservation area actually is being expanded beyond that previously approved for the site and will protect these resources even better). Additionally, a majority of the forest on-site is located within the stream buffer and therefore, the Project will provide significant forest conservation.

The development will satisfy all forest conservation and stormwater management requirements and provide Environmental Site Design (ESD) practices. The proposed Project will incorporate other principles of sustainable design to reduce the impact of development on the environment, including: place making; a mix of uses that provide the ability to live, work, shop and play in the community; pedestrian and cyclist amenities; a connected, multi-modal transportation network; and incorporation of various environmental protections and green building principles. (See also, Section VI a viii below).

c. Greenway Network

The proposed Project will incorporate an extensive network of on-site paths and trails and provides an opportunity to greatly improve connectivity. Phase I of the Project will provide

internal connections between the stream valley buffer, amenity areas, restaurants, parking and retail shops, as well as surrounding properties and the larger Clarksburg area.

d. Hierarchy of Roads and Streets

The Project's visibility and accessibility to I-270 will contribute to the success of this development as a world-class employment center. The proposed development includes an extensive transportation network that promotes multi-modal transportation, including internal streets, sidewalks, trails, bicycle paths and bus routes. Sidewalks will be provided along all internal streets and will be separated from the right-of-way by landscaping, to improve pedestrian safety. Additionally, the Project will provide dedicated bike lanes, both on-street and off-street.

The approved Preliminary Plan provides for the dedication of necessary right-of-way and phasing of on-site and off-site transportation improvements in connection with proposed development on the Property. This Project will meet or exceed those road phasing requirements.

a. Town Center

The proposed development will complement the neighborhood retail in the Town Center, as recommended in the Master Plan. The retail provided by the Project will not compete with the neighborhood serving retail envisioned in the Town Center. Rather, as determined during approval of the recent DPA for the Project, the development on the Property will serve as a catalyst for development in the surrounding neighborhoods. Furthermore, because a grocery store is identified as an important component of the Town Center, the proposed Project will not include a full size grocery store.

b. Transit- and Pedestrian-Oriented Neighborhood

To create the transit- and pedestrian-oriented neighborhoods envisioned in the Master Plan, a mix of uses is recommended in each neighborhood, including: retail, employment, open spaces, schools and housing. The Project provides a comprehensive mixed-use development that includes employment, entertainment, retail, and residential uses, as well as significant public amenities, all within walking distance. These uses, in combination with the residential and civic uses proposed in the western half of the Cabin Branch Neighborhood, will serve the needs of the residents and provide a unique opportunity to live, work and play in the community.

The Project has been designed to provide connections among the entire Cabin Branch Neighborhood, as well as to surrounding properties. The intricate, connected network of streets, bikeways, and paths within the Project will promote alternative modes of transportation and reduce automobile dependence. Additionally, bus access to the future Corridor City Transitway and the MARC Commuter Rail Station will be accommodated along the internal streets. These bus routes will also connect the Project with other properties outside the Cabin Branch Neighborhood.

c. Employment

The Master Plan identifies The Cabin Branch Neighborhood as a location for employment uses, because of its extensive frontage on I-270. The 283.5 acre site provides extensive employment opportunities and the proposed outlet center will serve as a catalyst for additional employment uses. The overall project provides opportunities for a broad employment base, including: office, R&D, light industrial, hotel, urgent care facilities, professional services, retail and entertainment uses. The overall Project has been designed to create a sense of place,

by providing the mix of uses, public amenities and open spaces that modern office tenants desire and demand.

## **VI. Zoning Ordinance Conformance**

### a. Purpose

Section 59-C-7.50 of the Zoning Ordinance provides that the general purpose of the MXPDP Zone is to encourage and facilitate development of large-scale, comprehensively planned, mixed-use centers. The MXPDP Zone is intended to provide a flexible framework for development of comprehensively planned communities. The MXPDP Zone covers 283.5 acres of the Cabin Branch Development, of which 84.24 acres constitute the first, outlet, retail, phase. The specific purposes of the MXPDP Zone are as follows:

*i. To establish standards and procedures through which the land use objectives and guidelines of approved and adopted master or sector plans can serve as the basis for evaluating an individual multi-use center development proposal.*

As discussed in Section V above, the proposed development complies with the recommendations and objectives of the Master Plan.

*ii. To encourage orderly, staged development of large-scale, comprehensively planned, multi-use centers by providing procedures for the submission of a concept plan for an entire site and subsequent development plans for each stage of development, as identified on the concept plan.*

The 283.5 acre portion of Cabin Branch has been planned as a mixed-use center since the original Development Plan was approved along with the MXPD zoning. The approved Development Plan Amendment (13-02) follows this plan for the orderly and staged development of this 283.5 acre site and regulates the mix of uses, land use quantities and proposed sequence of development on the Property beginning with the outlet retail and restaurant space. Additionally, as shown on the DPA, the Applicant will coordinate development on the Property with the other property owners in the western portion of the Cabin Branch Neighborhood.

*iii. To provide, where appropriate, higher density residential uses integrated into the overall multi-use center*

The overall Project will include two future multi-family nodes and senior units. This residential development will be provided in close proximity to the retail, office, employment and service uses proposed on the Property. Additionally, as previously discussed, the proposed development will include an extensive transportation network, with a variety of mode options, to link different areas of Cabin Branch, as well as to provide a connection to other nearby properties.

*iv. To ensure internal compatibility of residential and nonresidential uses by providing a suitable residential environment that is enhanced by the commercial, recreational, employment and institutional amenities within commercial and industrial components of the multi-use center.*

The proposed Project provides a mix of uses that will create an attractive and sustainable community. The proposed uses and site design will create a sense of place and identity for Cabin Branch that is attractive to all users of the site. The proposed outlet center will serve both local

and regional retail demand and the proposed amphitheater will provide additional recreational opportunities, for both residents and visitors.

- v. *To assure compatibility of the proposed land uses with surrounding uses by incorporating higher standards of land planning and site design than could be accomplished under conventional zoning categories.*

As discussed above, the proposed Project has been extensively planned to create a vibrant mixed-use community. The Development Plan Amendment prescribes the ultimate mix of uses, land use quantities, proposed transportation network and significant public use space for the entire 283.5 acre Property. This level of detail provides a much higher standard of planning and site design, than could be accomplished under conventional zoning. Additionally, the site has been specifically designed to ensure compatibility with the surrounding uses -- the more intense development will be located closest to I-270.

- vi. *To encourage and provide for open space not only for use as setbacks and yards surrounding structures and related walkways, but also conveniently located with respect to points of residential and commercial/ industrial concentration so as to function for the general benefit of the community and public at large as places for relaxation, recreation, and social activity. It is also intended that open space and amenities be located so as to achieve the physical and aesthetic integration of the uses and activities within each development. In addition, structured parking within mixed-use planned development is*

*encouraged to help achieve the open space and amenities objectives of the zone. Where surface parking is necessary, the purpose of this zone may be achieved by the provision of additional landscaping.*

The proposed development includes parks, trails and open spaces on-site, which offer the opportunity for active and passive recreation, relaxation and social activity. The Project provides both physical and visual connections among the mixed-use development and the stream valley. Additionally, the proposed development will provide connectivity to Black Hill Regional Park and the various uses on site.

The overall development includes both structured and surface parking. Phase I will provide surface parking, however, a majority of the surface parking provided will ultimately be replaced by structured parking and the office and entertainment uses proposed in connection with later phases. The surface parking will be landscaped and has been designed to take into consideration the unique site features and topography. Additionally, landscape buffers will be provided along Clarksburg Road to visually screen the parking areas from the right-of-way.

*vii. To encourage and provide for the development of comprehensive non-vehicular circulation networks, separated from vehicular roadways, which constitute a system of linkages among residential areas, open spaces, recreational areas, commercial and industrial areas and public facilities.*

As part of the proposed development, an intricate network of streets has been designed to provide internal connections between the various uses on the Property. The internal streets have

been designed to encourage pedestrian activity – all streets within the proposed development will include wide landscaped sidewalks.

Additionally, as previously mentioned, the proposed development includes greenways, and bike paths throughout the site. These paths will facilitate pedestrian and cyclists connections between the various uses on-site and among the larger Cabin Branch Neighborhood.

*viii. To encourage and provide for efficient use of energy resources through shared facilities or other economies of scale or technology, including innovative fuels and district heating, etc.*

The proposed development will incorporate sustainable design and green building practices including, but not limited to: solar power, specification of local and recycled building materials, incorporation of energy efficient appliance, HVAC/ mechanical equipment and bathroom fixtures, improved building insulation, dual glazed storefront windows, and electric vehicle charging stations. Additionally, the three main courtyards and buildings have been oriented to maximize their southern exposure, in order to capitalize on natural daylighting. (A more complete list of the extensive environmental and energy conservation features of the Project is attached).

*ix. To preserve and take the greatest possible aesthetic advantage of existing trees and to minimize the amount of grading necessary for construction of a development.*

A Forest Conservation Plan, Water Quality Plan and Stormwater Management Plan have been approved for the entire Cabin Branch Neighborhood. Revised Preliminary and Final Water

Quality Plans incorporating new Environmental Site Design features also have been approved by the Departments of Permitting Services. The proposed development will comply with the requirements of the aforementioned approvals. The “high street/low street” 2 level design reduces grading of the site and takes advantage of the natural slope.

b. Development Standards

Section 59-C-7.5 of the Zoning Ordinance sets forth the development standards for the MXPDP Zone. The proposed Project satisfies these development standards:

- i. *No land shall be classified in the MXPDP Zone unless it is recommended for mixed-use development within an approved and adopted master plan.*

The Master Plan recommended that the eastern portion of Cabin Branch be zoned MXPDP. As such, in 2003, pursuant to LMA G-806, 283.5 acres of Cabin Branch was rezoned MXPDP to allow for a comprehensive, mixed-use planned development on the Property and DPA 13-02 confirmed the zoning and this specific use.

- ii. *Land in the MXPDP Zone must contain a minimum of 20 acres.*

The Property has a total gross tract area of 283.5 acres and as such, greatly exceeds the minimum area requirement.

- iii. *Land in the MXPDP Zone shall be located adjacent to and readily accessible from existing or planned major highways or limited access*

*freeways. It is intended that adequate access be available to such sites so that traffic does not have an adverse impact on the surrounding area.*

The Property is prominently located at the intersection of I-270 and Clarksburg Road and has a significant amount of frontage on I-270, a major limited access freeway. Access to the proposed Project will be provided off of Clarksburg Road, just west of the I-270 interchange.

*iv. The MXPDP zone permits residential and commercial uses in accordance with the standards set forth in 59-C-7.52- 7.54.*

As previously mentioned, the MXPDP Zone permits, by-right, all types of residential uses. No residential uses are proposed as part of Phase I, however, the residential uses proposed in connection with later Phases are consistent with the recommendations of the Master Plan.

The MXPDP Zone also permits all permitted and special exception commercial uses allowed in the C-2 and H-M zones. Phase I includes 13,000 square feet of restaurant and 437,000 square feet retail use on the Property – restaurant and retail uses are allowed by-right in the MXPDP Zone. The proposed outlet center will provide significant employment opportunities and will serve as a catalyst for additional office/ employment development on the Property.

The proposed residential and commercial densities comply with the requirements of the MXPDP Zone (see Development Table for detailed information). The proposed 484,000 square feet of retail use is within the approved total of 2,420,000 square feet of commercial uses. Only 20 percent of the overall commercial development is proposed for retail use, in conformance with the requirements of the MXPDP Zone.

- v. *All uses shall be compatible with the existing and proposed uses in the vicinity.*

The proposed development was specifically designed to promote compatibility with surrounding land uses – as previously mentioned, the more intense uses have been located at the intersection of I-270 and Clarksburg Road and the less intensive, residential uses have been located along the western edge of the Property. The proposed Project will comply with the setback and height requirements of the MXPDP Zone.

- vi. *Minimum green area and amenity requirements*

The proposed Project will satisfy the green area and amenity requirements of the MXPDP Zone. The overall Project will provide more than 250 acres of green space, including 130 acres within the MXPDP zoned area. This will exceed the combined residential and non-residential green area requirement for the zone.

- vii. *Public facilities shall be provided as recommended in the Master Plan and all utility lines shall be placed underground. Adequate street lighting shall be provided.*

Numerous approvals have been obtained for infrastructure and site development for the larger Cabin Branch Neighborhood. The proposed Project will comply with the requirements of Chapter 19 of the Montgomery County Code, regarding water resource protection, and Chapter 22A, regarding forest conservation.

The public facilities are more than adequate to accommodate the proposed Project, as demonstrated by the valid APF approval. The traffic generated by the proposed development

will not exceed the congestion levels for the policy area or the trips approved in the preliminary plan.

All utility lines in the Project will be placed underground.

*viii. Off- street parking shall be provided in accordance with the provisions of Division 59-E of the Zoning Ordinance and shall be appropriately sized and landscaped.*

Section 59-E-3.7 requires regional shopping centers to provide 5.5 spaces per 1,000 square feet of gross leasable area, and restaurants to provide 25 spaces per 1,000 square feet of patron area, and 15 parking spaces normally per 1,000 square feet of outside patron area. Accordingly, 2,269 parking spaces are required for Phase I regional shopping center and 162 spaces for the restaurants. However, in connection with this Site Plan, pursuant to the provisions of Section 59-E-4.5, the Applicant is seeking a modest parking reduction from the required parking by approximately 5 percent. The parking reduction will ensure an appropriate amount of parking is provided on-site while, at the same time, avoiding any adverse visual and environmental impact of excess parking. If the reduction is approved, the proposed project will provide a total of approximately 2292 parking spaces.

The surface parking will be appropriately landscaped and will include bio-retention stormwater management facilities throughout. The bio-retention facilities will provide quantity and quality treatment of stormwater management on-site.

## VII. Findings Required for Site Plan Approval

Section 59-C-7.593 requires Site Plan approval for developments in the MXPB Zone, in accordance with the provisions of section 59-D-3. As described in the following Section, the proposed Project satisfies each of the requirements listed in Section 59-D-3.4 which governs the Planning Board's approval of a Site Plan application

- a. The site plan conforms to all non-illustrative elements of a development plan or diagrammatic plan, and all binding elements of a schematic development plan, certified by the Hearing Examiner under Section 59-D-1.64...*

The Site Plan conforms to all binding elements and non-illustrative elements of the recently approved Development Plan Amendment (13-02). The proposed retail and restaurant uses, and their associated density, are consistent with the approved DPA. The specific binding elements for the Development Plan Amendment are attached hereto for reference as Exhibit A.

The approved DPA states that the "building locations, footprints, parking locations and other design details will be finalized during subsequent subdivision and site plan processes." (see General Notes of DPA 13-02). The proposed Project will also conform closely with the illustrative drawings submitted as part of the DPA, such as the site access, location and layout of the outlet mall.

- b. The site plan meets all of the requirements of the zone in which it is located, and where applicable conforms to an urban renewal plan approved under Chapter 56*

Section VI of this Statement describes the compliance of the proposed Project with the requirements and standards of the MXPDP Zone. The Property is not subject to an urban renewal plan approved pursuant to Chapter 56.

- c. The locations of buildings and structures, open spaces, landscaping, recreation facilities, and pedestrian and vehicular circulation systems are adequate, safe and efficient*

The Site Plan has been designed to ensure that the locations of buildings and structures, open spaces, landscaping, recreation facilities, and pedestrian and vehicular circulation systems are adequate, safe and efficient. The pedestrian and vehicular circulation systems, in particular, have been carefully planned and designed to promote the safe and efficient movement of both pedestrians and vehicles. The Project will provide sidewalks throughout the community, with landscaped buffers for pedestrian safety. With regard to vehicles, as previously discussed, the proposed access off of Clarksburg Road, and expanded external and internal road network, will ensure safe and efficient vehicular access.

- d. Each structure and use is compatible with other uses and other site plans, and with existing and proposed adjacent development*

In connection with the Prior Approvals, the Montgomery County Council and Planning Board have determined that the proposed Project will be compatible with the surrounding uses at this location. The site has been strategically designed to promote compatibility with the surrounding uses – the most intense uses will be located closest to I-270. Additionally, the proposed mixed-use planned development will complement the existing and planned residential uses in the immediate vicinity and will provide residents with the ability to shop, work and play

near where they live, consistent with the Master Plan's goal of creating a pedestrian-oriented community.

- e. The site plan meets all applicable requirements of Chapter 22A regarding forest conservation, Chapter 19 regarding water resource protection, and any other applicable law.*

The Property is subject to the Forest Conservation Ordinance. The amended Forest Conservation Plan, for the entire Cabin Branch Neighborhood, was most recently approved on May 22, 2014 (82005015 E) and September 11, 2014 (82005015 F). This forest area will be protected (and actually expanded). The Department of Permitting Services ("DPS") approved the overall concept Stormwater Management Plan in 2004. The revised Stormwater Management Plans meets required stormwater management goals by using Environmental Site Design. As required by a condition of approval of the DPA, the Applicant submitted a revised Preliminary Water Quality Plan and Final Water Quality Plan concurrently with this Site Plan application and MC-DPS has approved them.

A Sediment and Erosion Control Plan will be prepared and submitted to DPS for approval prior to commencement of construction on the Property.

## **I. Conclusions**

As demonstrated by this Statement, the Project complies with all applicable requirements of the Zoning Ordinance that govern development under the MXPB Zone. Furthermore, the Project is in conformance with the recommendations of the Master Plan, as determined by the Zoning Hearing Examiner and County Council in connection with DPA 13-02, and as discussed

above. The Project will provide a much desired mixed-use planned community in Clarksburg, and will serve as a catalyst for future development. The mix of uses and amenities provided in Phase I will be the linchpin for a world class 21st century mixed-use community in Clarksburg.



# Washington Suburban Sanitary Commission

14501 Sweitzer Lane • Laurel, Maryland 20707-5901

## COMMISSIONERS

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## GENERAL MANAGER

Jerry N. Johnson

July 31, 2014

Ms. Sylke Knuppel  
Cabin ranch Management, LLC  
6905 Rockledge Drive, Suite 800  
Bethesda, MD 20817

Re: Amended Phase I Letter of Findings #7, WSSC Project Nos. DA3326Z02 and DA5636Z13 Cabin Branch

Dear Ms. Knuppel:

The letter of findings for the Cabin Branch project has been amended per your request. This amendment **supersedes** any previous letter of findings and amendments. The following changes have been approved:

- change in number of units or type of development,
- change in construction sequence,
- change in water and sewer main alignments, and
- change in number or configuration of parts of the project.

Please refer to the three updated 200'-scale sketches enclosed along with the summary table and an all-inclusive list of project conditions provided below:

<b>HYDRAULIC SUMMARY TABLE</b>	
Proposed Development: 720 Single Family Dwelling Units, 722 Town Homes, 798 Garden Apartments, 1 Elementary School, 79,775 sq. ft. of Hotel, 1325 seats in Restaurants, 7725 sq. ft. of Bank, 3255 sq. ft. of Gas/Repair Shop, 1800 sq. ft. Car Wash, 3 Club Houses with Pools ( total 1930 members), 429500 sq. ft. of Retail, 283,450 sq. ft. of Parking Garage, 100-seat Amphitheater, 1,873,900 sq. ft. Office	
200-ft Sheet: 230NW11-14, 231 NW 13-14, 232NW14	
<b>SEWER</b>	<b>WATER</b>
WWTP Service Area: Seneca Creek	Hydraulic Zone Group: Montgomery High Zone
Mini-Basin Number: 15-080, 15-081, 15-082	Pressure Zones: 760 A, temporary 760B
	High Grade: 772 feet Temporary Service: 772 Feet
	Low Grade: 720 feet Temporary Service: 720 feet

Ms. Sylke Knuppel  
 July 21, 2014  
 Page 2

The following is a list of conditions that apply to this project and must be met before a Systems Extension Permit (SEP) will be issued.

CONDITIONS FOR SERVICE

The following conditions and service requirements must be met prior to providing service to any unit in this project:

- Provide the deed to the WSSC acceptable land size for Clarksburg tank site having appropriate and acceptable access easement to the site. *Cabin Branch Management submitted a deed on March 24, 2013 for the Water Tower Parcel and Access easement. WSSC rejected both legal documents since the deed and easement did not meet WSSC requirements; also, WSSC tank site is located on parcels with ownership that will potentially change in near future. On April 29, 2013, WSSC agreed, in good faith, to allow Cabin Branch Management to develop Parts 6, 9, and 10 while the terms of “acceptable” land size and easements are agreed on. Additional units cannot receive service until all outstanding issues related to tank site deed and access easement are resolved. A new package for the deed and easement received on September 9, 2013 and is currently under review and negotiations.*
- Construct and place in-service, the following needed infrastructures for service:
  - i. Part 4, the proposed 16-inch water main, contract no. DA3326H02, connecting to the existing 16-inch water main (Contract No. DA8774B90). *This main is already constructed and mains are in service.*
  - ii. The required pressure reducing and relief valves, vault and all associated components in Part 4. *The valve and all components are in service.*
  - iii. Part 3, the proposed water and sewer mains, Contract No. DA3326D02. *This main is released for service.*
  - iv. Part 2, proposed water mains along West Old Baltimore Road contract no. DA3326C02 (part of CIP W-46.24); this main connects the proposed main in Part 3 (Contract No. DA3326D02) to the exiting 24” main along West Old Baltimore Road, (Contract No. DA3267A02). *This main is released for service.*

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Once terms of the deed and easement documents are finalized and the above infrastructures are in place, this project can move forward to build up to 0.2 million gallons per day (mgd) of domestic demand. See attached Approved "Exhibit" for construction sequence up to 0.2 mgd. Development beyond 0.2 mgd will require the construction of the 24" water main (Contract No. BL3326B02, part of CIP W-46.24) known as Part 28 and Part 60, the water alignment shown on Sheet 2 of the sketch. This main connects the existing 16" main (Contract No. 90-8724B) along MD Route 355 (Frederick Rd) near Milestone Manor to the existing 20" main (Contract No. 95-1313C) along West Old Baltimore Road. Portions of the 24" main are constructed during MD State and Montgomery County road reconstruction projects. Segments of the 24" main not covered under state and county projects can be constructed either by Cabin Branch Development to meet the development schedule or by WSSC when necessary to support developments in 760 pressure zone. Once the water main under contract No. BL3326B02 is constructed, the WSSC will build the tank as deemed essential to support developments in the area.

#### PART 37- HYDRAULIC ANALYSIS REQUIRED

Development plans for Part 37 of this project are not clear at this time. Currently, there is a placeholder for the sewer flow and water demand for these parts based on the information on the sketch. Water and sewer sizes may change or system fixes may be necessary as development for this part is more defined. A hydraulic amendment must be submitted for Part 37 once more information becomes available.

#### MANDATORY REFERRAL PROCESS

This project may be subject to the Maryland-National Capital Park and Planning Commission's Mandatory Referral Program, depending on its planned water / sewer infrastructures and associated appurtenances. It is the Applicant's responsibility to contact the appropriate County's Department of Park and Planning for specific guidance and their standards for Mandatory Referral Review. During Phase 2 Design Review, WSSC must be notified, if the project is subject to the Mandatory Referral Process.

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## ***SANITARY SEWER CONDITIONS***

### **REQUIRED SANITARY SEWER MAIN SIZES**

The extent of gravity sewer pipe diameters 10 inches and greater is shown on the attached sketch. All other sewers are to be 8-inch diameter gravity sewer.

Ten-inch diameter, or larger, sewers are required. The table below shows the design flow and **maximum** diameter for the segments indicated on the sketch.

Segment	Design Flow (mgd)	Assumed Minimum Slope (%)	Maximum Diameter (inches)
C-D	3.83	0.14	21
D-D'	3.95	0.12	24
D'-E	4.2	0.12	24
E-F	4.53	0.12	24
L-M	1.064	0.34	12
M-N	1.16	0.34	12
N-O	1.4	0.24	15
O-P	1.5	0.24	15
W'-W	0.67	0.46	10
W-X	0.84	0.46	10
X-Y	1.4	0.24	15
Y-S'	2.24	0.19	18
S'-S	2.3	0.19	18
Z'-Y	0.7	0.46	10
T-U	0.65	0.46	10
U – WWPS (Part 39)	0.87	0.46	10
Q-R	0.89	0.46	10
R-S	1.2	0.34	12
S-H'	3.3	0.14	21
H'-H	3.6	0.14	21

### **WASTEWATER PUMPING STATION REQUIRED**

A wastewater pumping station is required to be built by the applicant to serve parts of this property. The station should be designed for a peak flow of 0.9 million gallons per day. The preliminary size of the force main is 10 inches in diameter. The force main size will be finalized during the design phase.

### **CAPITAL IMPROVEMENT PROGRAM (CIP) PROJECT REQUIRED**

This project has been designated as a “major project” since a wastewater pumping station, force main and over 10,000 feet of 15-inch diameter and larger sanitary sewer mains are required to provide service. Therefore, County approval is required.

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The table below shows completed and current sewer projects associated with this development in the proposed WSSC FY15 Capital Improvement Program. For proposed projects, please submit an updated cost estimate, as well as the estimated start date and duration of construction, in order to update the project description in FY 16 CIP, which begins on July 1, 2015.

Project No.	Project Name in FY11	Current CIP Project Description	Status
S-84.46	Clarksburg Triangle Outfall Sewer, Part 1	4,220 feet of 24-inch	Completed in 2012
S-84.47	Clarksburg Triangle Outfall Sewer, Part 2	4,200 feet of 24-inch, 350 feet of 21-inch, 1,760 feet 18-inch, and 1,040 feet of 15-inch diameter outfall sewers	Most sections Completed in 2014; some segments are under design
S-84.60	Cabin Branch WWPS	0.9 mgd capacity	Planning
S-84.61	Cabin Branch WWPS Force Main	2,000 feet of 10-inch force main	Planning

Costs paid by the Applicant to construct approved CIP projects (mains 15 inch and larger, wastewater pumping station, and force main) may entitle the Applicant to a credit or reimbursement of the System Development Charge (SDC) imposed by WSSC. The amount of the credit will be subject to an SDC Credit Agreement for the pipeline facilities and Memorandum of Understanding (MOU) for the wastewater pumping station and the force main. Please declare in writing if you wish this project be considered for SDC credits and/or reimbursement. The declaration must be received no later than the first design plan submittal to be considered for SDC credits and/or reimbursement.

If the total construction costs plus the other costs is equal to or greater than \$500,000, then prevailing wages are required for the portions of CIP projects that are eligible for SDC Credit/Reimbursement.

#### INSTALL EJECTOR/GRINDER PUMPS

Based on the proposed grading plan, on-site ejector or grinder pumps and on-site low-pressure sewer may be required for service. A registered plumber must install the pumps at the developer's expense.

For properties to be served by a grinder pump system, the developer is responsible for all on-site installation (i.e., materials, electrical equipment, the grinder pump unit and plumbing hook-up which shall be installed by a registered plumber). Grinder pump units must be approved by WSSC. Ultimately, the property owner will be responsible for all on-site maintenance of grinder pump systems.

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Builder/developers/owners should disclose this requirement to purchasers at property settlement.

#### EXTRA-DEPTH SEWER

Due to proposed grades of the street, it will be necessary to construct extra-deep sewer 10 inch and greater. There are sewer segments in various parts of the project showing extra-deep sewers in narrow and difficult areas. These sewers require re-alignments or additional design requirements. See the WSSC Pipeline Design Manual, Section C-2.2, for right-of-way width requirements for deep sewers. Any pipe deeper than 20 feet (trench bottom) will require a special design that takes into consideration future maintenance of the deep sewer.

#### SEWER MAIN ALIGNMENT MAY REQUIRE REVISION

A possible alignment (marked on the attached sketch as alignment A') for sewer segment serving parts of Part 52, Part 30, Part 31, and Part 17 is recommended based on current information and submitted sewer profiles. In Part 57, a segment of sewers (marked T'-T on the sketch) may produce, at full capacity, a higher velocity than accepted by WSSC. Refer to Pipeline Design Manual, Section 9, S-9.1 to S-9.2 and Section 15 for acceptable pipe slopes and required design solutions. For both cases, the final determination is made during the Design review phase.

#### SHALLOW-DEPTH SEWER

Due to the topography, it will be necessary to construct shallow sewer. A minimum cover of 3 feet must be maintained over the sanitary sewer.

#### ENVIRONMENTAL IMPACTS

The proposed sewer main outfall will impact wetlands, stream buffers, 100-year flood plain, steep slopes and possibly large trees. The alignment may need adjustment during the design stage.

#### SHOW MINIBASIN BOUNDARY ON DESIGN PLANS

This project will be served by more than one sewer system mini-basin. Design plans that encompass more than one mini-basin should indicate the boundary as shown on the attached sketch.

#### SEWER SERVICE DEPENDENT ON OTHER CONSTRUCTION

The following table provides information on parts requiring major facility:

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Part	Dependencies
34	Part 39, Part 40 (WWPS and Force main) and required downstream gravity sewers
41	Part 39, Part 40 (WWPS and Force main) and required downstream gravity sewers
42	Part 39, Part 40 (WWPS and Force main) and required downstream gravity sewers
43	Part 39, Part 40 (WWPS and Force main) and required downstream gravity sewers
57	Part 39, Part 40 (WWPS and Force main) and required downstream gravity sewers
58	Part 39, Part 40 (WWPS and Force main) and required downstream gravity sewers
All Other Parts	Associated downstream gravity sewers

## ***WATER MAIN EXTENSION CONDITIONS***

### REQUIRED WATER MAIN SIZES

The diameters of the proposed mains, 4, 8, 10, 12, 16, and 24 inches mains are required to serve this project and are shown on the attached sketch.

### INSTALL SPECIALTY VALVES

An existing pressure zone is extended to serve this project on a temporary basis until Clarksburg elevated water storage tank (W-46.15) is constructed. A remote control pressure-reducing valve is required to regulate pressure from the higher pressure zone. Details for requirements of the remote control pressure-reducing valve will be provided during the Design Phase for this project. The applicant is responsible for design and construction of all required components needed to put the pressure reducing and relief valves in service.

1. Install a 12-inch remote control pressure reducing valve on the 16-inch water main along Whelan Lane (Contract No. DA3326H02) with the following initial settings:
  - HHG: pressure setting 754 feet
  - LHG: pressure setting 742 feet.
2. Install a 6" pressure relief valve downstream of pressure reducing valve. The relief valve setting will be 75 psi and its location is determined during Design phase.

WSSC production team provides additional and necessary information as needed.

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#### ISOLATION VALVES

Provide sufficient numbers of isolation valves on the new mains to provide redundancy.

Provide two connections between the existing 16-inch water main in MD 355 (Contract No. 68-2968A, 836A zone) and the new 24-inch diameter water main (BL3326B02, Part 60, 760A zone). The two connections should be located at: 1) Frederick Road near West Old Baltimore Road and 2) Frederick Road near Brink Road as shown on Sheet 2 of the sketch. Provide isolation valves on the interconnections keep valves closed and mark them DIVISION VALVE. Also, provide an in line 24" isolation valve along the proposed 24" mains between the two above described Division Valves. See Sheet 2 of the sketch for preliminary location of the isolation valve and Mark it KEEP OPEN. The exact location will be finalized during Design phase.

#### SPECIALTY VALVE TABLE AND SKETCH ON DESIGN PLANS

Provide a Specialty Valve Schedule Table on the plans to provide detailed instructions showing the valves to be opened or closed with each sequence of construction.

#### SHOW ZONE BOUNDARY ON DESIGN PLANS

This project will be served by more than one water pressure zone. Design plans that encompass both zones should indicate the zone boundary shown on the attached sketch.

#### CAPITAL IMPROVEMENT PROGRAM (CIP) PROJECT REQUIRED

This project has been designated a "major project" since a water storage facility and over 15,000 feet of 24-inch and 9,800 feet of 16-inch diameter mains are required. Therefore, County approval is required. The table below shows the current water projects related to this development in the WSSC proposed FY15 Capital Improvement Program. Please submit an updated cost estimate, as well as the estimated start date and duration of construction, to update the project description in FY 16 CIP, which begins on July 1, 2015.

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Project No.	Project Name in FY12 CIP	CIP Project Description	Status
W-46.24	Clarksburg Area Stage 3 water main, Part 4	7,900 feet of 24-inch water main	Contract C is complete and in service & Contract B is in Design phase
W-46.14	Clarksburg Area Stage 3 water main, Part 1, 2, & 3	7,100 feet of 24-inch water, 9,300 feet of 16-inch	Contracts H and D are complete and in service & Contracts E and F are in Design phase
W-46.15	Clarksburg Elevated Water Storage Facility	1 MG Elevated Storage Tank	In Design phase and will be built by WSSC

WSSC will construct the Clarksburg Elevated Water Storage Tank. Costs paid by the Applicant to construct approved CIP projects may entitle the Applicant to a credit or reimbursement of the System Development Charge (SDC) imposed by WSSC. The amount of the credit will be subject to an SDC Credit Agreement for the pipeline facilities. Please declare in writing if you want this project to be considered for SDC credits and / or reimbursement. The declaration must be received no later than the first design plan submittal to be considered for SDC credits and/or reimbursement.

If the total construction costs plus the other costs is equal to or greater than \$500,000, then prevailing wages are required for the portions of CIP projects that are eligible for SDC Credit/Reimbursement

#### PRESSURE REDUCING VALVES REQUIRED

Due to high water pressure conditions (greater than 80 psi), the on-site plumbing system requires pressure reducing valves for buildings with first floors below 587 feet.

#### INSTALL BOOSTER PUMPS

Due to low water pressure conditions (less than 40 psi), the on-site plumbing system requires booster pump installation. Booster pumps are required for buildings with first floor levels above elevation 628 feet.

#### OUTSIDE METERS

Any residential water service over 300 feet in length will require an outside meter. Any commercial water service connections, built to serve a standard or minor site utility (on-site) system over 80 feet in length will require an outside meter. Exceptions will be considered for existing properties / complexes currently served by inside meters where replacements or new service is required.

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**SERVICE DEPENDENT ON OTHER CONSTRUCTION**

Since this project will be built in separate parts, the following tables provide information on which parts are dependent on the other parts being constructed and released for service. This project has a two tiered conditions for service, see above. Once the first condition of service (required deed and easements for the elevated tank) is met, a second condition must be fulfilled prior to development parts receiving service. Therefore, the dependency table is broken up into two groups:

- 1) Group 'A': Parts in this group can request for service to *some units* through WSSC partial release process prior to the construction of contract B (parts 28 and 60, Sheet 2 of 3 of the Sketch) as long as total domestic demands of served units do not exceed the threshold of 0.2 mgd, see above. The construction sequence outlined in the table for Group 'A' is per input from the Applicant and is labeled "Water Allocation Exhibit", see Sheet 3 of 3 of the sketch. In the exhibit sketch, units and areas that are cross hatched exceed the demand threshold and cannot receive service until contract B is constructed and released for service.
- 2) Group 'B': Parts in this group can request service once contract B (parts 28 and 60) is released for service; and threshold limit on domestic demand will no longer be valid.

Group 'A' dependency table (Prior to Contract B release for service):

Part	Dependencies	Part	Dependencies
6	Part 4, Part 5	16	Part 4, Part 5, Part 6, Deed and Easements to the Tank Site
7	Part 4, Part 5, Part 6, Deed and Easements to the Tank Site	17	Part 4, Part 5, Part 6, Part 9, Deed and Easements to the Tank Site
8	Part 4, Part 5, Part 6, Part 7 Deed and Easements to the Tank Site	18	Part 4, Part 5, Part 6, Part 7, Part 8, Part 9, Part 12, Part 15, Part 17, Part 19, Part 48, Deed and Easements to the Tank Site
9	Part 4, Part 5	22	Part 4, Part 20, Part 21, Deed and Easements to the Tank Site
10	Part 4, Part 5, Part 6, Part 9	23	Part 61, Part 4, Part 5, Part 6, Part 12, Part 24, Part 29, Deed and Easements to the Tank Site
11	Part 4, Part 5, Part 6, Part 9, Deed and Easements to the Tank Site	48	Part 4, Part 5, Part 6, Part 7, Part 8, Part 9, Part 12, Part 15, Part 17, Part 18, Part 19, Part 25, Part 26, Part 30, Part 52, Deed and Easements to the Tank Site
12	Part 4, Part 5, Part 6, Part 9, Deed and Easements to the Tank Site	49	Part 4, Part 5, Part 6, Part 7, Part 8, Part 9, Part 17, Part 18, Part 48, Deed and Easements to the Tank Site

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13	Part 4, Part 5, Part 6, Part 12, Deed and Easements to the Tank Site	50	Part 4, Part 5, Part 6, Part 7, Part 8, Part 48, Part 28, Part 60, Deed and Easements to the Tank Site
14	Part 4, Part 5, Part 12, Part 6, Deed and Easements to the Tank Site	62	Part 4, Part 20, Deed and Easements to the Tank Site

Group 'B' dependency table (Post Contract B release for service):

Part	Dependencies	Part	Dependencies
15	Part 4, Part 5, Part 6, Part 7, Part 8, Part 28, Part 60, Deed and Easements to the Tank Site	42	Part 4, Part 5, Part 6, Part 32, Part 36, Part 54, Part 50, Part 23, Part 61, Part 28, Part 60, Deed and Easements to the Tank Site
19	Part 4, Part 5, Part 6, Part 7, Part 8, Part 9, Part 12, Part 17, Part 18, Part 28, Part 60, Deed and Easements to the Tank Site	43	Part 4, Part 5, Part 6, Part 35, Part 32, Part 42, Part 36, Part 54, Part 50, Part 23, Part 61, Part 28, Part 60, Deed and Easements to the Tank Site
24	Part 4, Part 5, Part 6, 12, Part 23, Part 29, Part 61, Deed and Easements to the Tank Site, Part 28, Part 60	44	Part 4, Part 20, Part 28, Part 60, Deed and Easements to the Tank Site
25	Part 4, Part 5, Part 6, Part 7, Part 8, Part 24, Part 28, Part 60, Deed and Easements to the Tank Site	45	Part 4, Part 20, Part 28, Part 60, Deed and Easements to the Tank Site
26	Part 4, Part 5, Part 6, Part 7, Part 8, Part 24, Part 28, Part 60, Deed and Easements to the Tank Site	47	Part 4, Part 5, Part 6, Part 7, Part 8, Part 23, Part 27, Part 24, Part 61, Part 28, Part 60, Deed and Easements to the Tank Site
27	Part 4, Part 5, Part 6, Part 7, Part 8, Part 24, Part 28, Part 60, Deed and Easements to the Tank Site	51	Part 4, Part 5, Part 6, Part 7, Part 8, Part 48, Part 50, Part 23, Part 61, Part 28, Part 60, Deed and Easements to the Tank Site
29	Part 23, Part 61, Part 4, Part 5, part 6, Part 24, Part 28, Part 60, , Deed and Easements to the Tank Site	52	Part 4, Part 5, Part 6, Part 7, Part 8, Part 9, Part 15, Part 19, Part 17, Part 18, Part 30, Part 31, Part 48, Part 28, Part 60, Deed and Easements to the Tank Site
30	Part 4, Part 5, Part 6, Part 7, Part 8, Part 9, Part 12, Part 17, Part 18, Part 23, Part 24, Part 31, Part 48, Part 52, Part 28, Part 60, Deed and Easements to the Tank Site	53	Part 4, Part 5, Part 6, Part 7, Part 8, Part 9, Part 15, Part 17, Part 18, Part 19, Part 48, Part 28, Part 60, Deed and Easements to the Tank Site
31	Part 4, Part 5, Part 6, Part 7, Part 8, Part 9, Part 12, Part 15, Part 17, Part 18, Part 19, Part 23, Part 24, Part 30, Part 48, Part 28, Part 60, Deed and Easements to the Tank Site	55	Part 4, Part 5, Part 6, Part 7, Part 8, Part 9, Part 15, Part 17, Part 18, Part 19, Part 48, Part 53, Part 50, Part 54, Part 23, Part 61, Part 28, Part 60, Deed and Easements to the Tank Site
33	Part 4, Part 5, Part 12, Part 6, Part 9, Part 28, Part 60, Deed and Easements to the Tank Site	56	Part 4, Part 5, Part 6, Part 7, Part 8, Part 23, Part 48, Part 61, Part 28, Part 60, Deed and Easements to the Tank Site
34	Part 4, Part 5, Part 6, Part 32, Part 28, and Part 60, Deed and Easements to the Tank Site	57	Part 4, Part 5, Part 32, Part 35, Part 36, Part 41, Part 54, Part 50, Part 23, Part 61, Part 28, and Part 60, Deed and Easements to the Tank Site

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37	Part 4, Part 5, Part 6, Part 23, Part 24, Part 61, Part 28, and Part 60, Deed and Easements to the Tank Site	58	Part 4, Part 5, Part 32, Part 35, Part 43, Part 36, Part 54, Part 50, Part 23, Part 61, Part 28, Part 60, Deed and Easements to the Tank Site
41	Part 4, Part 5, Part 6, Part 32, Part 35, Part 36, Part 54, Part 50, Part 23, Part 61, Part 28, Part 60, Deed and Easements to the Tank Site	59	Part 4, Part 5, Part 6, Part 7, Part 8, Part 28, Part 60, Deed and Easements to the Tank Site
63	Part 4, Part 20, Part 28, Part 60, Deed and Easements to the Tank Site	64	Part 4, Part 5, Part 12, Part 6, Part 28, Part 60, Deed and Easements to the Tank Site

### ***EASEMENT CONDITIONS***

#### **COORDINATION WITH OTHER BURIED UTILITIES**

Refer to WSSC Pipeline Design Manual pages G-1 and G-2 for utility coordination requirements. No structures or utilities (manholes, vaults, pipelines, poles, conduits, etc.) are permitted in the WSSC easement unless specifically approved by WSSC. Longitudinal occupancy of WSSC easements (by other utilities) is not permitted. Proposed utility crossings of WSSC pipelines or easements that do not adhere to WSSC's pipeline crossing and clearance standards will be rejected at the design plan review phase. Refer to WSSC Pipeline Design Manual Part Three, Section 3. Failure to adhere to WSSC crossing and clearance standards may result in significant impacts to the development plan including impacts to proposed street and building layouts.

The applicant must provide a separate "Utility Plan" to ensure that all existing and proposed site utilities have been properly coordinated with existing and proposed WSSC facilities and easements. Upon completion of the site construction, any utilities that are found to be located within WSSC's easements (or in conflict with WSSC pipelines) must be removed and relocated at the applicant's expense.

#### **IMPACTS DUE TO GRADING / PIPE LOADING CHANGES**

Any grading, change in pipe loading (including but not limited to proposed fill or excavation), adjustment to manhole rims, fire hydrant relocations, placement of access roads or temporary haul roads, temporary sediment control devices, paving construction or construction related activity of any kind over an existing WSSC water or sewer main or within an existing WSSC easement requires **advance approval** by WSSC. Any proposed public street grade establishment plan (GEP) with an existing WSSC water or sewer main of any size located within the existing or proposed public street right-of-way requires WSSC approval directly on the original GEP **prior to** approval of the GEP by the County Department of Public Works and Transportation. Any work (design, inspection, repair, adjustment, relocation or abandonment) of existing WSSC facilities is done at the sole expense of the applicant/builder/developer. Contact WSSC Relocations Unit

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at (301) 206-8672 for review procedures and fee requirements. See WSSC Design Manual C-11.

**PROVIDE FREE EASEMENT TO WSSC**

Easements across your property for water and sewer line placement must be provided at no cost to the WSSC. Also, an easement and construction easement across your property for future WSSC water/sewer line placement, as shown on the attached sketch, must be provided at no cost to WSSC. The Applicant shall execute and deliver on-property easements prior to the Certificate of Substantial Completion, which shall constitute an irrevocable offer by the Applicant to convey all on-property easements to WSSC.

**OFF-PROPERTY EASEMENTS MUST BE OBTAINED**

The proposed water and sewer main extensions will require the acquisition of easements from other property owners. It is the Applicant's responsibility to obtain these easements.

**ADHERE TO MINIMUM EASEMENT WIDTHS**

The minimum easement width for a normal (14 inches diameter or less) extension, either water or sewer, installed at normal depth is 20 feet. A minimum easement width of 30 feet is required when both normal-diameter water and gravity sewer lines are installed in the same easement at normal depth. Installation of deep or large water and/or sewer mains will require additional easement width. For minimum horizontal separation between a building and a WSSC pipeline, refer to the requirements in the current applicable Pipeline Design Manual, Part Three, Section 3. c. 2. Based on WSSC requirements, the minimum spacing between adjacent buildings with both water and sewer lines between them should be at least 40 feet and, in some cases, greater when connections, fire hydrants, or deep sewer or water lines are involved. Balconies and other building appurtenances are not to be within the easement. Additionally, water and sewer pipeline alignment should maintain 5 feet horizontal clearance from storm drain pipeline/structures and other utilities.

***CONNECTION AND SITE UTILITY CONDITIONS***

**MINIMIZE CONNECTION LENGTHS**

The length of all connections should be minimized.

**SITE UTILITY PROCESS REQUIRED**

The Site Utility process is usually required for water lines greater than 2 inches in diameter or sewer lines greater than 4 inches. Please submit Site Utility information to the WSSC Permit Services Unit at the One-Stop-Shop. Contact

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Permit Services at 301-206-4003 or at [www.wsscwater.com](http://www.wsscwater.com) for more information on submitting Site Utility plans.

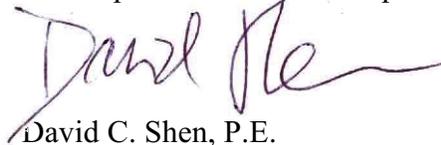
The next step in the process is Phase 2, Review for System Integrity. Contact Permit Services at 301-206-4003 or at [www.wsscwater.com](http://www.wsscwater.com) for more information on electronic submittal of System Integrity Review Packages. Should you wish to schedule a pre-design meeting, please contact Betty Yanchulis (301-206-8568) [Betty.Yanchulis@wsscwater.com](mailto:Betty.Yanchulis@wsscwater.com).

If you have any questions or concerns, please feel free to contact me at 301-206-8812 or [Shari.Djourshari@wsscwater.com](mailto:Shari.Djourshari@wsscwater.com).

Sincerely,



Shari Djourshari, Ph.D.  
Principal Hydraulic Engineer  
Development Services Group



David C. Shen, P.E.  
Development Planning Unit Coordinator  
Development Services Group

Enclosures

cc: Mr. Tom Miller – Rodgers Consulting, Inc.  
Mr. Alan Soukup ([alan.soukup@montgomerycountymd.gov](mailto:alan.soukup@montgomerycountymd.gov)) - Department of Environmental Protection - Montgomery County Government



## DEPARTMENT OF PERMITTING SERVICES

Diane R. Schwartz Jones  
Director

Isiah Leggett  
County Executive

August 25, 2014

Mr. Philip R. Hughes, P.E.  
Rodgers Consulting, Inc.  
19847 Rodgers Consulting, Inc.  
Germantown, Maryland 20874

Re: Revised **Preliminary Water Quality Plan**  
Stormwater Management **CONCEPT** Request  
for Cabin Branch  
Preliminary Plan #: 12003110B  
SM File #: 207133  
Tract Size/Zone: 535.4 ac./MXP, RMX-1/TDR  
Total Concept Area: 535.4 acres  
Watershed: Little Seneca

Dear Mr. Hughes:

Based on a review by the Department of Permitting Services Review Staff, the Revised Preliminary Water Quality Plan and stormwater management concept for the above mentioned site is **acceptable**. The stormwater management concept proposes to meet required stormwater management goals via ESD (e.g. bioswales, micro-bioretenion and green roofs) for all of the proposed development that isn't currently permitted, as shown as on the revised area of the Preliminary Water Quality Plan (PWQP) associated with this revision application. The previously permitted sections of the subject property remain unchanged by this revision and continue to be subject to the previous approval conditions. All of the previously required goals and conditions still apply. A BMP monitoring fee will be required for the area which is the subject of this PWQP revision.

The following **conditions** will need to be addressed **during** the detailed sediment control/stormwater management plan stage:

1. A detailed review of the stormwater management computations will occur at the time of detailed plan review.
2. All subsequent Final Water Quality Plans for this development must provide ESD to the Maximum Extent Practicable.

This list may not be all-inclusive and may change based on available information at the time of Final Water Quality Plan review.

Payment of a stormwater management contribution in accordance with Section 2 of the Stormwater Management Regulation 4-90 **is not required**.

This letter must appear on the sediment control/stormwater management plan at its initial submittal. The concept approval is based on all stormwater management structures being located outside of the Public Utility Easement, the Public Improvement Easement, and the Public Right of Way unless specifically approved on the concept plan. Any divergence from the information provided to this office; or additional information received during the development process; or a change in an applicable Executive Regulation may constitute grounds to rescind or amend any approval actions taken, and to reevaluate the site for additional or amended stormwater management requirements. If there are subsequent additions or modifications to the development, a separate concept request shall be required.

1255 Rockville Pike, 2nd Floor • Rockville, Maryland 20850 • 240-777-6300 • 240-777-6256 TTY

[www.montgomerycountymd.gov](http://www.montgomerycountymd.gov)

Philip Hughes  
August 25, 2014  
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If you have any questions regarding these actions, please feel free to contact Leo Galanko at 240-777-6242.

Sincerely,



Mark O. Etheridge, Manager  
Water Resources Section  
Division of Land Development Services

MCE: me lmg

cc: C. Conlon  
SM File # 207133

ESD Acres:	approx. 172
STRUCTURAL Acres:	approx. 363
WAIVED Acres:	N/A

## Jackson, Molline

---

**From:** Galanko, Leo <Leo.Galanko@montgomerycountymd.gov>  
**Sent:** Friday, November 07, 2014 9:58 AM  
**To:** Gary Unterberg  
**Cc:** Penn, Joshua; Jackson, Molline; Carter, John; Robert R. Harris (rrharris@lercheary.com); sylke.knuppel@whihomes.com; Gilmore, David; Philip Hughes  
**Subject:** Re: Cabin Branch Water Tower Site

Gary,

You are correct. The area for the Water Tower was included in the DA to Pond 11. Pond 11 was part of the approved infrastructure plan. And, WSSC will be required to provide full ESD when they move forward. Therefore I feel that all of the SWM goals will be fully met when the onsite ESD is provided.

**Leo M. Galanko, CPESC, CPSWQ**

Land Development/Water Resources  
 Montgomery County Department of Permitting Services  
 255 Rockville Pike, Rockville Maryland 20850-4166  
 Phone (240) 777-6242, Fax (240) 777-6339  
[leo.galanko@montgomerycountymd.gov](mailto:leo.galanko@montgomerycountymd.gov)

---

**From:** Gary Unterberg <[GUnterberg@RODGERS.com](mailto:GUnterberg@RODGERS.com)>  
**Sent:** Thursday, November 6, 2014 10:15 AM  
**To:** Galanko, Leo  
**Cc:** Penn, Joshua ([Joshua.Penn@montgomeryplanning.org](mailto:Joshua.Penn@montgomeryplanning.org)); Smith, Molline ([molline.smith@montgomeryplanning.org](mailto:molline.smith@montgomeryplanning.org)); Carter, John ([John.Carter@montgomeryplanning.org](mailto:John.Carter@montgomeryplanning.org)); Robert R. Harris ([rrharris@lercheary.com](mailto:rrharris@lercheary.com)); [sylke.knuppel@whihomes.com](mailto:sylke.knuppel@whihomes.com); Gilmore, David; Philip Hughes  
**Subject:** Cabin Branch Water Tower Site

Leo, MNCPPC has asked us to clarify the SWM/WQ status for the relocated Water Tower Site. The area for the new tower is included in the drainage area for modified SWM Pond 11. Pond 11 and the associated WQ Plan were approved with the Infrastructure Site Plan (8-20050150) in 2007 for grading. WSSC will provide redundant SWM at permit. The tower site is not included in the new ESD SWM area since it is in the Pond 11 drainage area. The September 30, 2014 letter is attached. Also attached is the Pond 11 drainage area as modified showing what goes to Pond 11 per earlier approvals and what is being revised to provide ESD.

Please confirm that this is correct.  
 If you have any questions give me a call.  
 Thanks  
 Gary

**Gary F. Unterberg, RLA, LEED AP BD+C**  
**Sr. Vice President**  
**Rodgers Consulting, Inc**  
 240-912-2117 Direct  
 301-873-4858 Cell

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**Creativity**  
**Enduring Values**

[www.rodgers.com](http://www.rodgers.com)

Unless expressly indicated, nothing contained in this e-mail is intended to be an offer to commit Rodgers Consulting, Inc., to any purchase, sale, contract, or other course of action.





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## FIRE MARSHAL COMMENTS

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**DATE:** 22-Sep-14  
**TO:** Gary Unterberg  
Rodgers Consulting, Inc.  
**FROM:** Marie LaBaw  
**RE:** Clarksburg Premium Outlets at Cabin Branch  
820140160

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### PLAN APPROVED

1. Review based only upon information contained on the plan submitted **22-Sep-14** .Review and approval does not cover unsatisfactory installation resulting from errors, omissions, or failure to clearly indicate conditions on this plan.
2. Correction of unsatisfactory installation will be required upon inspection and service of notice of violation to a party responsible for the property.

**\*\*\* Performance-based design \*\*\***



## DEPARTMENT OF PERMITTING SERVICES

Isiah Leggett  
County Executive

Diane R. Schwartz Jones  
Director

September, 30 2014

Mr. Philip R. Hughes, P.E.  
Rodgers Consulting, Inc.  
19847 Century Blvd., Suite 200  
Germantown, Maryland 20874

Re: **Final Water Quality Plan** Request for  
Clarksburg Premium Outlets at Cabin Branch  
Preliminary Plan #: 12003110B  
SM File #: 264517  
Tract Size/Zone: 84.24 acres/MXPD  
Total Concept Area: 84.24  
Watershed: Little Seneca

Dear Mr. Hughes:

Based on a review by the Department of Permitting Services Review Staff, the Final Quality Plan (FWQP) for the above mentioned site is **acceptable**. The Final Water Quality Plan proposes to meet required stormwater management goals via permeable pavement, biowales and micro-bioretenion. All of the previously required goals and conditions noted on the Preliminary Water Quality Plan still apply. A BMP monitoring fee will be required for the area of this FWQP.

The following **conditions** will need to be addressed **during** the detailed sediment control/stormwater management plan stage:

1. A detailed review of the stormwater management computations will occur at the time of detailed plan review.
2. Provide safe conveyance of the 10 and 100 year storms through Pond#11 and safe conveyance and non-erosive outfall of the 10 year storm in drainage area #12.

This list may not be all-inclusive and may change based on available information at the time of Final Water Quality Plan review.

Payment of a stormwater management contribution in accordance with Section 2 of the Stormwater Management Regulation 4-90 is **not required**.

This letter must appear on the sediment control/stormwater management plan at its initial submittal. The concept approval is based on all stormwater management structures being located outside of the Public Utility Easement, the Public Improvement Easement, and the Public Right of Way unless specifically approved on the concept plan. Any divergence from the information provided to this office; or additional information received during the development process; or a change in an applicable Executive Regulation may constitute grounds to rescind or amend any approval actions taken, and to reevaluate the site for additional or amended stormwater management requirements. If there are subsequent additions or modifications to the development, a separate concept request shall be required.

Mr. Philip Hughes  
September 30, 2014  
Page 2

If you have any questions regarding these actions, please feel free to contact Leo Galanko at 240-777-6242.

Sincerely,



Mark C. Etheridge, Manager  
Water Resources Section  
Division of Land Development Services

MCE: me lmg

cc: C. Conlon  
SM File # 264517

ESD Acres:	approx. 84.24
STRUCTURAL Acres:	N/A
WAIVED Acres:	N/A

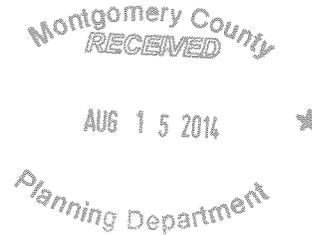


## DEPARTMENT OF TRANSPORTATION

Isiah Leggett  
County Executive

Arthur Holmes, Jr.  
Director

August 8, 2014



Ms. Molline Smith, Senior Planner  
Area 3 Planning Division  
The Maryland-National Capital  
Park & Planning Commission  
8787 Georgia Avenue  
Silver Spring, Maryland 20910-3760

RE: Preliminary Plan No. 12003110C  
Cabin Branch

*Molline:*  
Dear Ms Smith:

We have completed our review of the June 13, 2014 amended preliminary plan. This plan was reviewed by the Development Review Committee at its meeting on July 28, 2014. Since the plan amendments focus primarily on water quality and private street issues, while not affecting the previously approved County-maintained street network, we do not have any additional comments on the current plan.

Please note, however, that MCDOT did not review the Traffic Impact Study (TIS) last year for the Development Plan Amendment (DPA 13-02) as we were never paid the required MCDOT Development Review Fee. Should an updated TIS be required for this plan amendment, the applicant will need to pay a reduced TIS review fee before we will commence our review of that document.

If you have any questions or comments regarding this letter, please contact Mr. Deepak Somarajan, our Development Review Area Engineer for this project, at (240) 777-2194 or at [deepak.somarajan@montgomerycountymd.gov](mailto:deepak.somarajan@montgomerycountymd.gov).

Sincerely,

Gregory M. Leck, Manager  
Development Review Team

M:\corres\FY15\Traffic\Active\1203110C, Cabin Branch amended prelim plan, MCDOT plan review ltr.doc

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**Division of Traffic Engineering and Operations**

100 Edison Park Drive, 4th Floor • Gaithersburg, Maryland 20878  
Main Office 240-777-2190 • TTY 240-777-6013 • FAX 240-777-2080  
[trafficops@montgomerycountymd.gov](mailto:trafficops@montgomerycountymd.gov)

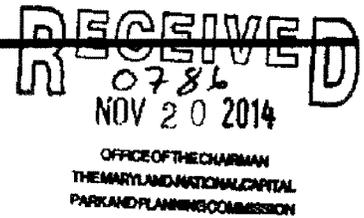
Ms. Molline Smith  
 Preliminary Plan No. 12003110C  
 August 8, 2014  
 Page 2

cc: Steve Dworkin Simon/Clarksburg Development  
 Gary Unterberg Rodgers Consulting  
 Frank Bossong Rodgers Consulting  
 Steven Robins Lerch Early & Brewer  
 Robert Harris Lerch Early & Brewer  
 Scott Newill MSHA AMD  
 Preliminary Plan folder  
 Preliminary Plan letters notebook

cc-e: Katherine Holt M-NCPPC Area -3  
 Ed Axler M-NCPPC Area -2  
 Catherine Conlon M-NCPPC DARC  
 Mark Etheridge MCDPS WRM  
 Atiq Panjshiri MCDPS RWPR  
 Sam Farhadi MCDPS RWPR  
 Marie LaBaw MCFRS  
 Fred Lees MCDOT DTEO  
 Mark Terry MCDOT DTEO  
 Deepak Somarajan MCDOT DTEO

**MCP-Chair**

**From:** Garcia <hgarcia33@comcast.net>  
**Sent:** Thursday, November 20, 2014 3:44 PM  
**To:** MCP-Chair  
**Subject:** Clarksburg



Hugo Garcia  
23055 Birch Mead Road  
Clarksburg, MD 20871  
November 4, 2014  
Mr. Casey Anderson  
Montgomery County Planning Board  
8787 Georgia Avenue  
Silver Spring, MD 20910

Dear Mr. Anderson,

As a resident of the Arora Hills community in Clarksburg, I am writing to urge you and your fellow Planning Board members approve the development of Clarksburg Premium Outlets at Cabin Branch.

This outlet center project will have an undeniably positive impact on the Clarksburg community as a whole. The financial investment, tax revenue, employment opportunities and enhanced property values will make Clarksburg Premium Outlets a local asset. Our community will also benefit from having new places to shop, dine and get together; something we have needed for a very long time.

The Clarksburg community is very excited about the outlet center and has embraced it. I encourage you and the other members of the Planning Board to support it as well.

Thank you,  
Hugo Garcia  
240-477-7446

**RECEIVED**  
6785  
NOV 20 2014**MCP-Chair**

**From:** Marilyn Balcombe <MBalcombe@GGChamber.org>  
**Sent:** Thursday, November 20, 2014 1:39 PM  
**To:** MCP-Chair  
**Subject:** Support for the Clarksburg Premium Outlets – Site Plan No. 820140160  
**Attachments:** Cabin Branch Outlets.pdf

OFFICE OF THE CHAIRMAN  
THE MARYLAND-NATIONAL CAPITAL  
PARK AND PLANNING COMMISSION

Dear Chairman Anderson,

Please accept and distribute the attached letter of support for the Site Plan for the Clarksburg Premium Outlets. This is an exciting project and the Gaithersburg-Germantown Chamber of Commerce is in full support.

Thank you,

*Marilyn*

Marilyn Balcombe  
President / CEO  
Gaithersburg-Germantown Chamber of Commerce  
301-840-1400 x15  
[mbalcombe@ggchamber.org](mailto:mbalcombe@ggchamber.org)  
[www.ggchamber.org](http://www.ggchamber.org)

910 Clopper Road  
Suite 205N  
Gaithersburg, MD 20878



# Gaithersburg-Germantown Chamber of Commerce, Inc.

910 Clopper Road, Suite 205N, Gaithersburg, Maryland 20878 (301) 840-1400, Fax (301) 963-3918

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**The Gazette**



**HUGHES**



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UNIVERSITY

MONTGOMERY COUNTY



THE TRISTAR WADDELL

November 20, 2014

Casey Anderson, Esq.  
Chairman  
Montgomery County Planning Board  
8787 Georgia Avenue  
Silver Spring, MD 20910

Re: Clarksburg Premium Outlets – Site Plan No. 820140160

Dear Chairman Anderson and Members of the Board:

We are writing to you today to express the support of the Gaithersburg-Germantown Chamber of Commerce for this application. As you may recall, last year we submitted a letter of support for the Development Plan Amendment to enable this project to proceed. We were pleased that the Planning Board and the County Council strongly supported the proposal.

Last week, the applicant made a presentation to our Economic Development Committee and we want to renew our support for the project. It is exciting to see a major new retail project come to the upcounty area. We recognize a lot of attention has been paid recently to down county development and we want to make sure the County continues to recognize the importance of our area as well. The upcounty is home to more than 250,000 people and thousands of jobs. Its continued vitality depends on new development opportunities like this.

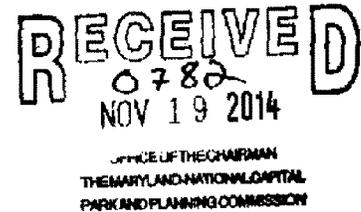
We believe the project will serve as a catalyst for other employment uses at Cabin Branch and in the upcounty and will provide highly desirable services like restaurants, stores and entertainment for residents and workers alike. Please count us among the many supporters of this project.

Sincerely,

Marilyn Balcombe  
President and CEO

cc: Robert Harris

[www.ggchamber.org](http://www.ggchamber.org)



November 13, 2014

Chairman Casey Anderson  
Montgomery County Planning Board  
8787 Georgia Avenue  
Silver Spring, MD 20910

Re: Site Plan Application #820140160 – Clarksburg Premium Outlets at  
Cabin Branch

Dear Mr. Anderson and Members of the Planning Board:

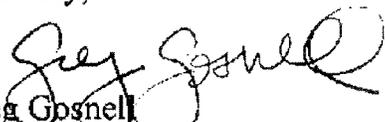
As the owners of the 11 acres of land on the north side of relocated MD-121 in the Cabin Branch Neighborhood, we are writing to support the site plan application for Clarksburg Premium Outlets at Cabin Branch.

Based on our extensive development experience in Montgomery County, we firmly believe the specialty retail offered by the outlet center will be good for Clarksburg. The outlet center will put the Clarksburg area on the map as a destination for quality shopping and dining. The caliber of the Premium Outlets brand will attract new visitors to Clarksburg and provide a new customer base for new and existing businesses.

Most importantly, the outlet center will be a catalyst for additional, well conceived, development which could bring a dynamic change on a vital stretch of I-270 that has been in a holding pattern literally for decades. A project of this scale and scope provides the retail services and restaurant options needed to attract other employment-generating development which brings revenue to the County and a better quality of life to its' citizens.

We encourage the Planning Board to approve the site plan application and allow this project to move forward.

Sincerely,

  
Greg Gosnell  
(For the Gosnell Family interests in Clarksburg)

**MCP-Chair****RECEIVED**0782  
NOV 19 2014OFFICE OF THE CHAIRMAN  
THE MARYLAND NATIONAL CAPITAL  
PARK AND PLANNING COMMISSION

**From:** Cherian Eapen <cherianeapen@hotmail.com>  
**Sent:** Wednesday, November 19, 2014 1:41 PM  
**To:** MCP-Chair  
**Cc:** Jackson, Molline  
**Subject:** Clarksburg Premium Outlets at Cabin Branch - Site Plan Application No. 820140160

Mr. Casey Anderson  
Planning Board Chair  
Montgomery County Planning Board  
8787 Georgia Avenue  
Silver Spring, MD 20910

Dear Casey:

As a Clarksburg resident, I am writing to you in support of Site Plan Application No. 820140160 for Clarksburg Premium Outlets at Cabin Branch. I believe the outlet center design fits well with the surrounding landscape and will offer ample public spaces. I am particularly supportive of the modern design elements proposed for the outlet center.

I believe that the proposed outlet center will be good addition to our community and could spur additional growth in Clarksburg. Clarksburg will also benefit from the transportation infrastructure improvements Cabin Branch development will bring to the area prior to the opening of the outlet center.

I kindly request that the Planning Board approve the site plan application and allow this project to move forward.

Please feel free to contact me if you have any questions.

Regards,  
Cherian Eapen  
23118 Birch Mead Road  
Clarksburg, MD 20871

**MCP-Chair****RECEIVED**  
0779  
NOV 18 2014

**From:** BARRY FANTLE <bfantle@aol.com>  
**Sent:** Tuesday, November 18, 2014 3:30 PM  
**To:** MCP-Chair; molline.smith@mncppc-mc.org.  
**Subject:** Clarksburg Premium Outlets at Cabin Branch

OFFICE OF THE CHAIRMAN  
THE MARYLAND NATIONAL CAPITAL  
PARK AND PLANNING COMMISSION

November 18, 2014

Mr. Casey Anderson  
 Chair  
 Montgomery County Planning Board  
 8787 Georgia Avenue  
 Silver Spring, MD 20910

Dear Mr. Anderson,

I am writing to express my support for the Clarksburg Premium Outlets at Cabin Branch development.

As a long-time Clarksburg resident, who has been very involved in the many issues surrounding the growth of our community, I have followed the progress of the Cabin Branch site and am glad to see the development move forward. I believe this outlet center project will bring tremendous opportunity to our area.

In addition to providing access to shopping and restaurants for Clarksburg residents, the outlet center will also bring many infrastructure, road improvements, and commercial growth to our area. Clarksburg residents have waited a long time for commercial growth and development in our community. Many promises have been made, but nothing has materialized. We're just starting to see commercial investment with the opening of the Harris Teeter and GrillmarX at the Clarksburg Village Center. The Clarksburg Premium Outlets at Cabin Branch help make sure we continue to move forward and bring much-needed retail and vitality to Clarksburg.

I also believe that this will benefit the residents of Clarksburg in regards to Town Center. Once retailers see the success of the outlet center, there will be more of a push to get Town Center completed and leased. As I stated in previous testimony before the board, this project is good for Clarksburg and good for Town Center.

I encourage you to approve the development of Clarksburg Premium Outlets at Cabin Branch.

Thank you for your time.

Sincerely,

Barry Fantle  
 12711 Clarks Crossing Drive

Clarksburg, MD 20871

**Lane, Brittany**

RECEIVED  
0790  
NOV 24 2014

**From:** Kathie Hulley <kathiehulley@gmail.com>  
**Sent:** Monday, November 24, 2014 9:38 AM  
**To:** MCP-Chair  
**Cc:** Adeyinka on behalf of Clarksburg Premium Outlets at Cabin Branch  
**Subject:** Clarksburg Premium Outlets at Cabin Branch  
**Attachments:** Premium Outlets Planning Board letter.pdf

OFFICE OF THE CHAIRMAN  
THE MARYLAND-NATIONAL CAPITAL  
PARK AND PLANNING COMMISSION

Attached please find a letter of support for the Clarksburg Premium Outlets at Cabin Branch.

Kathie Hulley

21809 Diller Lane  
Boys, MD 20841

November 21, 2014

Chairman Casey Anderson  
Montgomery County Planning Board  
8787 Georgia Avenue  
Silver Spring, MD 20910

RE: Clarksburg Premium Outlets at Cabin Branch

I am writing to express my strong support for the development of Clarksburg Premium Outlets at Cabin Branch.

I have lived in this community for more than 30 years and have been involved in the Clarksburg Master Plan process since 1991.

Clarksburg Premium Outlets at Cabin Branch will be a positive benefit to the Clarksburg community for the following reasons:

1. Access to retail – Our community has gone without adequate shopping and dining amenities for far too long. Instead of travelling to Germantown or Gaithersburg we will have options here in our community.
2. Local jobs – Many Clarksburg residents commute long distances to get to work and our young people do not have nearby options for local employment. This project will provide local jobs close to home.
3. Improvements to local infrastructure – Infrastructure improvements have lagged behind the needs of the community for sometime. The Cabin Branch project brings much needed traffic and road improvements to Clarksburg Road.
4. Economic development – This outlet center will be a catalyst that sparks additional commercial investment to Clarksburg to support the needs of residents.

We have had many meetings with the developers and I am impressed that our input is requested and that our ideas are incorporated where possible.

I encourage you to consider these benefits and support the development of Clarksburg Premium Outlets in order that the project be completed as soon as possible.

Sincerely



Kathie Hulley

**Ibi Sofillas**  
MyRealtyTeam Real Estate LLC  
ClarksburgLovelyHomes.com  
22220 Canterfield Way  
Germantown, MD 20876

November 3, 2014

Chairman Casey Anderson  
Montgomery County Planning Board  
8787 Georgia Avenue  
Silver Spring, MD 20910

Dear Mr. Anderson:

I am writing as strong supporter of the plans for Clarksburg Premium Outlets at Cabin Branch.

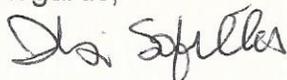
As a business owner and someone who is very involved in the community, I can personally attest to the excitement the Cabin Branch outlet center proposal has brought to the Clarksburg and neighboring communities. We simply cannot wait for the outlet center to open because we recognize that it will ultimately make Clarksburg a more vibrant and economically viable community. In my opinion, this project is logical next step for a community that has seen a lot of residential development, but still needs a commercial base and economic growth.

As I said in my testimony before the Zoning Hearing Examiner last summer, Clarksburg Premium Outlets at Cabin Branch means more local jobs, local investment, as well as local shopping and dining options in the near future. This is the type of forward-looking development that will bring additional long-term benefits to Clarksburg and the entire County.

I hope you will vote in favor of the development proposal and allow Clarksburg Premium Outlets to move forward.

Thank you for your consideration,

Regards,



Ibi Sofillas

Mark Lieberman  
23402 Clarksridge Road  
Clarksburg, MD 20871

November 3, 2014

Mr. Casey Anderson  
Montgomery County Planning Board  
8787 Georgia Avenue  
Silver Spring, MD 20910

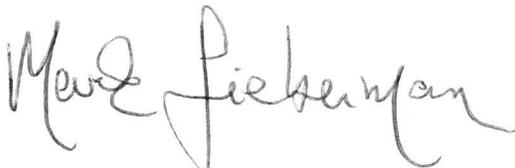
Dear Mr. Anderson and members of the Planning Board,

I am writing to express my strong support for the planned Clarksburg Premium Outlets at Cabin Branch. I have been a Town Center resident for almost 10 years. After many broken promises, this is very exciting and just what this community needs to get re-energized and focused on positive growth.

The outlet center will bring local jobs to our community and also signal to others that Clarksburg is ready for economic development and growth. Our community has waited a long time for this type of investment and I'm happy to see someone is finally paying attention.

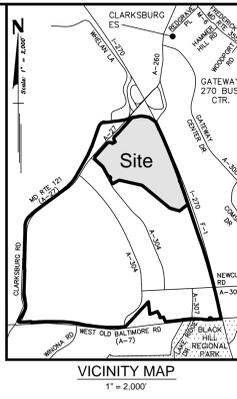
I am looking forward to the groundbreaking of the outlet center and encourage you to vote in favor of this project.

Sincerely,

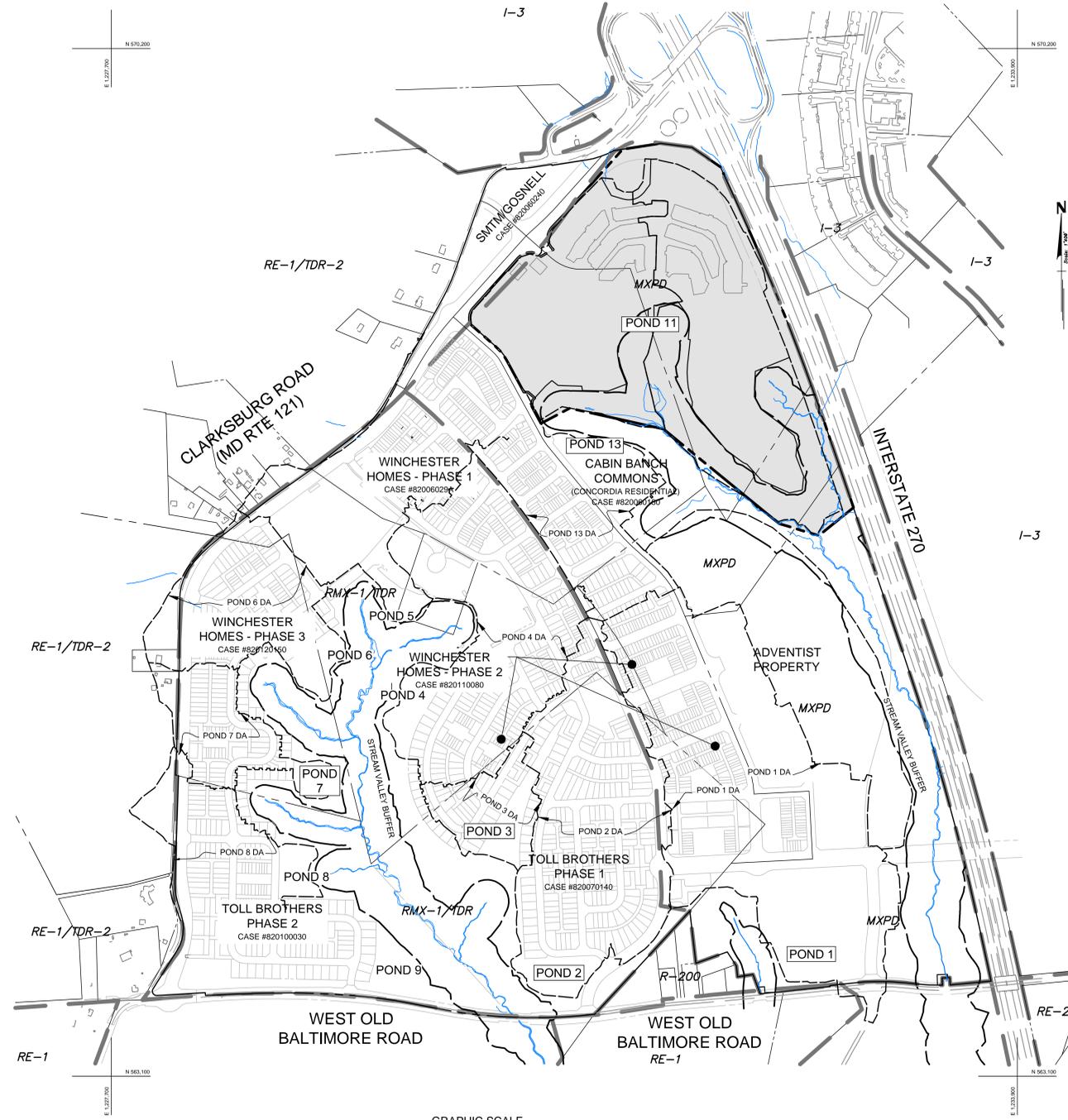
A handwritten signature in black ink that reads "Mark Lieberman". The signature is written in a cursive, flowing style.

Mark Lieberman  
(301) 528-0122

# CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1 FINAL WATER QUALITY PLAN



PRELIMINARY NOT FOR CONSTRUCTION



**Applicant/Owner:**  
SIMON/CLARKSBURG DEVELOPMENT, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, New Jersey 07068  
Phone: (973) 403-3167  
Contact: Mr. Steve Dworkin

**Attorney:**  
LERCH, EARLY & BREWER, CHTD.  
3 Bethesda Metro Center  
Suite 460  
Bethesda, Maryland 20814  
Phone: (301) 841-3826  
Contact: Mr. Robert Harris

**Architect:**  
ARCHITECTS ORANGE  
144 North Orange Street  
Orange, California 92866  
Phone: (714) 639-9860  
Contact: Mr. Darrel Hebenstreit

**Civil Engineer/Land Planner:**  
RODGERS CONSULTING, INC.  
19847 Century Boulevard, Suite 200  
Germantown, Maryland 20874  
Phone: (301) 948-4700  
Contact: Mr. Gary F. Unterberg

**Landscape Architect:**  
TERRAPLAN  
20 Champlain Boulevard, Suite 102  
Toronto, Ontario, M3H 2Z1, Canada  
Phone: (416) 638-4911  
Contact: Mr. Matthew Bernstein

**Development Team:**



**Sheet List Table**

Sheet Number	Sheet Title
1	FINAL WATER QUALITY PLAN COVER SHEET
2	FINAL WATER QUALITY PLAN - OVERALL SITE
3	FINAL WATER QUALITY PLAN - DRAINAGE AREA 11
4	FINAL WATER QUALITY PLAN - DRAINAGE AREA 12
5	FINAL WATER QUALITY PLAN - DRAINAGE AREA 12



NOTE: UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES AND ARE SUBJECT TO CHANGE AT FINAL ENGINEERING.

**CALL "MISS UTILITY" AT 1-800-257-7777 48 Hours Before Start Of Construction**

**PROFESSIONAL CERTIFICATION**  
I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13970, Expiration Date 6/1/14.



REVISION	DATE	REVISION	DATE	REVISION	DATE

**Owner/Developer:**  
Simon/Clarksburg Development, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, NJ 07068  
Phone: (973) 403-3167  
Contact: Steve Dworkin  
e-mail: sdworkin@simon.com

**CLARKSBURG PREMIUM OUTLETS  
AT CABIN BRANCH  
NORTH DISTRICT, PHASE 1**

WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3

**RODGERS CONSULTING**  
19847 Century Boulevard  
Suite 200  
Germantown, Maryland 20874  
Ph: 301.948.4700 (Main)  
Ph: 301.253.6509 (Frederick)  
Fx: 301.948.6256  
www.rodgers.com

BASE DATA	BY	DATE
DESIGNED		
DRAWN		
REVIEWED		
RODGERS CONTACT:		
RELEASE FOR		
BY: _____	DATE: _____	

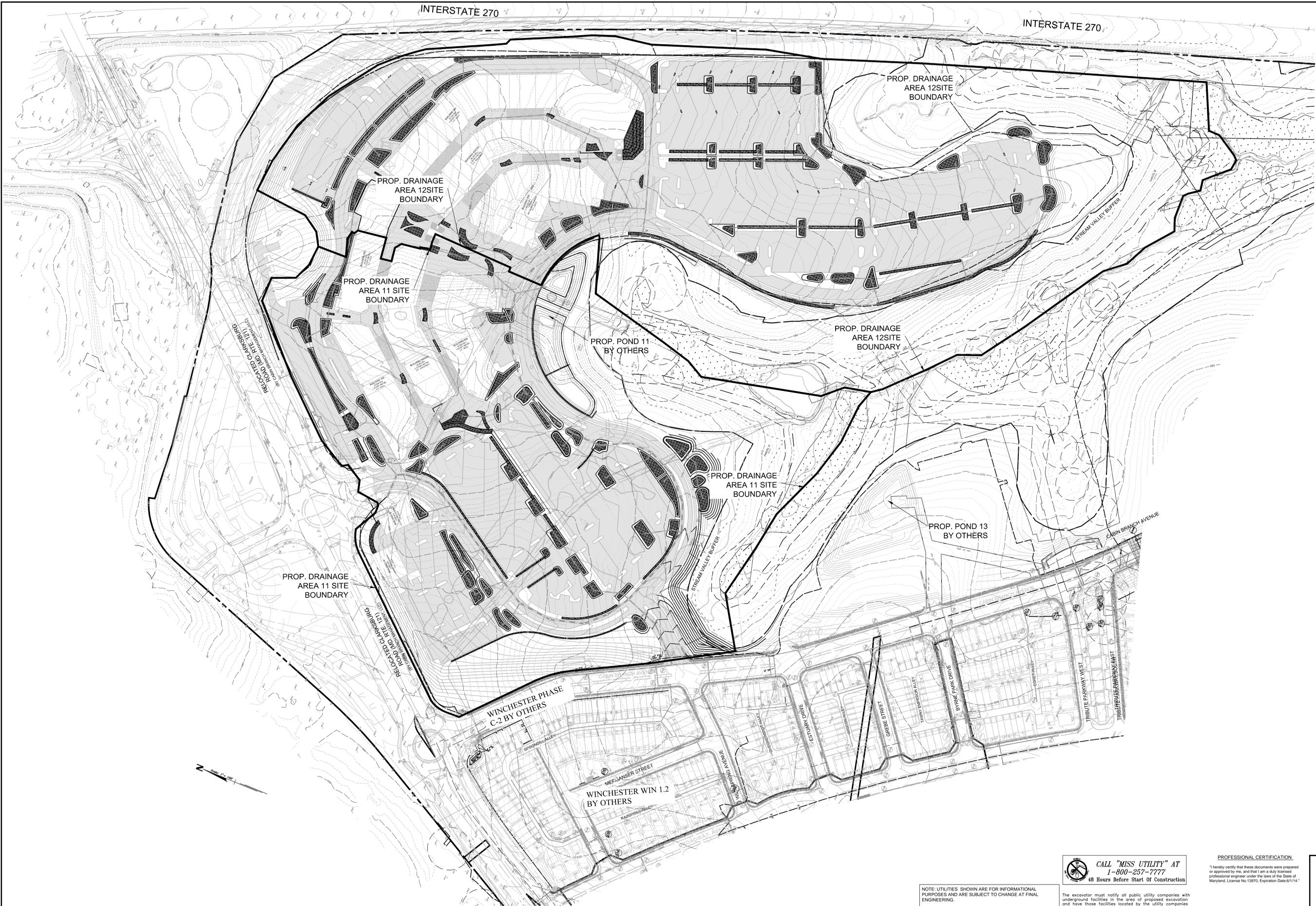
**FINAL WATER QUALITY PLAN  
COVER SHEET**

SCALE: 1" = 400'  
JOB No. 1134A3  
DATE: MAY, 2014  
SHEET No. 1 of 5

FINAL WATER QUALITY PLAN COVER SHEET

INTERSTATE 270

INTERSTATE 270



PRELIMINARY NOT FOR CONSTRUCTION



NOTE: UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES AND ARE SUBJECT TO CHANGE AT FINAL ENGINEERING.

**CALL "MISS UTILITY" AT 1-800-257-7777**  
48 Hours Before Start Of Construction

**PROFESSIONAL CERTIFICATION**  
I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13970, Expiration Date: 6/1/14.



REVISION	DATE	REVISION	DATE	REVISION	DATE

Owner/Developer:  
Simon/Clarksburg Development, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, NJ 07068  
Phone: (973) 403-3167  
Contact: Steve Dworkin  
e-mail: sdworkin@simon.com

**CLARKSBURG PREMIUM OUTLETS  
AT CABIN BRANCH  
NORTH DISTRICT, PHASE 1**

WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3

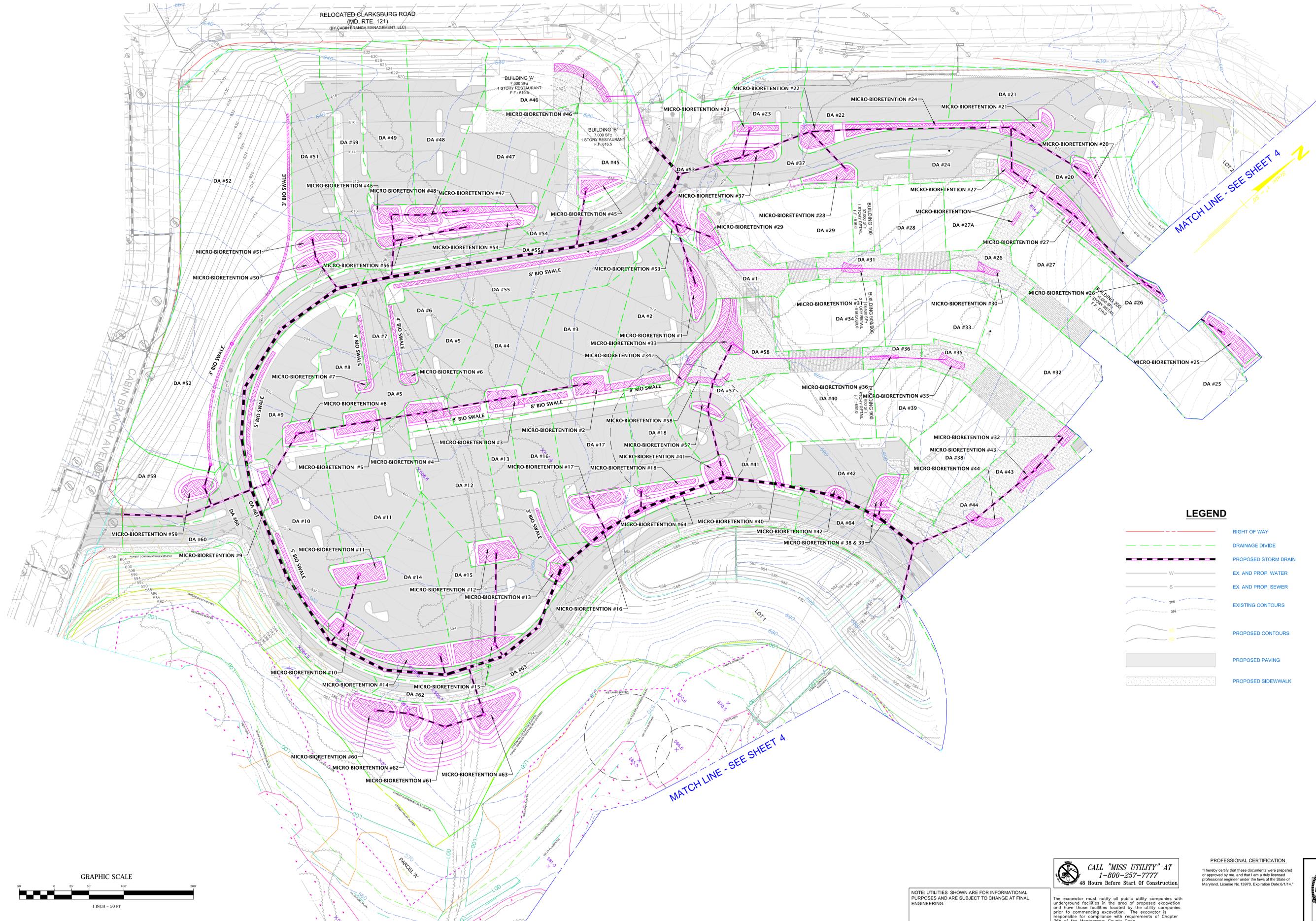
**RODGERS CONSULTING**  
Knowledge • Creativity • Enduring Values

19847 Century Boulevard  
Suite 202  
Germantown, Maryland 20874  
Ph: 301.948.4700 (Main)  
Ph: 301.233.6609 (Frederick)  
Fx: 301.948.6256  
www.rodgers.com

BASE DATA	BY	DATE
DESIGNED		
DRAWN		
REVIEWED		
RODGERS CONTACT:		
RELEASE FOR		
BY		DATE

**FINAL WATER QUALITY PLAN -  
OVERALL SITE**

SCALE: 1" = 100'  
JOB No. 1134A3  
DATE: MAY, 2014  
SHEET No. 2 of 5

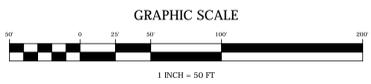


MATCH LINE - SEE SHEET 4

MATCH LINE - SEE SHEET 4

**LEGEND**

	RIGHT OF WAY
	DRAINAGE DIVIDE
	PROPOSED STORM DRAIN
	EX. AND PROP. WATER
	EX. AND PROP. SEWER
	EXISTING CONTOURS
	PROPOSED CONTOURS
	PROPOSED PAVING
	PROPOSED SIDEWALK



NOTE: UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES AND ARE SUBJECT TO CHANGE AT FINAL ENGINEERING.

CALL "MISS UTILITY" AT 1-800-257-7777 48 Hours Before Start of Construction

PROFESSIONAL CERTIFICATION  
I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13970, Expiration Date 6/1/14.



REVISION	DATE	REVISION	DATE	REVISION	DATE

Owner/Developer:  
Simon/Clarksburg Development, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, NJ 07068  
Phone: (973) 403-3167  
Contact: Steve Dworkin  
e-mail: sdworkin@simon.com

**CLARKSBURG PREMIUM OUTLETS  
AT CABIN BRANCH  
NORTH DISTRICT, PHASE 1**

WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3

**RODGERS CONSULTING**  
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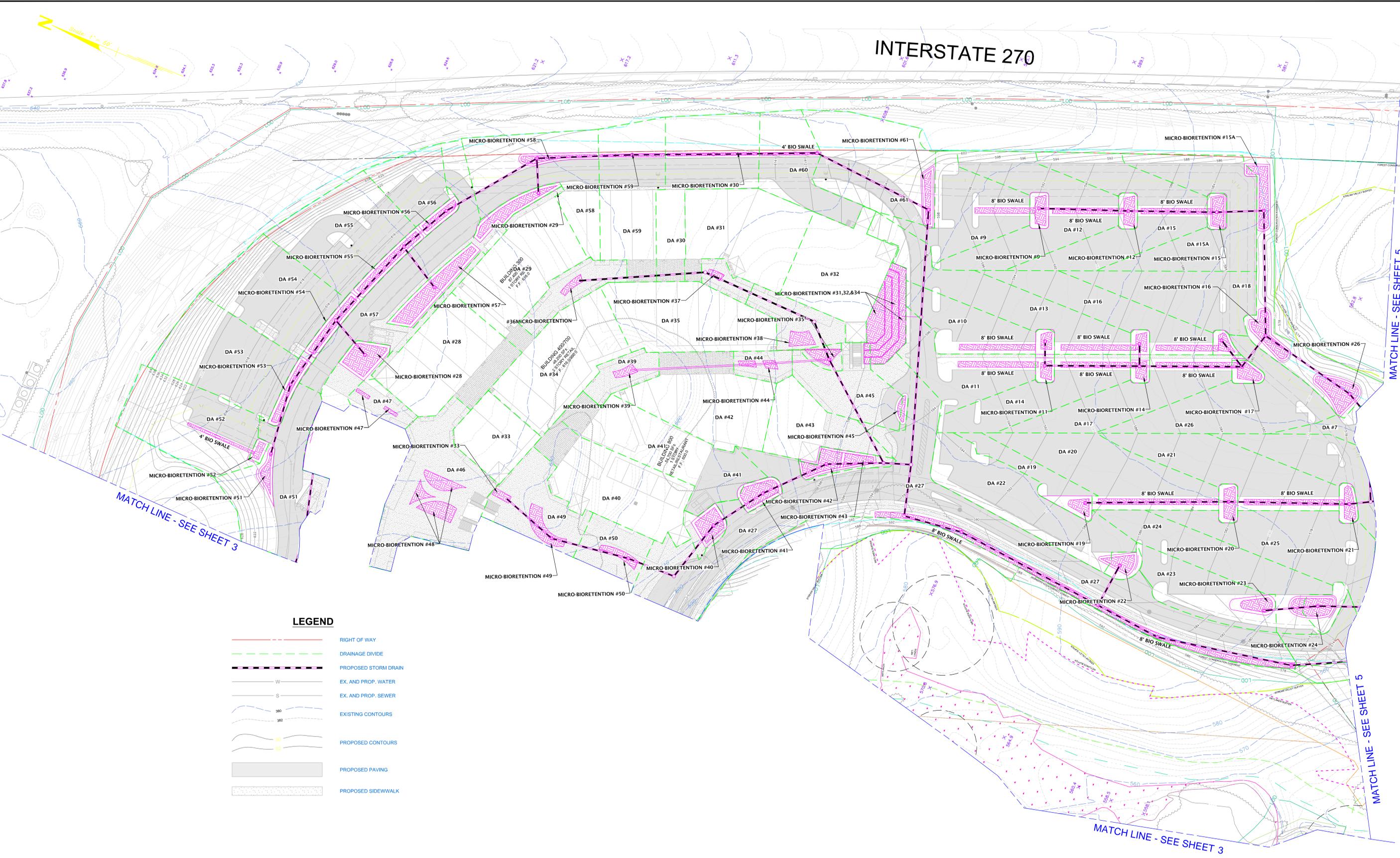
19847 Century Boulevard  
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Germantown, Maryland 20874  
Ph: 301-948-4700 (Main)  
Ph: 301-253-6609 (Frederick)  
F: 301-948-6256  
www.rodgers.com

BASE DATA	BY	DATE
DESIGNED		
DRAWN		
REVIEWED		
RODGERS CONTACT:		
RELEASE FOR		
BY		DATE

**FINAL WATER QUALITY PLAN -  
DRAINAGE AREA 11**

SCALE:	1"=50'
JOB No:	1134A3
DATE:	MAY, 2014
SHEET No:	3 of 5

# INTERSTATE 270



MATCH LINE - SEE SHEET 3

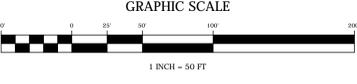
MATCH LINE - SEE SHEET 3

MATCH LINE - SEE SHEET 5

MATCH LINE - SEE SHEET 5

**LEGEND**

	RIGHT OF WAY
	DRAINAGE DIVIDE
	PROPOSED STORM DRAIN
	EX. AND PROP. WATER
	EX. AND PROP. SEWER
	EXISTING CONTOURS
	PROPOSED CONTOURS
	PROPOSED PAVING
	PROPOSED SIDEWALK



CALL "MISS UTILITY" AT 1-800-257-7777 48 Hours Before Start of Construction

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NOTE: UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES AND ARE SUBJECT TO CHANGE AT FINAL ENGINEERING.

The excavator must notify all public utility companies with underground facilities in the area of proposed excavation and have those facilities located by the utility companies prior to commencing excavation. The excavator is responsible for compliance with requirements of Chapter 36A of the Montgomery County Code.

REVISION	DATE	REVISION	DATE	REVISION	DATE

Owner/Developer:  
Simon/Clarksburg Development, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, NJ 07068  
Phone: (973) 403-3167  
Contact: Steve Dworkin  
e-mail: sdworkin@simon.com

**CLARKSBURG PREMIUM OUTLETS  
AT CABIN BRANCH  
NORTH DISTRICT, PHASE 1**  
WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3

**RODGERS CONSULTING**  
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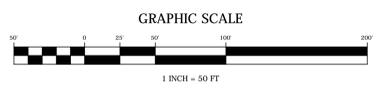
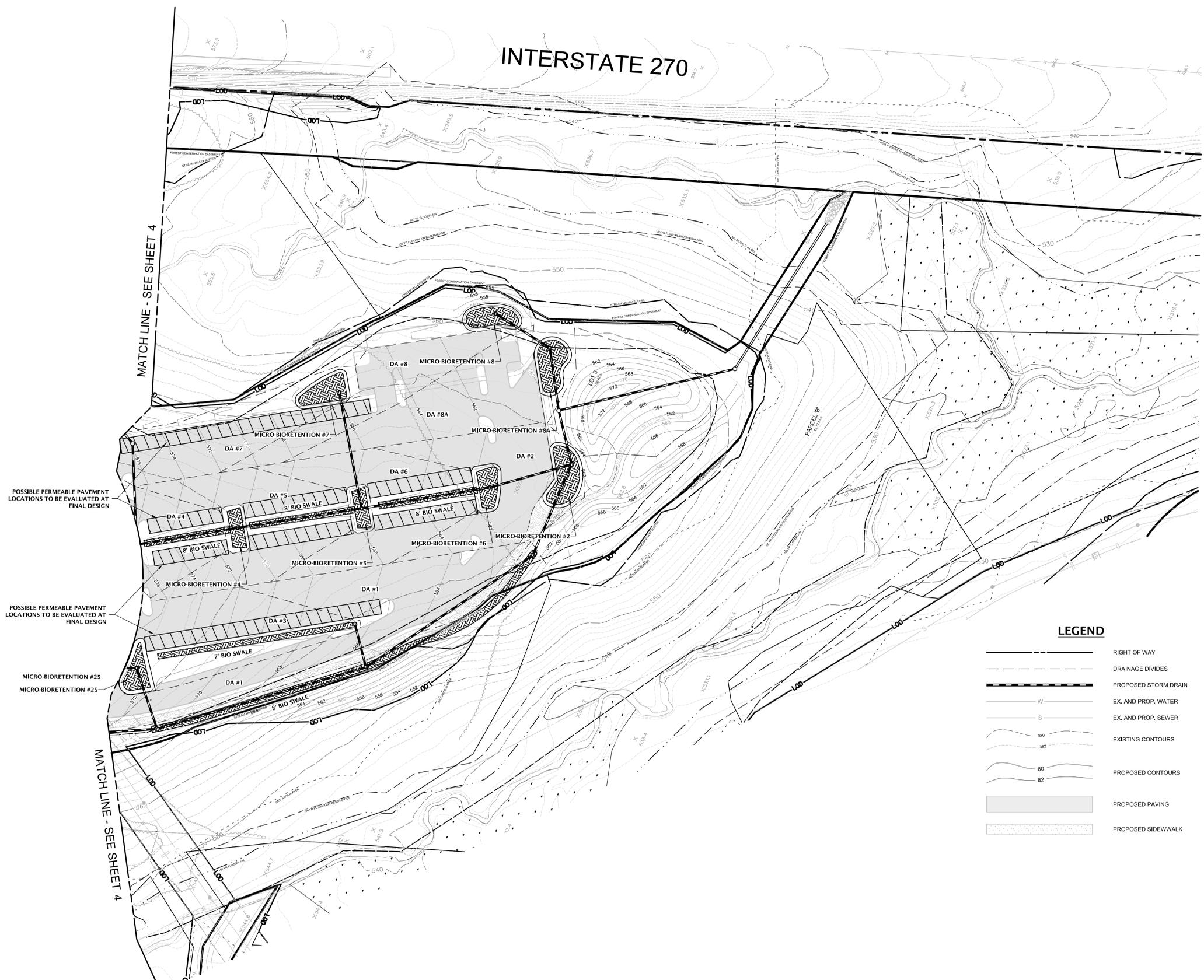
19847 Century Boulevard  
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F: 301-948-6256  
www.rodgers.com

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DESIGNED		
DRAWN		
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RELEASE FOR		
BY		DATE

## FINAL WATER QUALITY PLAN - DRAINAGE AREA 12

SCALE:	1"=50'
JOB No.:	1134A3
DATE:	MAY, 2014
SHEET No.:	4 of 5

PRELIMINARY NOT FOR CONSTRUCTION



NOTE: UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES AND ARE SUBJECT TO CHANGE AT FINAL ENGINEERING.

CALL "MISS UTILITY" AT 1-800-257-7777 48 Hours Before Start Of Construction

PROFESSIONAL CERTIFICATION  
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REVISION	DATE	REVISION	DATE	REVISION	DATE

Owner/Developer:  
Simon/Clarksburg Development, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, NJ 07068  
Phone: (973) 403-3167  
Contact: Steve Dworkin  
e-mail: sdworkin@simon.com

**CLARKSBURG PREMIUM OUTLETS  
AT CABIN BRANCH  
NORTH DISTRICT, PHASE 1**  
WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3

**RODGERS CONSULTING**  
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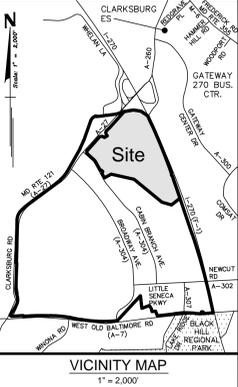
19847 Century Boulevard  
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BASE DATA	BY	DATE
DESIGNED		
DRAWN		
REVIEWED		
RODGERS CONTACT:		
RELEASE FOR		
BY		DATE

**FINAL WATER QUALITY PLAN -  
DRAINAGE AREA 12**

SCALE:	1"=50'
JOB No.	1134A3
DATE	MAY, 2014
SHEET No.	5 of 5

# CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1 SITE PLAN 820140160



PRELIMINARY NOT FOR CONSTRUCTION

**Applicant/Owner:**  
SIMON/CLARKSBURG DEVELOPMENT, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, New Jersey 07068  
Phone: (973) 403-3167  
Contact: Mr. Steve Dworkin

**Attorney:**  
LERCH, EARLY & BREWER, CHTD.  
3 Bethesda Metro Center  
Suite 460  
Bethesda, Maryland 20814  
Phone: (301) 841-3826  
Contact: Mr. Robert Harris

**Architect:**  
ARCHITECTS ORANGE  
144 North Orange Street  
Orange, California 92866  
Phone: (714) 639-9860  
Contact: Mr. Darrel Hebenstreit

**Civil Engineer/Land Planner:**  
RODGERS CONSULTING, INC.  
19847 Century Boulevard, Suite 200  
Germantown, Maryland 20874  
Phone: (301) 948-4700  
Contact: Mr. Gary F. Unterberg

**Landscape Architect:**  
TERRAPLAN  
20 Champlain Boulevard, Suite 102  
Toronto, Ontario, M3H 2Z1, Canada  
Phone: (416) 638-4911  
Contact: Mr. Matthew Bernstein

**Development Team:**



ALL ROADWAY AND BIKE PATH IMPROVEMENTS SHOWN WITHIN THE PUBLIC RIGHT OF WAY FOR MD 121 (CLARKSBURG ROAD) WILL BE BUILT BY CABIN BRANCH MANAGEMENT, LLC, UNDER A SEPARATE PHASE OF THIS PROJECT.

THIS SITE PLAN REFINES AND SUPERSEDES SPECIFIC AREAS COVERED BY THE INFRASTRUCTURE ROADS ONLY SITE PLAN No. 820050150 APPROVED 6/14/07 AND THE LIMITED PLAN AMENDMENT Nos. 82005015A APPROVED 4/30/08, 82005015B APPROVED 12/1/11, 82005015C APPROVED 11/27/12, 82005015D APPROVED 5/2/13, 82005015E APPROVED 5/22/14 AND 820050015F APPROVED 9/11/14.

NOTE: UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES AND ARE SUBJECT TO CHANGE AT FINAL ENGINEERING.

Sheet Title	File Name
Cover Sheet	07-SITE-820140160-001
Approval Sheet	07-SITE-820140160-002
Overall Project and Zoning Information	07-SITE-820140160-003
Sheet Reference and Composite Site Plan	07-SITE-820140160-004
Site Plan	07-SITE-820140160-005
Site Plan	07-SITE-820140160-006
Site Plan	07-SITE-820140160-007
Site Plan	07-SITE-820140160-008
Site Plan	07-SITE-820140160-009
Site Plan	07-SITE-820140160-010
Site Plan	07-SITE-820140160-011
Site Plan	07-SITE-820140160-012
Site Plan	07-SITE-820140160-013
Site Notes and Details	07-SITE-820140160-014
Site Notes and Details	07-SITE-820140160-015
Local Area Drawing	20-LOCAL-820140160-016
Sheet Reference and Composite Landscape Plan	08-LL-820140160-001
Landscape Plan Area 5	08-LL-820140160-002
Landscape Plan Area 6	08-LL-820140160-003
Landscape Plan Area 7	08-LL-820140160-004
Landscape Plan Area 8	08-LL-820140160-005
Landscape Plan Area 9	08-LL-820140160-006
Landscape Plan Area 10	08-LL-820140160-007
Landscape Plan Area 11	08-LL-820140160-008
Landscape Plan Area 12	08-LL-820140160-009
Plant List and Notes	08-LL-820140160-010
Planting Details	08-LL-820140160-011
Landscape Details	08-LL-820140160-012
Landscape Details	08-LL-820140160-013
Landscape Details	08-LL-820140160-014
Landscape Details	08-LL-820140160-015
Electrical Site Lighting Plan - Area 'A'	08-LL-820140160-016
Electrical Site Lighting Plan - Area 'B'	08-LL-820140160-017
Electrical Site Lighting Plan - Area 'C'	08-LL-820140160-018
Electrical Site Lighting Plan - Area 'D'	08-LL-820140160-019
Electrical Site Lighting Plan - Area 'E'	08-LL-820140160-020
Electrical Site Lighting Plan - Area 'F'	08-LL-820140160-021
Electrical Site Lighting Plan - Area 'G'	08-LL-820140160-022
Electrical Site Lighting Plan - Area 'H'	08-LL-820140160-023
Lighting Details	08-LL-820140160-024
Central Court View 1 and Precedents	09-ARCH-820140160-001
Central Court View 2 and Precedents	09-ARCH-820140160-002
West Court View 1 and Precedents	09-ARCH-820140160-003
West Court View 2 and Precedents	09-ARCH-820140160-004
East Court View 1 and Precedents	09-ARCH-820140160-005
East Court View 2 and Precedents	09-ARCH-820140160-006
Site Section	09-ARCH-820140160-007
High Street Floor Plan	09-ARCH-820140160-008
Low Street Floor Plan	09-ARCH-820140160-009
Bldg. 100 Floor Plan	09-ARCH-820140160-010
Bldg. 200 Floor Plan	09-ARCH-820140160-011
Bldg. 300 Floor Plan	09-ARCH-820140160-012
Bldg. 400 Floor Plan	09-ARCH-820140160-013
Bldg. 500 Floor Plan	09-ARCH-820140160-014
Bldg. 600 Floor Plan	09-ARCH-820140160-015
Bldg. 700 Floor Plan	09-ARCH-820140160-016
Bldg. 800 Floor Plan	09-ARCH-820140160-017
Bldg. 900 Floor Plan	09-ARCH-820140160-018
Overall Roof Plan	09-ARCH-820140160-019
Bldg. 100 Roof Plan	09-ARCH-820140160-020
Bldg. 200 Roof Plan	09-ARCH-820140160-021
Bldg. 300 Roof Plan	09-ARCH-820140160-022
Bldg. 400/700 Roof Plan	09-ARCH-820140160-023
Bldg. 500/600 Roof Plan	09-ARCH-820140160-024
Bldg. 800 Roof Plan	09-ARCH-820140160-025
Bldg. 900 Roof Plan	09-ARCH-820140160-026
Bldg. 100 Elevations	09-ARCH-820140160-027
Bldg. 200 Elevations	09-ARCH-820140160-028
Bldg. 300 Elevations	09-ARCH-820140160-029
Bldg. 400/700 Elevations	09-ARCH-820140160-030
Bldg. 500/600 Elevations	09-ARCH-820140160-031
Bldg. 800 Elevations	09-ARCH-820140160-032
Bldg. 900 Elevations	09-ARCH-820140160-033
Bldg. 100 Colored Elevations	09-ARCH-820140160-034
Bldg. 200 Colored Elevations	09-ARCH-820140160-035
Bldg. 300 Colored Elevations	09-ARCH-820140160-036
Bldg. 400/700 Colored Elevations	09-ARCH-820140160-037
Bldg. 500/600 Colored Elevations	09-ARCH-820140160-038
Bldg. 800 Colored Elevations	09-ARCH-820140160-039
Bldg. 900 Colored Elevations	09-ARCH-820140160-040
Rear Buildings Plans and Elevations	09-ARCH-820140160-041
Rear Buildings Plans and Elevations and Details	09-ARCH-820140160-042
	09-ARCH-820140160-043

**DEVELOPER'S CERTIFICATE**  
The Undersigned agrees to execute all the features of the Site Plan Approval No. 820140160, including Approval Conditions, Development Program, and Central Site Plan.

Developer's Name: \_\_\_\_\_ Company: \_\_\_\_\_ Contact Person: \_\_\_\_\_

Address: \_\_\_\_\_

Phone: \_\_\_\_\_

Signature: \_\_\_\_\_

**PROFESSIONAL CERTIFICATION**  
I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13970, Expiration Date 6/1/16.

**CALL "MISS UTILITY" AT 1-800-257-7777 48 Hours Before Start of Construction**

**SITE PLAN No. 820140160**

REVISION	DATE	REVISION	DATE	REVISION	DATE
				ADDRESSED DRC COMMENTS	10/1/14

**Owner/Developer:**  
Simon/Clarksville Development, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, NJ 07068  
Phone: (973) 403-3167  
Contact: Steve Dworkin  
e-mail: sdworkin@simon.com

**CLARKSBURG PREMIUM OUTLETS  
AT CABIN BRANCH  
NORTH DISTRICT, PHASE 1**

WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3

19847 Century Boulevard  
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Ph: 301.948.4700 (Main)  
Ph: 301.233.6409 (Frederick)  
F: 301.948.6256  
www.rodgers.com

BASE DATA	BY	DATE
DESIGNED		
DRAWN		
REVIEWED		
RODGERS CONTACT:		
RELEASE FOR		
BY: _____	DATE: _____	

**COVER SHEET**

SCALE: 1" = 400'

JOB No. 1134A3

DATE: MAY, 2014

SHEET No. S1 of 16

N:\MD-Montgomery\Clarksville Region\dwg\Cabin Branch\RetailSite Plan\01-COVER-SP RETAIL.dwg Layout1 Oct 01, 2014, 12:13pm

07-SITE-820140160-001: CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1 SITE PLAN COVER SHEET

GENERAL NOTES

- 1. The site is comprised of part of Parcels 187, 645 & 345 as shown on the Montgomery County Tax Maps EV23 & EV33. The gross tract area is 84.24+/- acres.
2. The property Tax Account Identification Numbers are 03470044, 00016916 & 03470055.
3. The site is located within Montgomery County Election District #2.
4. The site is to be served by public water & sewer systems. This site is presently classified in water and sewer category W-3, S-3 found on W.S.S.C. 230NW14.
5. Horizontal Datum: Maryland Coordinate System - N.A.D. 83/91, the Vertical Datum: NGVD29.
6. Boundary information is from tax map EV and boundary survey of tax map EV prepared by Rodgers Consulting, Inc. dated February 2002.
7. The 5-foot contour interval topographic information is from M-NCP&PC information, and the site's 2-foot contour interval topographic information was taken from Wings Aerial Mapping dated March, 2001.
8. The site is located within the Clarksburg Planning Area 13.
9. There is one perennial stream, 2.69 ac of wetlands, and 6.54 ac of floodplain on site. The site is within the Little Seneca Creek/Cabin Branch Watershed.
10. No rare, threatened, or endangered species of plants or animals were observed on the site at this time.
11. The locations of existing underground utilities are approximate locations as per available utility company records. The exact location of all underground utilities should be verified by "Miss Utility" (1-800-257-7777) prior to any excavation. Rodgers Consulting, Inc. does not express or imply any guarantee or warranty as to the location or existence of any underground utility.
12. Lot lines and lot areas are preliminary and subject to change on the Final Plat.
13. Water, sewer, and storm drain design is shown for informational purposes only and is subject to change.
14. NR/FSD number and file: 42002008R, approved 4/10/03.
15. SWM Concept Plan file: 207133, approved 5/13/04.
16. Preliminary Water Quality Plan approval date: June 4, 2003. Revision approved May 13, 2004, revised 8/25/14.
17. The Preliminary Plan was approved June 9, 2008, case #12003110B, revised 8/25/14.
18. The Preliminary Plan required the submission of "Infrastructure & Roads Only Site Plan" 60 days prior to Site Plan submittal. The Infrastructure Plans were approved June 14, 2007 by MNCPPC #820050150 and amended on June 9, 2008 #82005015A, March 7, 2012 #82005015B, November 27, 2012 #82005015C & May 2, 2013 #82005015D.
19. This plan incorporates a PUE in alleys and adjacent to some public R.O.W. See detailed PUE plan and utility letters for details.
20. Storm water management facilities will be reviewed and inspected by DPS Water Resources Section.
21. All private roads will be built to the Tertiary Road Standards for Montgomery County.
22. The building footprints shown on the site plan are illustrative. Final building locations within the building envelope will be determined during the building permit process. Please refer to the zoning data table for development standards such as, setbacks, building restriction lines and lot coverage. Other limitations for site development may also be included in the conditions of the Planning Board's approval.
23. Building envelopes designate the maximum extents of the building footprint. The footprint of the building will be determined during the final architectural design. Revisions to sidewalk or plaza area due to footprint shift may be subject to amendment of the Site Plan.

SITE DATA

- 1. Zone: MXP, RMX-1/ TDR
2. Cabin Branch Site Area (Gross): 535.04 Ac ±
3. Clarksburg Premium Outlets at Cabin Branch, North District- Phase 1 Site Area: 84.24 Ac ±
4. 100 year floodplain is based on a study prepared by Rodgers Consulting, Inc. dated March 26, 2002, Floodplain application number 204747. There are 31.63 Ac ± of floodplain in Cabin Branch and 2.60 Ac ± within this application.
5. Proposed Use: Retail & Restaurants
6. Utility Companies:
- Allegheny Power/Potomac Edison
- Verizon
- Comcast
- Washington Gas
- WSSC

DEVELOPMENT INFORMATION GRID:

Table with columns: Method of Development, Zoning, Overlay Zone, Acres, Des. Code, Prev. Ex. Res. DU/Comm'l sf, Retained Res. DU/Comm'l sf, Proposed Res. DU/Comm'l sf, MPDU, Other Affordable Housing, # of Lots, # of Outlets.

\* Maximum number of Dwelling Units allowed by zoning

Table with columns: Acres Dedicated to Public Use, Public Road ROWs, Other ROWs, Schools, Parks, Other, Total.

CABIN BRANCH NEIGHBORHOOD OVER ALL DEVELOPMENT TRACKING

Table with columns: Proposed Zone (I), MPD Zone (II), Proposed RMX1/TDR Zone (I), TOTAL (I).

Table with columns: Proposed Zone (I), MPD Zone (II), Proposed RMX1/TDR Zone (I), TOTAL (I).

Table with columns: Proposed Zone (I), MPD Zone (II), Proposed RMX1/TDR Zone (I), TOTAL (I).

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Table with columns: Proposed Zone (I), MPD Zone (II), Proposed RMX1/TDR Zone (I), TOTAL (I).

1) SECTION 10-10.2.1 STANDARD METHOD OF DEVELOPMENT (REGULATORY) (RM1/TDR) ZONE... STANDARD METHOD PROJECTS IN THESE ZONES MUST OCCUR IN CONFORMANCE WITH THE DEVELOPMENT METHOD STANDARDS AND SPECIAL REGULATIONS OF THEIR ZONING ZONES AS CONTAINED IN 29-21-131, 29-21-132 & 29-21-139. THE MAXIMUM DENSITY WITH THE USE OF TDRS IS 11 DWELLING UNITS PER ACRE. THE LAND USES ALLOWED ARE THOSE LISTED IN THEIR ZONING ZONE. DEVELOPMENT UNDER THE METHOD MUST COMPLY WITH OFFSET, NUMBER, LENGTH, AND OTHER RULES CONTAINED IN THE APPLICABLE MASTER OR SECTOR PLAN APPROVED BY THE DISTRICT COUNCIL. THE APPROVED PRELIMINARY PLAN 12003110B AND THE SUBJECT TO CHANGE PER CONDITION WHICH STATES: FINAL APPROVAL OF THE NUMBER AND LOCATIONS OF THE BUILDINGS, DWELLING UNITS, ON-SITE PARKING, SITE CIRCULATION, SIDEWALKS AND BIKE PATHS WILL BE DETERMINED AT SITE PLAN.

CABIN BRANCH NEIGHBORHOOD - MXP DATA TABLE

Table with columns: MXP Zone (I), Development Data Standard Method, MXP STD., Master Plan Zoning, Preliminary Plan, Site Plan.

DEVELOPMENT PROGRAM

RETAIL.....437,000 sf
RESTAURANT.....13,000 sf

Table with columns: PLAN TYPE, PLAN NUMBER, DATE.

Table with columns: GROSS AREA (Acres), GREEN AREA REQUIRED (Acres), GREEN AREA PROVIDED (Acres).



DEVELOPER'S CERTIFICATE
The Undersigned agrees to execute all the features of the Site Plan Approval No. 820140160, including Approval Conditions, Development Program, and Control Site Plan.

CALL "MISS UTILITY" AT 1-800-257-7777 48 Hours Before Start of Construction

PROFESSIONAL CERTIFICATION
I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13970, Expiration Date 6/1/16.



SITE PLAN No. 820140160

CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1

RODGERS CONSULTING logo and contact information: 19847 Century Boulevard, Suite 200, Germantown, Maryland 20874.

Table with columns: BY, DATE, BASE DATA, DESIGNED, DRAWN, REVIEWED, RELEASE FOR.

OVERALL PROJECT AND ZONING INFORMATION

Table with columns: SCALE, JOB No., DATE, SHEET No.

Table with columns: REVISION, DATE, REVISION, DATE, OWNER/DEVELOPER: Simon/Clarksburg Development, LLC.

PRELIMINARY NOT FOR CONSTRUCTION (vertical text on the right edge)



PRELIMINARY NOT FOR CONSTRUCTION

07-SITE-820140160-004: CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1 SHEET REFERENCE AND COMPOSITE SITE PLAN

REVISION	DATE	REVISION	DATE	REVISION	DATE
		ADDRESSED DRC COMMENTS	10/1/14		

Owner/Developer:  
 Simon/Clarksburg Development, LLC  
 c/o Simon Premium Outlets  
 105 Eisenhower Parkway  
 Roseland, NJ 07068  
 Phone: (973) 403-3167  
 Contact: Steve Dworkin  
 e-mail: sdworkin@simon.com

## CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1

WSSC MAP #'s 231NW14 & 231NW13  
 TAX MAP GRID #'s EV23 & EV3

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 www.rodgers.com

BY	DATE
BASE DATA	
DESIGNED	
DRAWN	
REVIEWED	
RODGERS CONTACT:	
RELEASE FOR	
BY _____	DATE _____

## SHEET REFERENCE AND COMPOSITE SITE PLAN

SCALE: 1" = 100'  
 JOB No. 1134A3  
 DATE: MAY, 2014  
 SHEET No. S4 of 16

**DEVELOPER'S CERTIFICATE**  
 The Undersigned agrees to execute all the features of the Site Plan Approval No. 820140160, including Approval Conditions, Development Program, and Control Site Plan.  
 Developer's Name: \_\_\_\_\_  
 Address: \_\_\_\_\_  
 Phone: \_\_\_\_\_  
 Signature: \_\_\_\_\_

**PROFESSIONAL CERTIFICATION**  
 I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13870, Expiration Date 6/1/16.

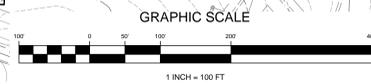
SITE PLAN No.  
820140160

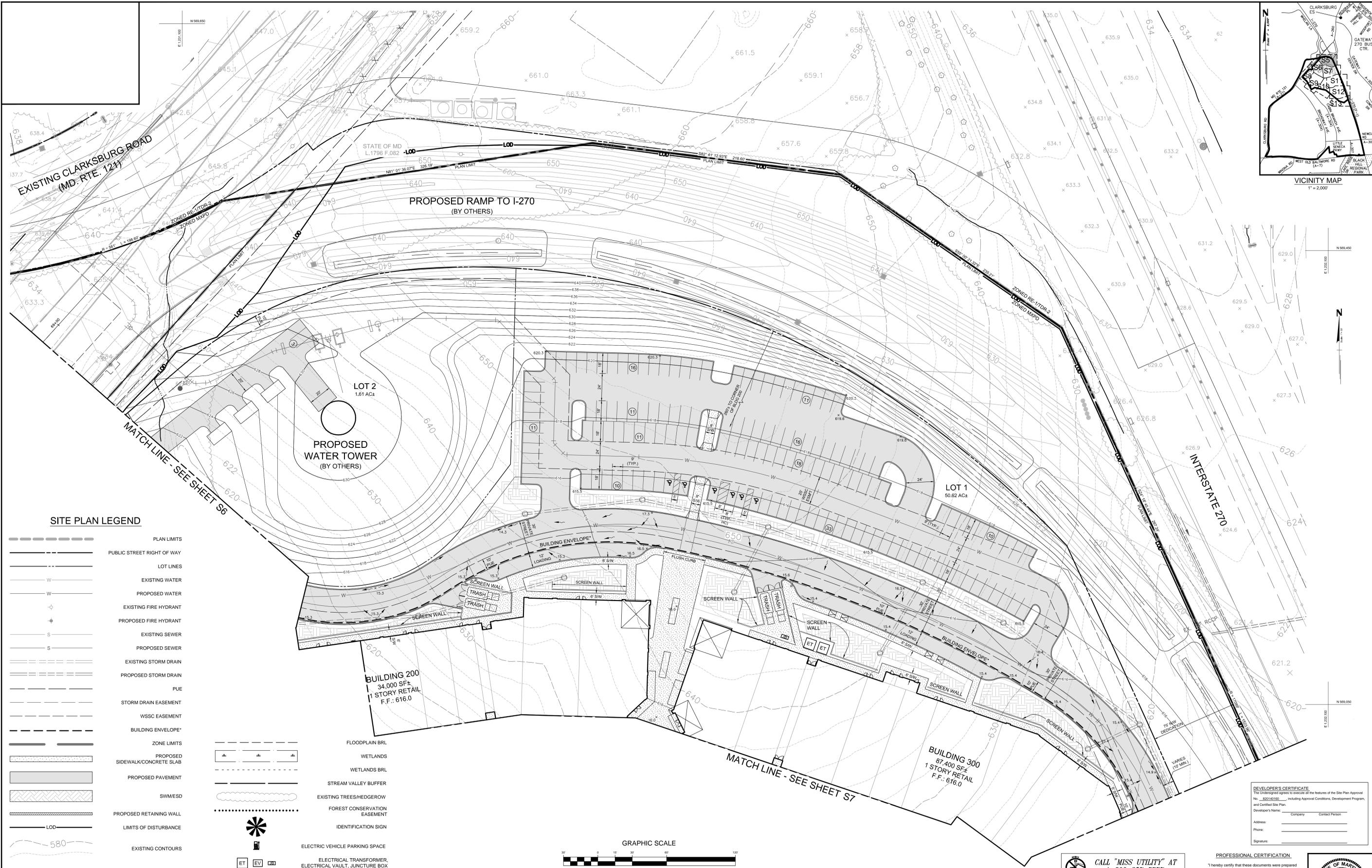
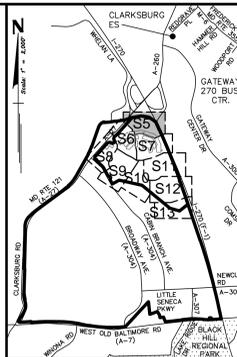
**CALL "MISS UTILITY" AT  
 1-800-257-7777  
 48 Hours Before Start of Construction**

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NOTE: UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES AND ARE SUBJECT TO CHANGE AT FINAL ENGINEERING.





**SITE PLAN LEGEND**

- PLAN LIMITS
- PUBLIC STREET RIGHT OF WAY
- LOT LINES
- EXISTING WATER
- PROPOSED WATER
- EXISTING FIRE HYDRANT
- PROPOSED FIRE HYDRANT
- EXISTING SEWER
- PROPOSED SEWER
- EXISTING STORM DRAIN
- PROPOSED STORM DRAIN
- PUE
- STORM DRAIN EASEMENT
- WSSC EASEMENT
- BUILDING ENVELOPE\*
- ZONE LIMITS
- PROPOSED SIDEWALK/CONCRETE SLAB
- PROPOSED PAVEMENT
- SWM/ESD
- PROPOSED RETAINING WALL
- LIMITS OF DISTURBANCE
- EXISTING CONTOURS
- PROPOSED CONTOURS
- POND/STREAM
- 100 YEAR FLOODPLAIN
- FLOODPLAIN BRL
- WETLANDS
- WETLANDS BRL
- STREAM VALLEY BUFFER
- EXISTING TREES/HEDGEROW
- FOREST CONSERVATION EASEMENT
- IDENTIFICATION SIGN
- ELECTRIC VEHICLE PARKING SPACE
- ELECTRICAL TRANSFORMER, ELECTRICAL VAULT, JUNCTURE BOX
- GREASE TRAP



\*BUILDING ENVELOPE: SEE ARCHITECTURE SHEETS FOR BUILDING INFORMATION. SEE LANDSCAPE SHEETS FOR PLAZA/PEDESTRIAN AREAS. SEE GENERAL NOTE 23 ON SHEET S3.

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Address: \_\_\_\_\_  
Phone: \_\_\_\_\_  
Signature: \_\_\_\_\_

**SITE PLAN No. 820140160**



REVISION	DATE	REVISION	DATE	REVISION	DATE
		ADDRESSED DRC COMMENTS	10/1/14		

Owner/Developer:  
Simon Clarksburg Development, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, NJ 07068  
Phone: (973) 403-3167  
Contact: Steve Dworkin  
e-mail: sdworkin@simon.com

**CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1**  
WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3

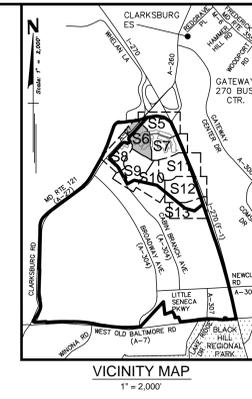


BASE DATA	BY	DATE
DESIGNED		
DRAWN		
REVIEWED		
RODGERS CONTACT:		
RELEASE FOR		

**SITE PLAN**

SCALE: 1" = 30'
JOB No. 1134A3
DATE: MAY, 2014
SHEET No. S5 of 18

MATCH LINE - SEE SHEET S5



PRELIMINARY NOT FOR CONSTRUCTION



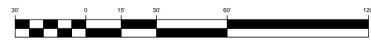
MATCH LINE - SEE SHEET S8

MATCH LINE - SEE SHEET S9

SITE PLAN LEGEND

- PLAN LIMITS
PUBLIC STREET RIGHT OF WAY
LOT LINES
EXISTING WATER
PROPOSED WATER
EXISTING FIRE HYDRANT
PROPOSED FIRE HYDRANT
EXISTING SEWER
PROPOSED SEWER
EXISTING STORM DRAIN
PROPOSED STORM DRAIN
PUE
STORM DRAIN EASEMENT
WSSC EASEMENT
BUILDING ENVELOPE\*
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PROPOSED SIDEWALK/CONCRETE SLAB
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SWM/ESD
PROPOSED RETAINING WALL
LIMITS OF DISTURBANCE
EXISTING CONTOURS
PROPOSED CONTOURS
POND/STREAM
100 YEAR FLOODPLAIN
FLOODPLAIN BRL
WETLANDS
WETLANDS BRL
STREAM VALLEY BUFFER
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IDENTIFICATION SIGN
ELECTRIC VEHICLE PARKING SPACE
ELECTRICAL TRANSFORMER, ELECTRICAL VAULT, JUNCTURE BOX
GREASE TRAP

GRAPHIC SCALE



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SITE PLAN No. 820140160



Table with columns: REVISION, DATE, REVISION, DATE, REVISION, DATE. Includes revision history for the site plan.

Owner/Developer: Simon Clarksburg Development, LLC c/o Simon Premium Outlets 105 Eisenhower Parkway Roseland, NJ 07068 Phone: (973) 403-3167 Contact: Steve Dworkin e-mail: sdworkin@simon.com

CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1

WSSC MAP #'S 231NW14 & 231NW13 TAX MAP GRID #'S EV23 & EV3

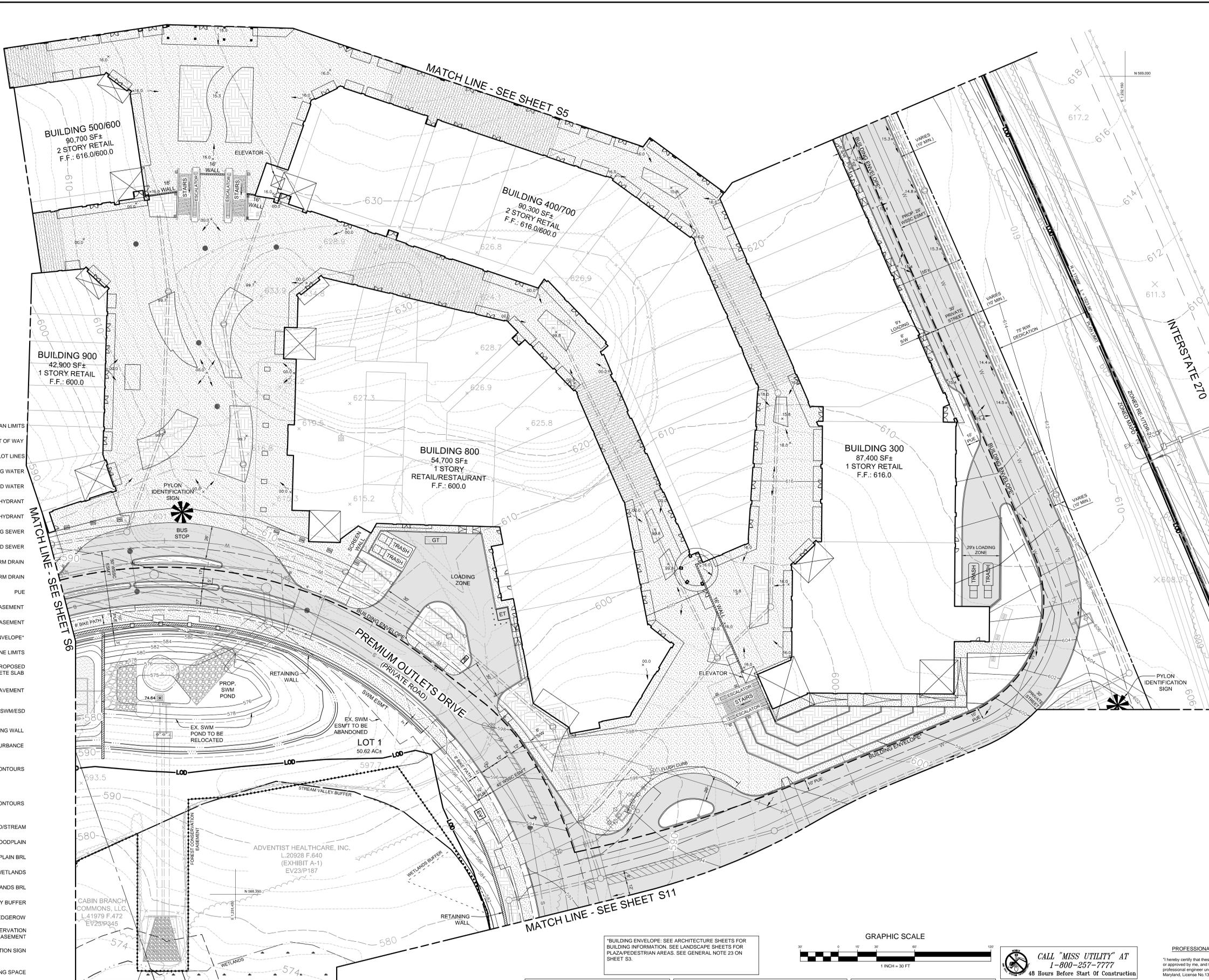
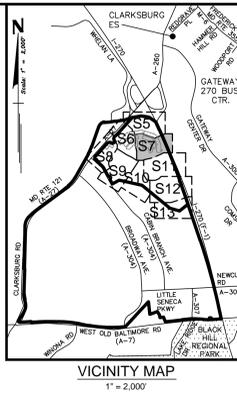


19847 Century Boulevard Suite 200 Germantown, Maryland 20874 Ph: 301.948.4700 (Main) Ph: 301.233.6029 (Frederick) Fx: 301.948.6256 www.rodgers.com

Table with columns: BASE DATA, DESIGNED, DRAWN, REVIEWED, RODGERS CONTACT, RELEASE FOR.

SITE PLAN

SCALE: 1" = 30' JOB No. 1134A3 DATE: MAY, 2014 SHEET No. S6 of 16



SITE PLAN LEGEND

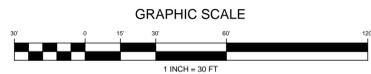
- PLAN LIMITS
PUBLIC STREET RIGHT OF WAY
LOT LINES
EXISTING WATER
PROPOSED WATER
EXISTING FIRE HYDRANT
PROPOSED FIRE HYDRANT
EXISTING SEWER
PROPOSED SEWER
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STREAM VALLEY BUFFER
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FOREST CONSERVATION EASEMENT
IDENTIFICATION SIGN
ELECTRIC VEHICLE PARKING SPACE
ELECTRICAL TRANSFORMER, ELECTRICAL VAULT, JUNCTURE BOX
GREASE TRAP

MATCH LINE - SEE SHEET S6

MATCH LINE - SEE SHEET S5

MATCH LINE - SEE SHEET S11

MATCH LINE - SEE SHEET S10



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PROFESSIONAL CERTIFICATION I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13970, Expiration Date 6/1/16.

DEVELOPER'S CERTIFICATE The Undersigned agrees to execute all the features of the Site Plan Approval No. 820140160, including Approval Conditions, Development Program, and Control Site Plan. Developer's Name: Simon Clarksburg Development, LLC Company: Simon Clarksburg Development, LLC Contact Person: Steve Dworkin Address: 105 Eisenhower Parkway, Roseland, NJ 07068 Phone: (973) 403-3167 Signature: Steve Dworkin

SITE PLAN No. 820140160



Table with columns: REVISION, DATE, REVISION, DATE, REVISION, DATE, REVISION, DATE. Includes revision history for the site plan.

Owner/Developer: Simon Clarksburg Development, LLC c/o Simon Premium Outlets 105 Eisenhower Parkway Roseland, NJ 07068 Phone: (973) 403-3167 Contact: Steve Dworkin e-mail: sdworkin@simon.com

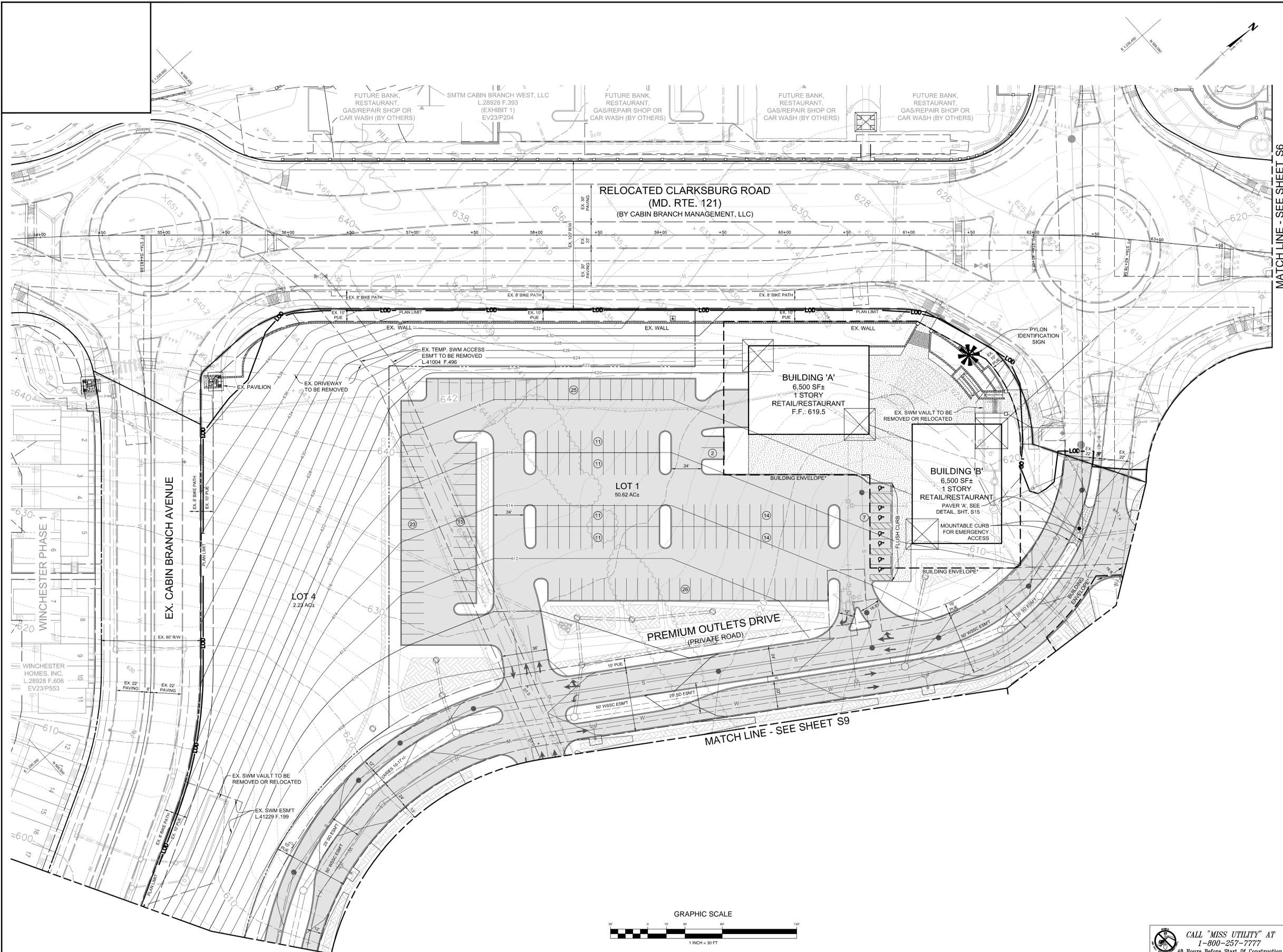
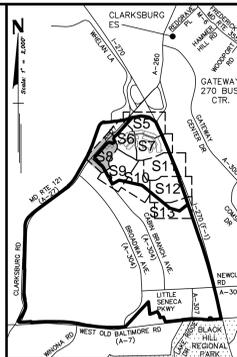
CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1

RODGERS CONSULTING logo and contact information: 19847 Century Boulevard Suite 200 Germantown, Maryland 20874 Ph: 301.948.4700 (Main) Ph: 301.233.6029 (Frederick) Fx: 301.948.6256 www.rodgers.com

Table with columns: BASE DATA, DESIGNED, DRAWN, REVIEWED, RODGERS CONTACT, RELEASE FOR. Includes a signature line for the engineer.

SITE PLAN title block containing scale (1" = 30'), job number (1134A3), date (MAY, 2014), sheet number (S7 of 16), and project name (CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1 SITE PLAN).

PRELIMINARY NOT FOR CONSTRUCTION



**SITE PLAN LEGEND**

- PLAN LIMITS
- PUBLIC STREET RIGHT OF WAY
- LOT LINES
- EXISTING WATER
- PROPOSED WATER
- EXISTING FIRE HYDRANT
- PROPOSED FIRE HYDRANT
- EXISTING SEWER
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- WSSC EASEMENT
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- PROPOSED PAVEMENT
- SWMESD
- PROPOSED RETAINING WALL
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- WETLANDS BRL
- STREAM VALLEY BUFFER
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- IDENTIFICATION SIGN
- ELECTRIC VEHICLE PARKING SPACE
- ELECTRICAL TRANSFORMER, ELECTRICAL VAULT, JUNCTURE BOX
- GREASE TRAP



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**PROFESSIONAL CERTIFICATION**

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**SITE PLAN No. 820140160**

REVISION	DATE	REVISION	DATE	REVISION	DATE

Owner/Developer:  
Simon Clarksburg Development, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, NJ 07068  
Phone: (973) 403-3167  
Contact: Steve Dworkin  
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WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3



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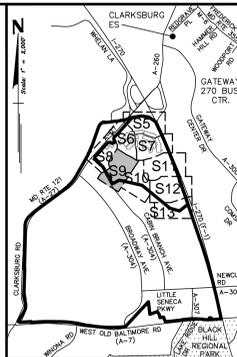
BASE DATA	BY	DATE
DESIGNED		
DRAWN		
REVIEWED		

RODGERS CONTACT:  
RELEASE FOR:  BY: \_\_\_\_\_ DATE: \_\_\_\_\_

**SITE PLAN**

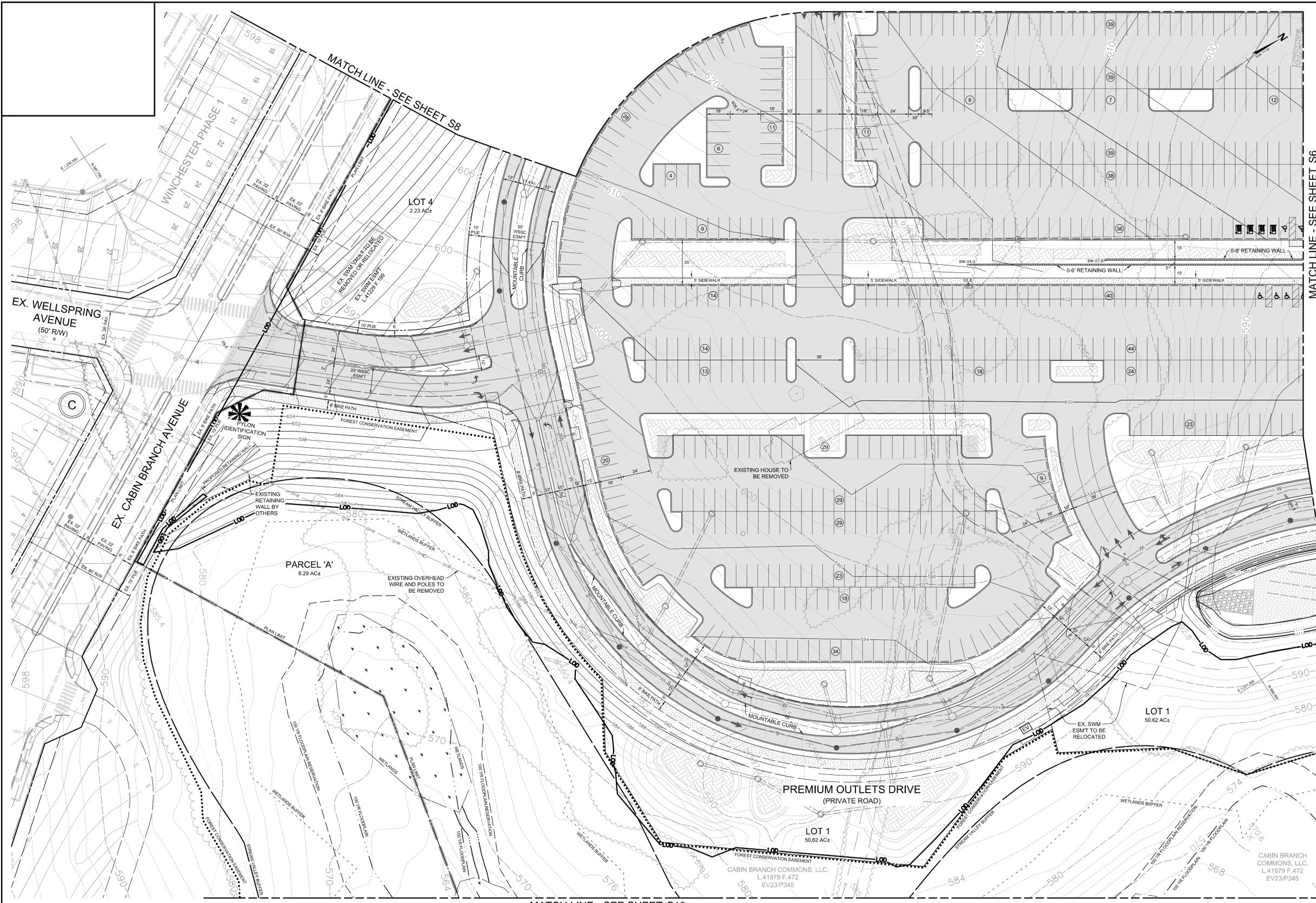
SCALE:	1" = 30'
JOB No.:	1134A3
DATE:	MAY, 2014
SHEET No.:	S8 of 18

PRELIMINARY NOT FOR CONSTRUCTION



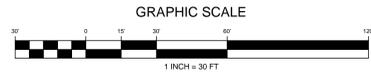
SITE PLAN LEGEND

- PLAN LIMITS
- PUBLIC STREET RIGHT OF WAY
- LOT LINES
- EXISTING WATER
- PROPOSED WATER
- EXISTING FIRE HYDRANT
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- IDENTIFICATION SIGN
- ELECTRIC VEHICLE PARKING SPACE
- ELECTRICAL TRANSFORMER, ELECTRICAL VAULT, JUNCTURE BOX
- GREASE TRAP



MATCH LINE - SEE SHEET S10

MATCH LINE - SEE SHEET S6



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 Developer's Name: \_\_\_\_\_  
 Company: \_\_\_\_\_ Contact Person: \_\_\_\_\_  
 Address: \_\_\_\_\_  
 Phone: \_\_\_\_\_  
 Signature: \_\_\_\_\_

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**SITE PLAN No.**  
820140160

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REVISION	DATE	REVISION	DATE	REVISION	DATE
		ADDRESSED DRC COMMENTS	10/1/14		

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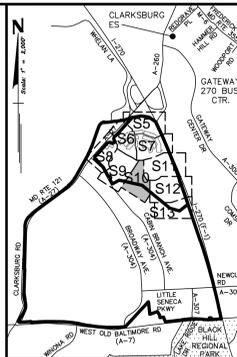
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 WSSC MAP #'s 231NW14 & 231NW13  
 TAX MAP GRID #'s EV23 & EV3

**RODGERS CONSULTING**  
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BASE DATA	BY	DATE
DESIGNED		
DRAWN		
REVIEWED		
RODGERS CONTACT:		
RELEASE FOR		
BY: _____	DATE: _____	

**SITE PLAN**

SCALE: 1" = 30'  
 JOB No. 1134A3  
 DATE: MAY, 2014  
 SHEET No. S9 of 18

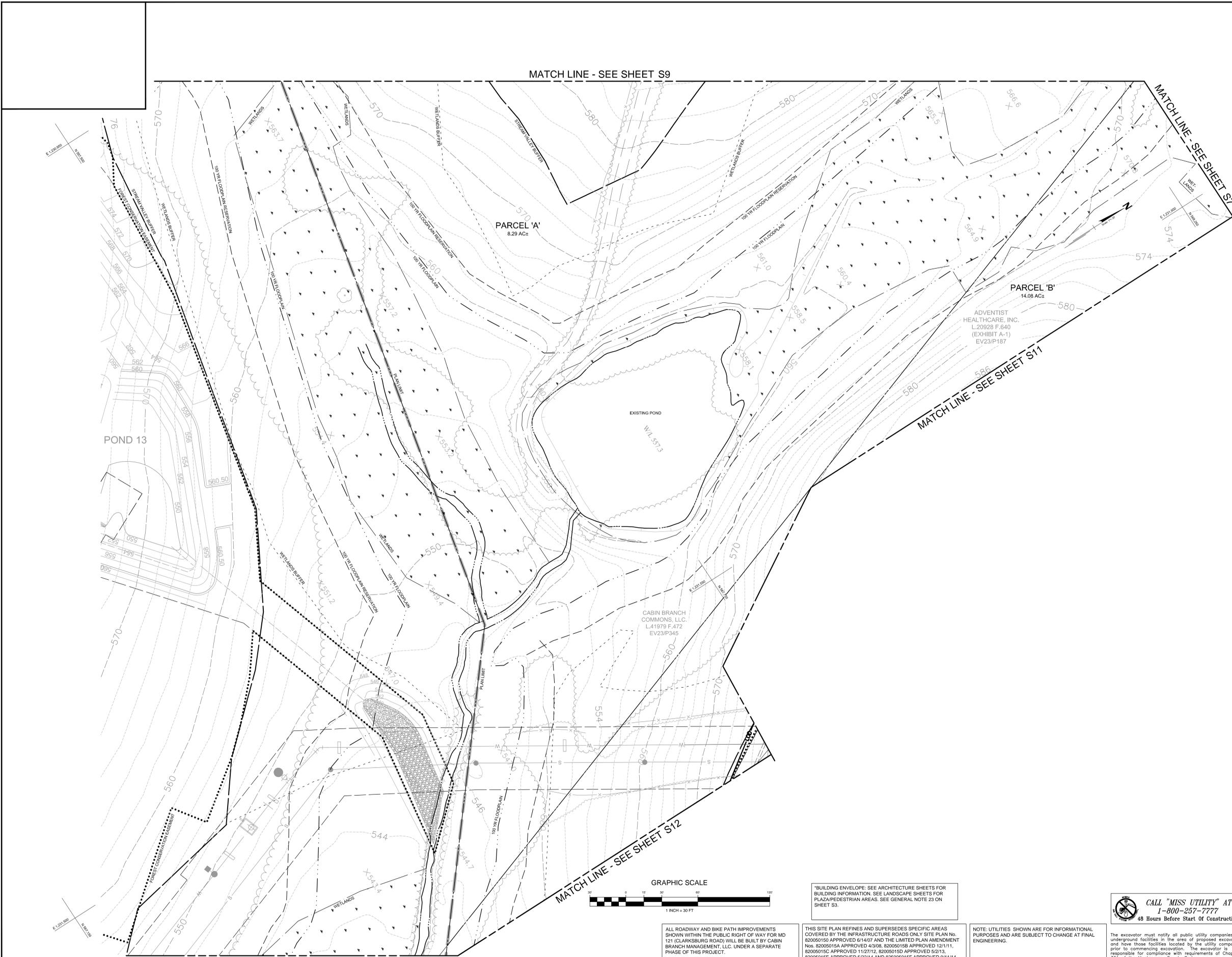


MATCH LINE - SEE SHEET S9

MATCH LINE - SEE SHEET S7

MATCH LINE - SEE SHEET S11

MATCH LINE - SEE SHEET S12



SITE PLAN LEGEND

- Legend items: PLAN LIMITS, PUBLIC STREET RIGHT OF WAY, LOT LINES, EXISTING WATER, PROPOSED WATER, EXISTING FIRE HYDRANT, PROPOSED FIRE HYDRANT, EXISTING SEWER, PROPOSED SEWER, EXISTING STORM DRAIN, PROPOSED STORM DRAIN, PUE, STORM DRAIN EASEMENT, WSSC EASEMENT, BUILDING ENVELOPE\*, ZONE LIMITS, PROPOSED SIDEWALK/CONCRETE SLAB, PROPOSED PAVEMENT, SWMESD, PROPOSED RETAINING WALL, LIMITS OF DISTURBANCE, EXISTING CONTOURS, PROPOSED CONTOURS, POND/STREAM, 100 YEAR FLOODPLAIN, FLOODPLAIN BRL, WETLANDS, WETLANDS BRL, STREAM VALLEY BUFFER, EXISTING TREES/HEDGEROW, FOREST CONSERVATION EASEMENT, IDENTIFICATION SIGN, ELECTRIC VEHICLE PARKING SPACE, ELECTRICAL TRANSFORMER, ELECTRICAL VAULT, JUNCTURE BOX, GREASE TRAP.

DEVELOPER'S CERTIFICATE form with fields for Name, Address, Phone, Signature, and Date.

PROFESSIONAL CERTIFICATION text block.

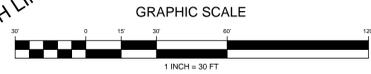


SITE PLAN No. 820140160

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ALL ROADWAY AND BIKE PATH IMPROVEMENTS SHOWN WITHIN THE PUBLIC RIGHT OF WAY FOR MD 121 (CLARKSBURG ROAD) WILL BE BUILT BY CABIN BRANCH MANAGEMENT, LLC. UNDER A SEPARATE PHASE OF THIS PROJECT.

THIS SITE PLAN REFINES AND SUPERSEDES SPECIFIC AREAS COVERED BY THE INFRASTRUCTURE ROADS ONLY SITE PLAN No. 820050150 APPROVED 6/14/07 AND THE LIMITED PLAN AMENDMENT Nos. 82005015A APPROVED 4/3/08, 82005015B APPROVED 12/1/11, 82005015C APPROVED 11/27/12, 82005015D APPROVED 5/2/13, 82005015E APPROVED 5/22/14 AND 825000515F APPROVED 9/11/14.

The excavator must notify all public utility companies with underground facilities in the area of proposed excavation and have those facilities located by the utility companies prior to commencing excavation. The excavator is responsible for compliance with requirements of Chapter 36A of the Montgomery County Code.

REVISION table with columns for REVISION, DATE, and ADDRESSED DRC COMMENTS.

Owner/Developer: Simon Clarksburg Development, LLC c/o Simon Premium Outlets 105 Eisenhower Parkway Roseland, NJ 07068 Phone: (973) 403-3167 Contact: Steve Dworkin e-mail: sdworkin@simon.com

CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1

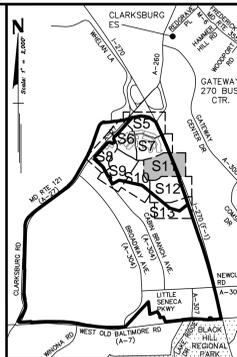


19847 Century Boulevard Suite 200 Germantown, Maryland 20874 Ph: 301.948.4700 (Main) Ph: 301.233.6029 (Frederick) Fx: 301.948.6256 www.rodgers.com

Table with columns: BASE DATA, DESIGNED, DRAWN, REVIEWED, RODGERS CONTACT, RELEASE FOR, BY, DATE.

SITE PLAN

SCALE: 1" = 30' JOB No. 1134A3 DATE: MAY, 2014 SHEET No. S10 of 18



**SITE PLAN LEGEND**

	PUBLIC STREET RIGHT OF WAY
	LOT LINES
	EXISTING WATER
	PROPOSED WATER
	EXISTING FIRE HYDRANT
	PROPOSED FIRE HYDRANT
	EXISTING SEWER
	PROPOSED SEWER
	EXISTING STORM DRAIN
	PROPOSED STORM DRAIN
	PUE
	STORM DRAIN EASEMENT
	WSSC EASEMENT
	BUILDING ENVELOPE*
	ZONE LIMITS
	PROPOSED SIDEWALK/CONCRETE SLAB
	PROPOSED PAVEMENT
	SWMESD
	PROPOSED RETAINING WALL
	LIMITS OF DISTURBANCE
	EXISTING CONTOURS
	PROPOSED CONTOURS
	POND/STREAM
	100 YEAR FLOODPLAIN
	FLOODPLAIN BRL
	WETLANDS
	WETLANDS BRL
	STREAM VALLEY BUFFER
	EXISTING TREES/HEDGE ROW
	FOREST CONSERVATION EASEMENT
	IDENTIFICATION SIGN
	FUTURE ELECTRIC VEHICLE PARKING SPACE
	ELECTRICAL TRANSFORMER, ELECTRICAL VAULT, JUNCTION BOX
	GREASE TRAP



\*BUILDING ENVELOPE: SEE ARCHITECTURE SHEETS FOR BUILDING INFORMATION. SEE LANDSCAPE SHEETS FOR PLAZA/PEDESTRIAN AREAS. SEE GENERAL NOTE 23 ON SHEET S3.

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PROFESSIONAL CERTIFICATION  
I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13970, Expiration Date: 6/1/16.



DEVELOPER'S CERTIFICATE  
The Undersigned agrees to execute all the features of the Site Plan Approval No. 820140160, including Approval Conditions, Development Program, and Control Site Plan.  
Developer's Name: \_\_\_\_\_  
Address: \_\_\_\_\_  
Phone: \_\_\_\_\_  
Signature: \_\_\_\_\_

SITE PLAN No. 820140160

REVISION	DATE	REVISION	DATE	REVISION	DATE

Owner/Developer:  
Simon Clarksburg Development, LLC  
c/o Simon Premium Outlets  
105 Eisenhower Parkway  
Roseland, NJ 07068  
Phone: (973) 403-3167  
Contact: Steve Dworkin  
e-mail: sdworkin@simon.com

**CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1**

WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3

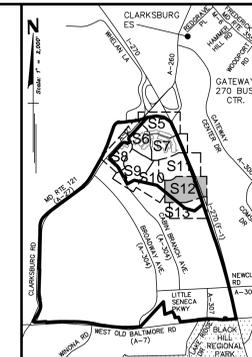
**RODGERS CONSULTING**  
19847 Century Boulevard  
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Ph: 301.948.4700 (Main)  
Ph: 301.233.6029 (Frederick)  
F: 301.948.6256  
www.rodgers.com

BASE DATA	BY	DATE
DESIGNED		
DRAWN		
REVIEWED		
RODGERS CONTACT:		
RELEASE FOR		
BY		DATE

**SITE PLAN**

SCALE: 1" = 30'  
JOB No. 1134A3  
DATE: MAY, 2014  
SHEET No. S11 of 18

MATCH LINE - SEE SHEET S11



PRELIMINARY NOT FOR CONSTRUCTION

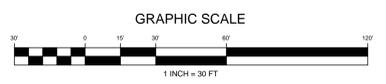
MATCH LINE - SEE SHEET S10



SITE PLAN LEGEND

- PLAN LIMITS
- PUBLIC STREET RIGHT OF WAY
- LOT LINES
- EXISTING WATER
- PROPOSED WATER
- EXISTING FIRE HYDRANT
- PROPOSED FIRE HYDRANT
- EXISTING SEWER
- PROPOSED SEWER
- EXISTING STORM DRAIN
- PROPOSED STORM DRAIN
- PUE
- STORM DRAIN EASEMENT
- WSSC EASEMENT
- BUILDING ENVELOPE
- ZONE LIMITS
- PROPOSED SIDEWALK/CONCRETE SLAB
- PROPOSED PAVEMENT
- SWMESD
- PROPOSED RETAINING WALL
- LIMITS OF DISTURBANCE
- EXISTING CONTOURS
- PROPOSED CONTOURS
- POND/STREAM
- 100 YEAR FLOODPLAIN
- FLOODPLAIN BRL
- WETLANDS
- WETLANDS BRL
- STREAM VALLEY BUFFER
- EXISTING TREES/HEDGE ROW
- FOREST CONSERVATION EASEMENT
- IDENTIFICATION SIGN
- ELECTRIC VEHICLE PARKING SPACE
- ELECTRICAL TRANSFORMER, ELECTRICAL VAULT, JUNCTION BOX
- GREASE TRAP

INTERSTATE 270



**DEVELOPER'S CERTIFICATE**  
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 Developer's Name: \_\_\_\_\_ Company: \_\_\_\_\_ Contact Person: \_\_\_\_\_  
 Address: \_\_\_\_\_  
 Phone: \_\_\_\_\_  
 Signature: \_\_\_\_\_

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SITE PLAN No. 820140160

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Phone: (973) 403-3167  
Contact: Steve Dworkin  
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AT CABIN BRANCH  
NORTH DISTRICT, PHASE 1**  
WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3



BASE DATA	BY	DATE
DESIGNED		
DRAWN		
REVIEWED		

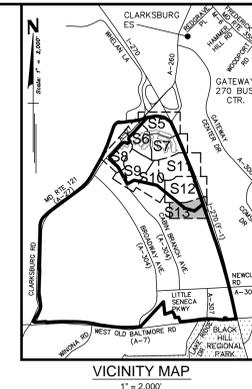
RELEASE FOR \_\_\_\_\_  
 BY: \_\_\_\_\_ DATE: \_\_\_\_\_

SITE PLAN

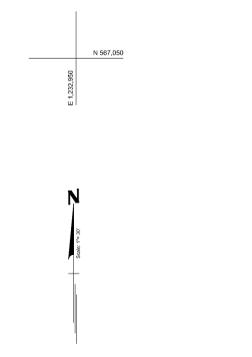
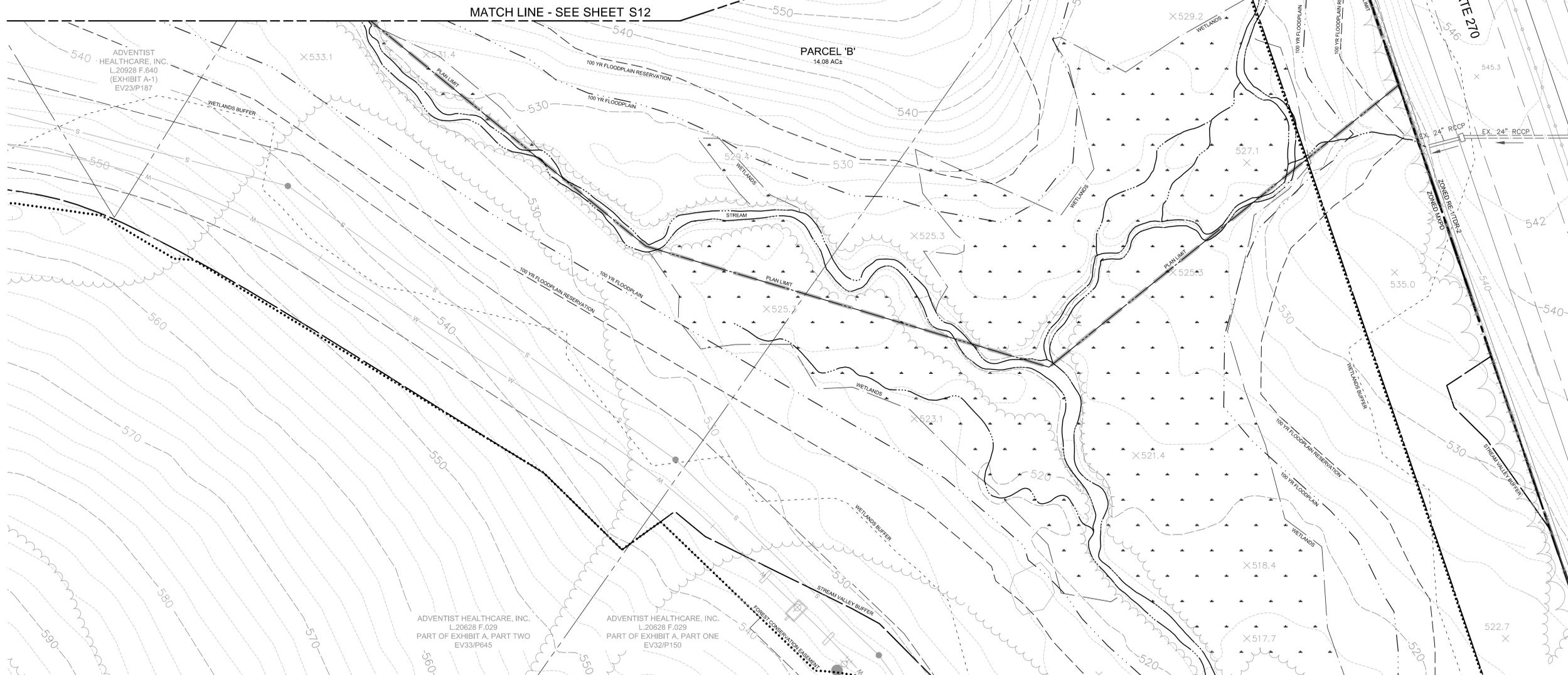
SCALE:	1" = 30'
JOB No.	1134A3
DATE:	MAY, 2014
SHEET No.	S12 of 18

**SITE PLAN LEGEND**

	PLAN LIMITS		ZONE LIMITS		WETLANDS
	PUBLIC STREET RIGHT OF WAY		PROPOSED SIDEWALK/CONCRETE SLAB		WETLANDS BRL
	LOT LINES		PROPOSED PAVEMENT		STREAM VALLEY BUFFER
	EXISTING WATER		SWM/ESD		EXISTING TREESHEDGEROW
	PROPOSED WATER		PROPOSED RETAINING WALL		FOREST CONSERVATION EASEMENT
	EXISTING FIRE HYDRANT		LIMITS OF DISTURBANCE		IDENTIFICATION SIGN
	PROPOSED FIRE HYDRANT		EXISTING CONTOURS		ELECTRIC VEHICLE PARKING SPACE
	EXISTING SEWER		PROPOSED CONTOURS		ELECTRICAL TRANSFORMER, ELECTRICAL VAULT, JUNCTURE BOX
	PROPOSED SEWER		POND/STREAM		GREASE TRAP
	EXISTING STORM DRAIN		100 YEAR FLOODPLAIN		
	PROPOSED STORM DRAIN		FLOODPLAIN BRL		
	PUE				
	WSSC EASEMENT				
	BUILDING ENVELOPE*				



PRELIMINARY NOT FOR CONSTRUCTION



ADVENTIST HEALTHCARE, INC.  
L.20628 F.029  
PART OF EXHIBIT A, PART TWO  
EV33/P645

ADVENTIST HEALTHCARE, INC.  
L.20628 F.029  
PART OF EXHIBIT A, PART ONE  
EV32/P150

ADVENTIST HEALTHCARE, INC.  
L.20628 F.640  
(EXHIBIT A-1)  
EV23/P187

GRAPHIC SCALE

1 INCH = 30 FT

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Developer's Name: \_\_\_\_\_  
Address: \_\_\_\_\_  
Phone: \_\_\_\_\_  
Signature: \_\_\_\_\_

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I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13970, Expiration Date 6/1/16.

**STATE OF MARYLAND**  
**PROFESSIONAL ENGINEER**  
FOR REVIEW ONLY

**SITE PLAN No.**  
**820140160**

REVISION	DATE	REVISION	DATE	REVISION	DATE

Owner/Developer:  
**Simon Clarksburg Development, LLC**  
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**CLARKSBURG PREMIUM OUTLETS  
AT CABIN BRANCH  
NORTH DISTRICT, PHASE 1**

WSSC MAP #'s 231NW14 & 231NW13  
TAX MAP GRID #'s EV23 & EV3

**RODGERS CONSULTING**  
*Knowledge • Creativity • Enduring Values*

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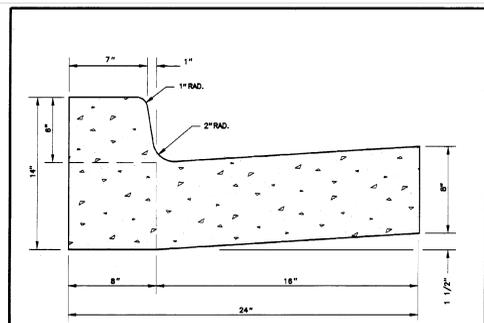
BY	DATE

RELEASE FOR: \_\_\_\_\_  
BY: \_\_\_\_\_ DATE: \_\_\_\_\_

**SITE PLAN**

SCALE: 1" = 30'  
JOB No. 1134A3  
DATE: MAY, 2014  
SHEET No. S13 of 18

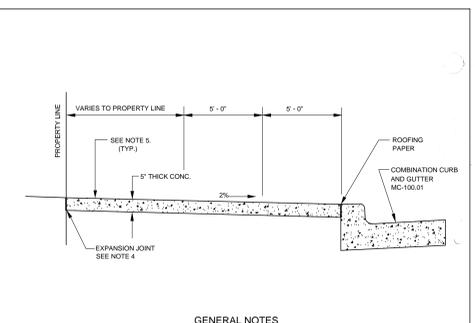
07-SITE-820140160-013: CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1 SITE PLAN



**GENERAL NOTES**

- REFER TO MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION SPECIFICATIONS FOR MATERIALS, METHODS OF CONSTRUCTION AND EXPANSION JOINT LOCATIONS.
- THIS STANDARD SHALL BE USED ON PRIMARY RESIDENTIAL, ARTERIAL AND BUSINESS DISTRICT ROADS AS WELL AS CURB RETURNS AND INLET THROATS.
- WHENEVER STANDARD MC-101.01 CURB IS USED IN CONJUNCTION WITH THIS STANDARD, A TEN FOOT TRANSITION SHALL BE PROVIDED FROM STANDARD MC-100.01 TO STANDARD MC-101.01 FOR CURB RETURNS AND CURB SECTIONS WHICH INCLUDE INLETS.
- THE STANDARD DISTANCE BETWEEN JOINTS SHALL BE TEN FEET (MAXIMUM AND MINIMUM DISTANCES SHALL BE THIRTEEN FEET AND FIVE FEET RESPECTIVELY).
- EXPANSION JOINT MATERIAL SHALL BE 1/2 INCH PREFORMED CORK, TRIMMED AND SEALED WITH NON-STAINING TWO-COMPONENT POLYSULFIDE OR POLYURETHANE ELASTOMERIC TYPE SEALANT COMPLYING WITH FS TT-S-00227.

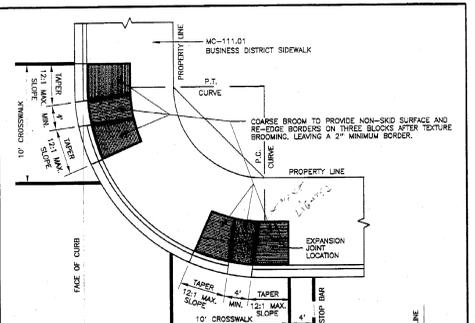
APPROVED	JAN 5 196	REVISED		MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION
	DATE			COMBINATION CONCRETE CURB AND GUTTER TYPE A
				STANDARD NO. MC-100.01



**GENERAL NOTES**

- REFER TO MARYLAND STATE HIGHWAY ADMINISTRATION SPECIFICATIONS FOR MATERIALS AND METHODS OF CONSTRUCTION.
- REFER TO MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION DESIGN STANDARDS No. MC-302.01 FOR DETAILS AT DRIVEWAY.
- EXPANSION JOINT MATERIAL SHALL BE PLACED AROUND POLES, HYDRANTS, ETC. AND ALONG THE PROPERTY LINE WHEN THE SIDEWALK ABUTS ANY RIGID PAVEMENT, SIDEWALK OR STRUCTURE.
- EXPANSION JOINT MATERIAL SHALL HAVE A MAXIMUM LONGITUDINAL SPACING OF 100 FEET. THE MATERIAL SHALL BE 1/2 INCH PREFORMED CORK, TRIMMED AND SEALED WITH NON-STAINING, TWO-COMPONENT POLYSULFIDE OR POLYURETHANE ELASTOMERIC TYPE SEALANT COMPLYING WITH FS TT-S-00227.
- SCORE THE CONCRETE TO A DEPTH OF 1/3 THE SLAB THICKNESS TO PROVIDE WEAKENED PLANE TRANSVERSE JOINTS AT 5'-0" INTERVALS PARALLEL WITH AND PERPENDICULAR TO THE CURBING.

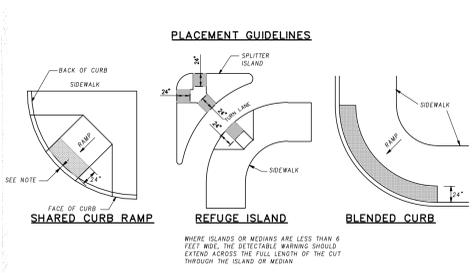
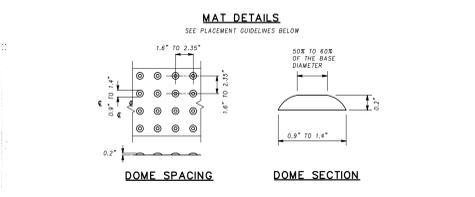
APPROVED	JAN 5 196	REVISED		MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION
	DATE			BUSINESS DISTRICT SIDEWALK
				STANDARD NO. MC-111.01



**GENERAL NOTES**

- THIS STANDARD TO BE USED WITH CURB RADIUS LESS THAN 30'.
- REFER TO MARYLAND STATE HIGHWAY ADMINISTRATION SPECIFICATIONS FOR MATERIALS AND METHODS OF CONSTRUCTION.
- SIDEWALK RAMPS SHOULD BE LOCATED AS INDICATED, HOWEVER, EXISTING SURFACE UTILITIES MAY AFFECT PLACEMENT.
- EXPANSION JOINT MATERIAL SHALL BE 1/2 INCH PREFORMED CORK, TRIMMED AND SEALED WITH NON-STAINING, TWO-COMPONENT POLYSULFIDE OR POLYURETHANE ELASTOMERIC TYPE SEALANT, COMPLYING WITH FS TT-S-00227.

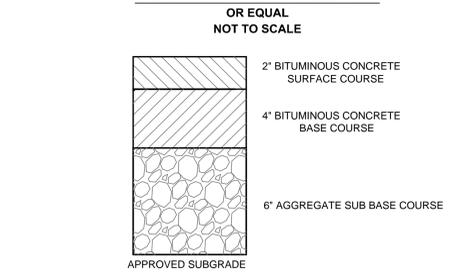
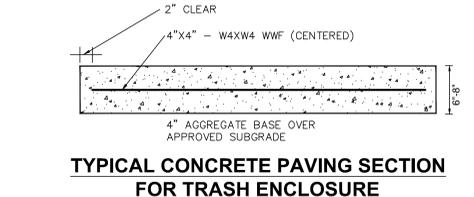
APPROVED	JAN 5 196	REVISED		MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION
	DATE			BUSINESS DISTRICT SIDEWALK DUAL RAMP
				STANDARD NO. MC-113.02



**NOTES**

- THE DETECTABLE WARNING SURFACE SHALL BE LOCATED SO THAT THE EDGE NEAREST THE CURB LINE IS 4 TO 8 INCHES FROM THE FACE OF CURB.
- FOR SLOPED APPLICATIONS DETECTABLE WARNING SHALL BE PLACED SUCH THAT THE DOWNS CLOSEST TO THE BACK OF CURB ARE NO LESS THAN 2.5" AND NO MORE THAN 3.0" FROM THE BACK OF CURB. TRUNCATED DOME SURFACES SHALL BE FABRICATED TO PREVENT FAIL DOWN ONLY.
- DETECTABLE WARNING SURFACE SHALL BE PAD FOR IN ACCORDANCE WITH SECTION 611 OF THE SPECIFICATIONS.
- DETECTABLE WARNING SURFACES ARE REQUIRED AT STREET CROSSING & SIGNALIZED INTERSECTIONS.

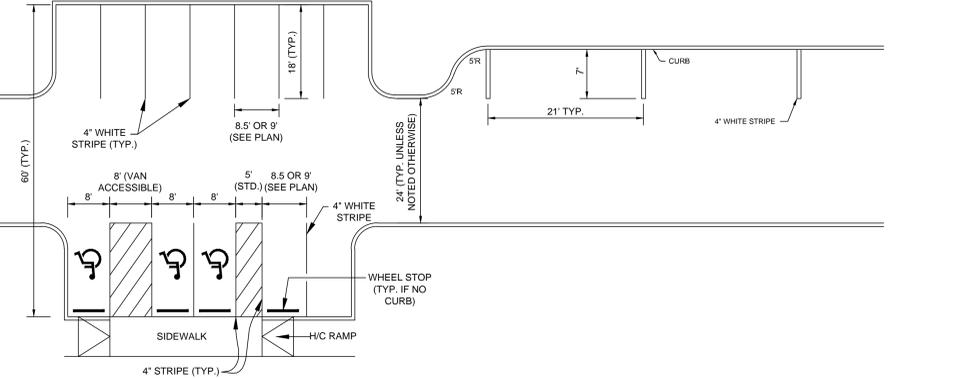
APPROVED	JAN 5 196	REVISED		MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION
	DATE			BUSINESS DISTRICT SIDEWALK DUAL RAMP
				STANDARD NO. MC-113.02



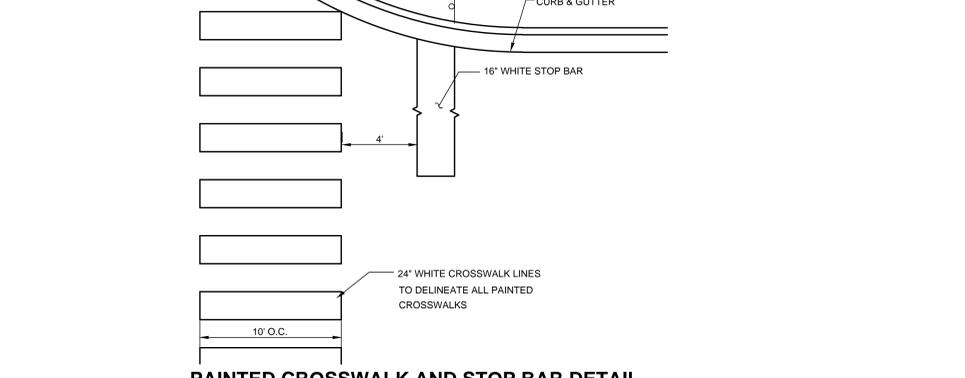
**NOTES**

- FINAL ADJUSTMENTS TO THE PAVEMENT DESIGN SECTIONS MAY BE REQUIRED BASED ON RESULTS OF CBR TEST TAKEN OF THE PAVEMENT SUBGRADES SOILS AT THE TIME OF CONSTRUCTION. THE ASPHALTIC CONCRETE AND BASE COURSE SHOULD BE IN ACCORDANCE WITH THE APPLICABLE MARYLAND STATE HIGHWAY ADMINISTRATION SPECIFICATIONS.

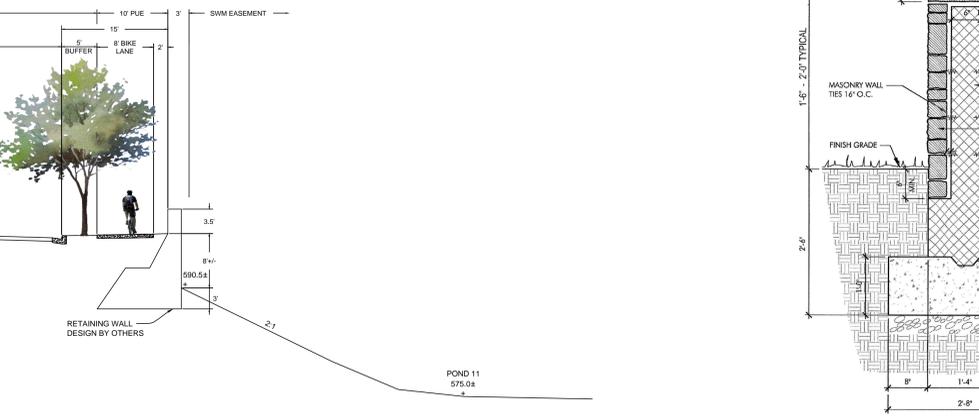
APPROVED	JAN 5 196	REVISED		MONTGOMERY COUNTY DEPARTMENT OF TRANSPORTATION
	DATE			BUSINESS DISTRICT SIDEWALK DUAL RAMP
				STANDARD NO. MC-113.02



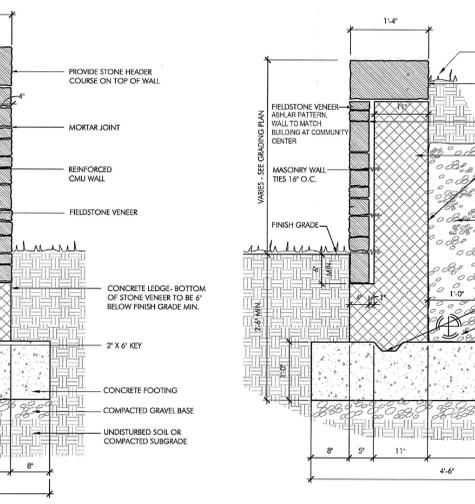
**TYPICAL PARKING SPACE STRIPING DETAIL**  
NOT TO SCALE



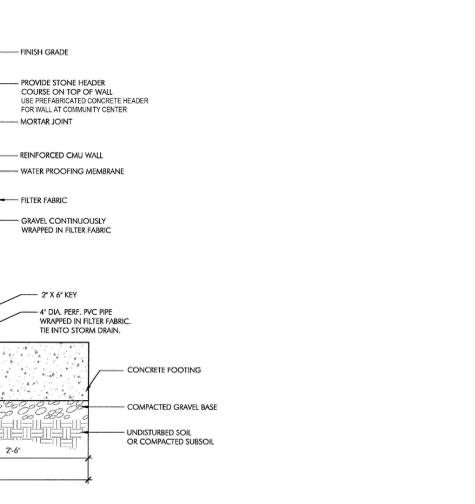
**PAINTED CROSSWALK AND STOP BAR DETAIL**  
NOT TO SCALE



**POND 11 WALL SECTION**  
NOT TO SCALE



**FIELDSTONE SEAT WALL (OR EQUAL)**  
Not to Scale



**MASONRY RETAINING WALL (OR EQUAL)**  
Not to Scale

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WSSC MAP #'s 231NW14 & 231NW13  
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DESIGNED		DATE	
DRAWN			
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RELEASE FOR			

BASE DATA		DATE	
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RELEASE FOR			

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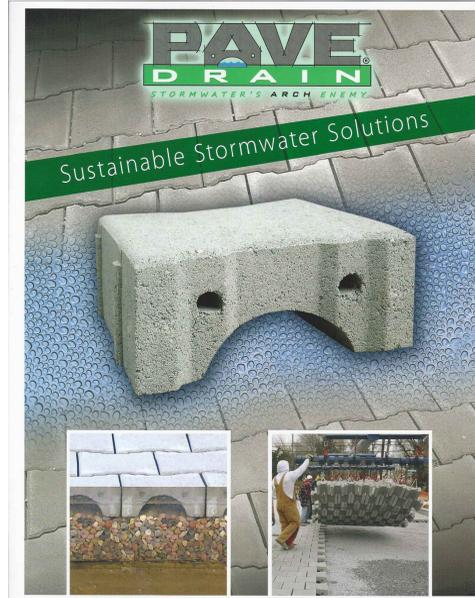
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**CALL "MISS UTILITY" AT 1-800-257-7777 48 Hours Before Start of Construction**

**SITE PLAN No. 820140160**

**SCALE: N/A**  
JOB No. 1134A3  
DATE: MAY, 2014  
SHEET No. S14 of 18

PRELIMINARY NOT FOR CONSTRUCTION



**Structural and Environmental Benefits**  
 The PaveDrain® System combines modern day functionality and a structural concept used for curbs to create the revolutionary permeable paving solution. PaveDrain incorporates a patented arch design in the middle of an articulating concrete block to create an internal storage chamber that can be filled with stormwater runoff while simultaneously providing strength for heavy vehicular loads. The PaveDrain System is designed to be a critical component of Low Impact Development (LID) allowing for the infiltration of stormwater runoff...on-site.

The PaveDrain System is a Permeable Articulating Concrete Block/Mat (P-ACB/M) that provides environmental benefits as a designated solution for compliance with local regulatory stormwater requirements. The durability and performance in terms of life-cycle costs, make it the prudent alternative to all other paving solutions. PaveDrain and its aggregate bedding stone is engineered to create a natural, vertical infiltration path, that recharges local groundwater all while using nature's inherent ability to filter first flush pollutants. This allows for a reduction or elimination in stormwater infrastructure while minimizing the environmental impact. In addition to effectively managing stormwater runoff the PaveDrain System is also being developed using recycled materials which will make it one of the leading green technologies for stormwater management. PaveDrain works.

**ASTM Standards & ADA Compliance**  
 The PaveDrain System meets the requirements of ASTM D6684-04 and is recognized by the USEPA as a structural Best Management Practice (BMP) for stormwater infiltration. The Americans with Disabilities Act (ADA) Design Guidelines require that surface openings shall not exceed 1/2" and shall be firm, stable, and slip resistant. The PaveDrain System easily exceeds all these requirements by incorporating a 3/8" gap between individual PaveDrain blocks.

**Applications**

- ☐ Parking Lots
- ☐ Alley Ways
- ☐ Low Speed Roadways
- ☐ Intersections
- ☐ Emergency Access Lanes
- ☐ Residential Driveways

**Why the PaveDrain System?**  
 As part of the Clean Water Act, the EPA developed the National Pollution Discharge Elimination System (NPDES) to improve water quality by regulating point sources that discharge pollutants into waters of the U.S. The vast majority of storm water drainage systems are considered to be point sources.

**Infiltration Report**  
 The PaveDrain System® was tested and documented by American Excelsior Company's ErosionLab™ to help better understand its infiltration capabilities. It was installed over two plots that contained an identical six inch aggregate bedding layer, but different underlying soils consisting of clay and sand. The plots were exposed to increasing simulated one hour duration rainfall events (i.e. 4", 6" and 8") until runoff was measured. View the entire ErosionLab report by visiting our website at [www.pavedrain.com](http://www.pavedrain.com) and go to How it Works.

**PaveDrain System Properties**  
 Thickness: 5.65" (± .18")  
 Unit Dimensions: 11.25" x 11.25"  
 Unit Weight: 42-45 lbs  
 Units/SF: 1 SF Nominal  
 Percentage Open Space: 3% Surface, 20% Storage Area

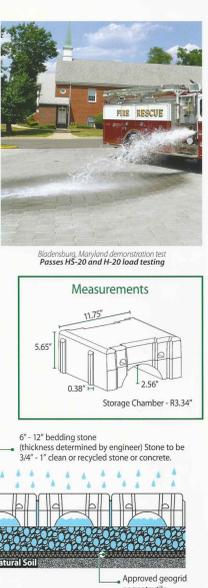
**Installation Diagram**

**Measurements**  
 11.25"  
 5.65"  
 0.38"  
 2.56"  
 Storage Chamber - R3.34"

**PaveDrain System Blocks End View Cross Section**  
 6" - 12" bedding stone (thickness determined by engineer) Stone to be 3/4" - 1" clean or recycled stone or concrete.

Storage above the base!

→ PaveDrain → Approved geogrid or geotextile → Natural Soil



**LEED Credits: Sustainable Sites**

- CREDIT 6.1 Stormwater Design: Rate & Quality Control: 1 Point  
 Limit distribution of surface water flows by managing stormwater runoff.
- CREDIT 6.2 Stormwater Treatment: 1 Point  
 Implement a stormwater management plan that reduces impervious cover, promotes on-site filtration and eliminates contaminants.
- CREDIT 5.1 Site Development: 1 Point  
 Protect or restore habitat.
- CREDIT 7.1 Heat Island Effect, Non-Roof: 1 Point  
 Reduce heat islands.

**Materials & Resources:**  
 • CREDIT 5.2 Regional Materials: 1 Point

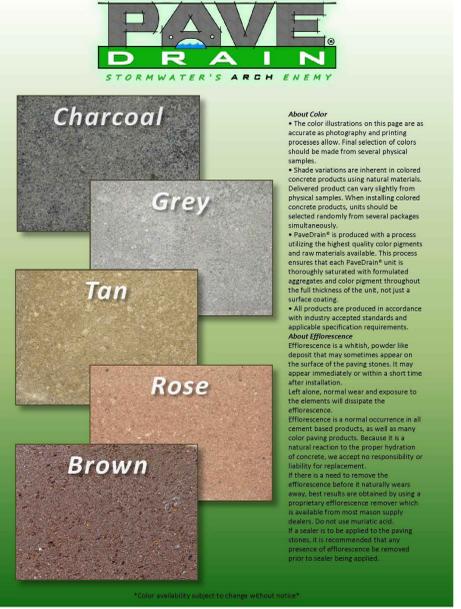
**Maintenance and Repair**  
 The PaveDrain System is easy to maintain using industrial vacuuming equipment, if needed. If a significant amount of sediment accumulates in the aggregate bedding stone, PaveDrain is the only system that can be mechanically lifted out allowing for the subgrade to be cleaned and then the same mats re-installed. Repair of individual PaveDrain blocks can be accomplished without removing the [mat] cables. For more detailed information on these topics go to [www.pavedrain.com](http://www.pavedrain.com) and select Installation Guidelines, and then click on repair.

**Additional Benefits**

- Initial installations show a drastic reduction in the use of deicing salts over traditional asphalt and concrete surfaces.
- Installed with conventional construction equipment
- Available in several color systems
- Fast, economical installation of intersection construction or retrofits of existing pavements
- No seams to catch on snow plows
- Available in multiple mat sizes for customized applications
- Regional manufacturing supporting local economies
- Adaptable to small areas where retention ponds are not practical

Represented Locally By:

**PaveDrain, LLC**  
 PVB 202  
 7265 S. 76th Street, Franklin, WI 53132-9041  
 (414) 423-6531 Office  
[www.pavedrain.com](http://www.pavedrain.com)  
 ©2011 PaveDrain, LLC All Rights Reserved  
 PaveDrain System is a registered trademark of PaveDrain, LLC.

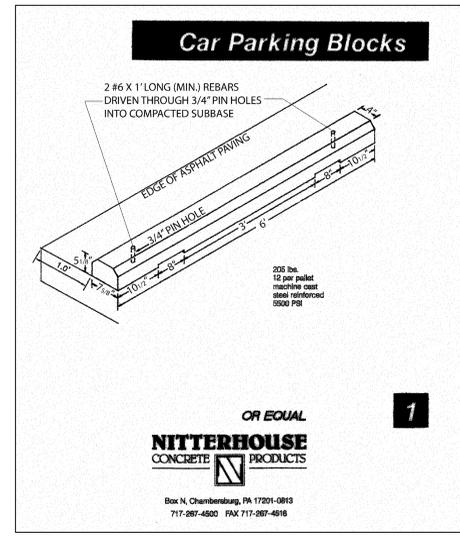


**PAVER 'A' BY:**  
 Pave Drain  
 Tel: 414-423-6531  
 7245 S. 76th Street  
 Franklin, WI 53132-9041

**Color:** Tan  
**Notes:**  
 - Pavers used for fire lane. Must pass HS-20 and H-20 loading tests.  
 - To be installed per manufactures specifications.

**Or Equal**

**PAVER 'A' DETAIL**  
 OR EQUAL  
 (FOR EMERGENCY VEHICLE ACCESS. SEE SHEET 58)



OR EQUAL  
**NITTERHOUSE**  
 CONCRETE PRODUCTS  
 Box N, Chambersburg, PA 17029-0813  
 717-287-4500 FAX: 717-287-4516

TYPICAL PARKING LOT CURB BLOCK DETAIL  
 OR EQUAL

ALL ROADWAY AND BIKE PATH IMPROVEMENTS SHOWN WITHIN THE PUBLIC RIGHT OF WAY FOR MD 121 (CLARKSBURG ROAD) WILL BE BUILT BY CABIN BRANCH MANAGEMENT, LLC, UNDER A SEPARATE PHASE OF THIS PROJECT.

THIS SITE PLAN REFINES AND SUPERSEDES SPECIFIC AREAS COVERED BY THE INFRASTRUCTURE ROADS ONLY SITE PLAN No. 820050150 APPROVED 6/14/07 AND THE LIMITED PLAN AMENDMENT Nos. 82005015A APPROVED 4/3/08, 82005015B APPROVED 12/1/11, 82005015C APPROVED 11/27/12, 82005015D APPROVED 5/2/13, 82005015E APPROVED 5/22/14 AND 820050015F APPROVED 9/11/14.

NOTE: UTILITIES SHOWN ARE FOR INFORMATIONAL PURPOSES AND ARE SUBJECT TO CHANGE AT FINAL ENGINEERING.

CALL "MISS UTILITY" AT  
 1-800-257-7777  
 48 Hours Before Start of Construction

The excavator must notify all public utility companies with underground facilities in the area of proposed excavation and have those facilities located by the utility companies prior to commencing excavation. The excavator is responsible for compliance with requirements of Chapter 36A of the Montgomery County Code.

**PROFESSIONAL CERTIFICATION**  
 I hereby certify that these documents were prepared or approved by me, and that I am a duly licensed professional engineer under the laws of the State of Maryland, License No. 13970, Expiration Date: 6/1/16.\*

**SITE PLAN No.**  
 820140160

**DEVELOPER'S CERTIFICATE**  
 The Underigned agrees to execute all the features of the Site Plan Approval No. 820140160, including Approval Conditions, Development Program, and Control Site Plan.  
 Developer's Name: \_\_\_\_\_  
 Address: \_\_\_\_\_  
 Phone: \_\_\_\_\_  
 Signature: \_\_\_\_\_



REVISION	DATE	REVISION	DATE	REVISION	DATE
		ADDRESSED DRC COMMENTS	10/1/14		

**Owner/Developer:**  
 Simon/Clarksburg Development, LLC  
 c/o Simon Premium Outlets  
 105 Eisenhower Parkway  
 Roseland, NJ 07068  
 Phone: (973) 403-3167  
 Contact: Steve Dworkin  
 e-mail: [sdworkin@simon.com](mailto:sdworkin@simon.com)

**CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH NORTH DISTRICT, PHASE 1**

WSSC MAP #'s 231NW14 & 231NW13  
 TAX MAP GRID #'s EV23 & EV3

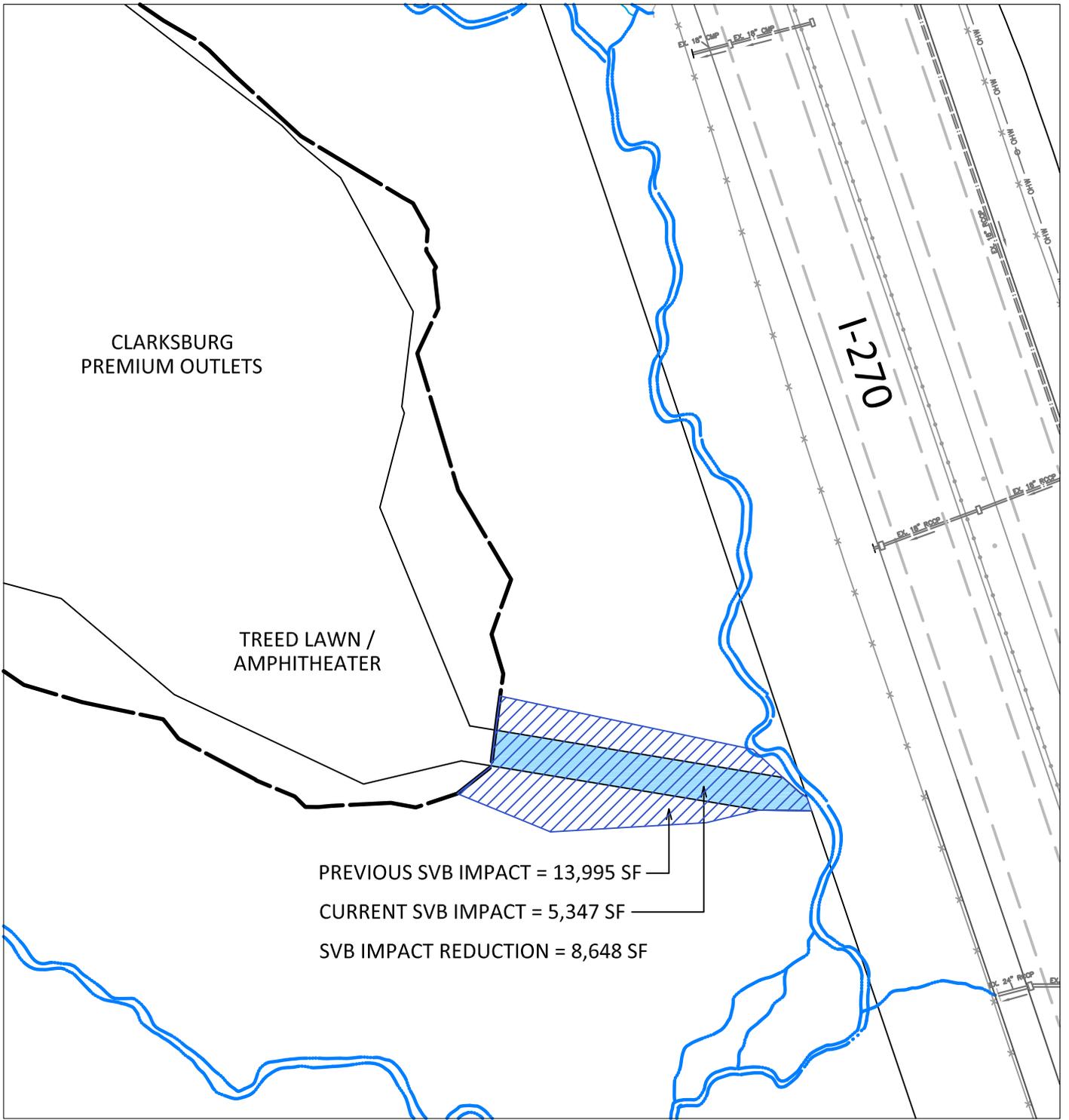
**RODGERS CONSULTING**  
 Knowledge • Creativity • Enduring Values

19847 Century Boulevard  
 Suite 200  
 Germantown, Maryland 20874  
 Ph: 301.948.4700 (Main)  
 Ph: 301.233.6609 (Frederick)  
 Fx: 301.948.6256  
[www.rodgers.com](http://www.rodgers.com)

BY	DATE
BASE DATA	
DESIGNED	
DRAWN	
REVIEWED	
RODGERS CONTACT:	
RELEASE FOR	
BY: _____ DATE: _____	

**SITE NOTES AND DETAILS**

SCALE: N/A  
 JOB No. 1134A3  
 DATE: MAY, 2014  
 SHEET No. S15 of 18



**CLARKSBURG PREMIUM OUTLETS AT CABIN BRANCH  
DRAINAGE AREA 12  
STREAM VALLEY BUFFER IMPACT REDUCTION EXHIBIT**

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# CABIN BRANCH MANAGEMENT, LLC

c/o Winchester Homes Inc.  
6905 Rockledge Drive, Suite 800  
Bethesda, Maryland 20817  
Tel (301) 803-4800  
Toll Free: (800) 527-8558  
Fax (301) 803-4929

November 12, 2014

The Honorable Casey Anderson  
Chair  
The Montgomery County Planning Board  
8787 Georgia Avenue  
Silver Spring, MD 20910

Re: Clarksburg Premium Outlets at Cabin Branch (Site Plan No. 820140160)

Dear Chairman Anderson:

We are writing to you in connection with this pending Site Plan application and, more specifically, Design Guidelines applicable to development at Cabin Branch.

As you are aware, Cabin Branch is a large, mixed-use planned community. The project was approved initially in 2003 under Local Map Amendment No. G-806 and a related Development Plan. The initial approval called for a diverse mix of housing, highway retail, neighborhood retail and employment, including a hospital and medical services as well as office, R&D and other non-residential uses. The first Site Plan for the project (820060290, MCPB No. 08-96) approved an initial set of Design Guidelines to guide development of the site while it, and the Design Guidelines themselves, recognized the need for those Guidelines to evolve as different elements of the overall project proceeded.

As you are aware, last year, the Montgomery County Council, with the full support of Park and Planning Staff and the Montgomery County Planning Board, approved a Development Plan Amendment (DPA No. 13-02) calling for the substitution of 450,000 square feet of destination retail in place of the hospital previously contemplated on the site. The current Site Plan application referenced above reflects the implementation of that approved Development Plan. The initial Design Guidelines included general provisions for the employment areas, including the contemplated hospital, while Condition No. 5 of the initial Site Plan noted that the Design Guidelines for the retail, healthcare and employment areas would be re-evaluated with each plan related to those uses. That is the purpose of this request.

Working with the Cabin Branch Town Architect, Cabin Branch Management and the various property owners at Cabin Branch have developed Design Guidelines appropriate for the planned destination retail. These incorporate the general principles initially adopted for Cabin Branch, and add more specific guidelines for the planned destination retail. These Guidelines will ensure that development of the destination retail, and subsequent employment phases, will

be compatible with the overall Cabin Branch project and consistent with the design principles for the overall site.

Additionally, acting through its designated Development Review Committee, Cabin Branch Management and the Cabin Branch owners have reviewed the subject Site Plan for the destination retail, and have concluded that the application is consistent with the Design Guidelines. Therefore, we ask that you approve the Design Guidelines for the destination retail and the Site Plan itself.

Very truly yours,



**CABIN BRANCH MANAGEMENT, LLC.**

Sylke Knuppel, PE

Director of Land Development

Enclosure

Cc: Robert Harris, Lerch Early and Brewer

Cc: Smita Anand, KTG

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## Destination Retail

### **VI. DESTINATION RETAIL, OFFICE, & EMPLOYMENT GUIDELINES**

Due to the complexity of design the details of the Destination Retail area will be evaluated in detail with the detailed Site Plan. In developing the Destination Retail area the following must be employed in the design. It is recognized that the facilities around or in support of the retail center will grow and develop over time. Later phases will likely provide for increased density. Planning for and flexibility in the design of the initial stages should be considered so as not to preclude the future expansions.

A. SITE DESIGN - The following general requirements and design guidelines are to be employed in the site design.

1) General Requirements

Public Streets in the commercial districts shall have sidewalks on both sides. a) Public streets shall be defined and reinforced by elements which may include one or a combination of the following:

- i. building facades
- ii. masonry walls
- iii. decorative fences
- iv. hedges
- v. related landscape improvements to establish street edges.

Buildings not subject to Destination Retail uses, along public streets should create a street edge or face a public plaza or open space at significant nodal points

- b) Graded slopes should not exceed 3:1. Retaining walls are recommended where slopes exceed 3:1. Slopes must be properly stabilized to prevent erosion. Lawn areas should have slopes of at least 2% to prevent standing water.
  - c) Setbacks from property lines for structures will be established during Site Plan review. Foundation planting between the building and the curb of a parking bay is encouraged other than in circumstances such as retail buildings with sidewalks along their front façade.
  - d) Street entry drives shall be flanked on both sides by a minimum of one of the street edge elements as outlined in 1(a) above. Discretion by the designer and reviewer should be employed to recognize site constraints and future phases.
  - e) A comprehensive pedestrian circulation system of sidewalks and/ or walkways should be provided within a parcel, with connections to adjacent parcels and adjacent pedestrian networks. The circulation system should be developed considering the source, needs, and destination of pedestrians and other special populations.
  - f) Drainage systems must conform to County requirements. Any storm water retention pond perimeters must be landscaped.
- 2) Fences, Barriers and Walls
- a) Permanent perimeter or property line fencing (other than necessary retaining walls, screen walls or decorative streetscape walls) is discouraged in the office, employment and Destination Retail areas. Fences other than decorative are not allowed in front of buildings except where requirements for security, public and/or pedestrian safety is essential. Security gates should not be allowed unless warranted by special circumstances. Security gates or barriers can be used if their location is such that they will not unnecessarily impede the community's local pedestrian or vehicular movements and are designed to be aesthetically compatible with the project's building and site.
  - b) Permanent chain link fencing is prohibited. In locations that are hidden from public view, a limited amount of black, vinyl-coated chain link fencing with posts of wood or

metal painted black to match the fence or decorative aluminum fence can be used. Galvanized (silver) chain link fencing is prohibited except for temporary construction use.

- c) Retaining walls shall be designed to be architecturally compatible with the community.
- 3) Parking Areas
- a) Parking areas must be buffered either by existing vegetation, new landscape material, decorative walls, landscaped berms, evergreen hedges, or a combination of these techniques.
  - b) Landscaping and tree islands must be used to reduce the visual impact of large, paved surfaces and parked cars.
  - c) Where curbs are proposed in a parking area they shall be concrete. Railroad tie or asphalt curbs are not acceptable. Wheel stops are not encouraged and shall only be used in a local condition , such as where necessary to allow drainage.
  - d) Where parking structures subject to Destination Retail uses or other future uses in same phase, are visible from streets, they should be designed with architectural detail consistent with the architectural character of the community. Unless limited by dimensions, sloped ramps should be located internal to the structure so that they are not visible along front facades. Attention should be given to the design of parking garage interior lighting to avoid light spillage. Vehicular access to and from parking should be designed with the consideration of the public safety and to minimize pedestrian conflicts
  - e) Light fixtures on top of parking structures should be limited in height and shielded as appropriate.
  - f) Parking areas may have control gates for parking control purposes.
- 4) Site Lighting
- a) High pressure sodium vapor (yellow-orange) lighting is prohibited. Lighting must be a “white” light such as metal halide.
  - b) Pole mounted light fixtures must direct the light downward, and the lens must not project below the housing of the fixture. Fixture height should not exceed 30 feet above grade for any office or commercial use located adjacent to or across the street from a proposed residential use.
  - c) Decorative fixtures may be used and should be compatible with the fixtures proposed by other developments within Cabin Branch along pedestrian walkways.
  - d) All private exterior lighting must be full cut-off and all up lighting must have hoods/deflectors.

B. ARCHITECTURAL DESIGN - While not prescribing to a specific architectural style, the design of all office, employment, and Destination Retail building elevations in terms of size, massing, details and the use of materials and colors, should be compatible and complimentary with the architectural character of the community. Buildings should be sympathetic to their natural environment.

1) General Requirements

- a) Building elevations are important and should be designed “in the round” with suitable details and materials on all sides to the extent practical given service/ loading requirements. Buildings should have an entrance facing a street or commons.. Large or long expanses of walls must be broken up by appropriate detailing such as reveals, piers, pilasters, projections or decorative patterns.

- b) Canopies extending from the retail uses must contain recessed lighting that does not extend below the surface of the canopy.
  - c) The architectural design plans for the retail area must include an aesthetically appropriate comprehensive tenant sign program.
- 2) Mechanical Equipment
- a) Electrical and mechanical equipment, generators, etc., whether located on the roof or on the ground, must be screened from public view in a manner compatible with the architectural elements of the building or by the use of berms, walls, fences, plant material, placement underground, or other appropriate means.
  - b) Noise producing mechanical equipment should be located and screened so as to moderate any impact upon adjacent uses. This includes loading areas that may service refrigeration trucks in proximity to residential.
- 3) Service Areas
- a) Service, loading, and trash disposal areas including exterior dumpsters and compactors should occur away from or be screened from streets, common areas, parks and plazas. Such enclosures must have self-closing solid or semi-solid metal gates. Chain link fencing, with or without “slats” or inserts is not permitted.
- 4) There shall be a consistent architectural theme or style incorporated into the retail area.
- a) Materials/color palate should be complementary to surrounding residential development.
  - b) Design features should include variation in roof form/materials, massing, detailing, offset wall planes, shade and shadow control, asymmetrical facades, building materials, and/or varied coloration.
  - c) Tree lined, ample sidewalks shall be provided along the main street and the streets that form the perimeter of the retail area.
  - d) The use of awnings, iron work, formal and informal outdoor seating, tables, umbrellas, pennants, and other special treatments should be used to create a sense of place and a comfortable, inviting environment
- 5) The architectural design plans for the retail area must include a comprehensive tenant sign program and criteria.

## C. LANDSCAPE DESIGN

### 1) General Requirements

- a) Landscaping must be installed as an integral feature of each project. This includes finished grading, seeding, sodding, decorative ground cover, foundation plantings, shrubs, shade trees, flowering trees and evergreen trees. Irrigation is recommended in critical locations.
- b) The proposed landscape design concept must:
  - i) Reinforce architectural design objectives.
  - ii) Buffer views of parking areas, service areas, mechanical equipment, and utility boxes, etc. with a combination of deciduous and evergreen trees and shrubs.
  - iii) Buffer storm water management facilities.
  - iv) Provide canopy trees along streets, parking aisles and within planting islands.
  - v) Provide color, texture and visual interest.
- c) Parking areas must be buffered either by existing vegetation, new landscape material, landscaped berms, evergreen hedges or decorative walls.
- d) Landscape islands and areas must be planted with trees, shrubs, grass, or ground cover.

- e) Landscape plans must be prepared by and signed and sealed by a professional landscape architect or engineer.

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## Destination Retail

### **VI. HEALTHCARE DESTINATION RETAIL, OFFICE, & EMPLOYMENT GUIDELINES**

Due to the complexity of design the details of the ~~Hospital/Medical~~ Destination Retail area will be evaluated in detail with the detailed Site Plan. In developing the Destination Retail ~~Hospital/Medical~~ area the following must be employed in the design. It is recognized that the facilities around or in support of the retail center ~~hospital~~ will grow and develop over time. Later phases will likely provide for increased density. Planning for and flexibility in the design of the initial stages should be considered so as not to preclude the future expansions.

A. SITE DESIGN - The following general requirements and design guidelines are to be employed in the site design.

#### 1) General Requirements

##### Public Streets in the commercial districts shall have sidewalks on both sides.

a) Public streets shall be defined and reinforced by elements which may include one or a combination of the following:

- i. building facades
- ii. masonry walls
- iii. decorative fences
- iv. hedges
- v. related landscape improvements to establish street edges.

Buildings not subject to Destination Retail ~~healthcare~~ uses, along public streets should create a street edge or face a public plaza or open space at significant nodal points

- b) Graded slopes should not exceed 3:1. Retaining walls are recommended where slopes exceed 3:1. Slopes must be properly stabilized to prevent erosion. Lawn areas should have slopes of at least 2% to prevent standing water.
  - c) Setbacks from property lines for structures will be established during Site Plan review. Foundation planting between the building and the curb of a parking bay is encouraged other than in circumstances such as retail buildings with sidewalks along their front façade.
  - d) Street entry drives shall be flanked on both sides by a minimum of one of the street edge elements as outlined in 1(a) above. Discretion by the designer and reviewer should be employed to recognize site constraints and future phases.
  - e) A comprehensive pedestrian circulation system of sidewalks and/ or walkways should be provided within a parcel, with connections to adjacent parcels and adjacent pedestrian networks. The circulation system should be developed considering the source, needs, and destination of pedestrians ~~and of ambulation needs of patients~~ and other special populations.
  - f) Drainage systems must conform to County requirements. Any storm water retention pond perimeters must be landscaped.
- #### 2) Fences, Barriers and Walls
- a) Permanent perimeter or property line fencing (other than necessary retaining walls, screen walls or decorative streetscape walls) is discouraged in the office, employment and Destination Retail ~~healthcare~~ areas. Fences other than decorative are not allowed in front of buildings except where requirements for security, public and/or patient pedestrian safety is essential. Security gates should not be allowed unless warranted by special circumstances. Security gates or barriers can be used if their location is such that they will not unnecessarily impede the community's local pedestrian or vehicular movements and are designed to be aesthetically compatible with the project's building and site.

- b) Permanent chain link fencing is prohibited. In locations that are hidden from public view, a limited amount of black, vinyl-coated chain link fencing with posts of wood or metal painted black to match the fence or decorative aluminum fence can be used. Galvanized (silver) chain link fencing is prohibited except for temporary construction use.
  - c) Retaining walls shall be designed to be architecturally compatible with the community.
- 3) Parking Areas
- a) Parking areas must be buffered either by existing vegetation, new landscape material, decorative walls, landscaped berms, evergreen hedges, or a combination of these techniques.
  - b) Landscaping and tree islands must be used to reduce the visual impact of large, paved surfaces and parked cars.
  - c) Where curbs are proposed in a parking area they shall be concrete. Railroad tie or asphalt curbs are not acceptable. Wheel stops are not encouraged and shall only be used in a local condition ~~and not project wide, such as where necessary to~~ allow drainage.
  - d) Where parking structures subject to ~~healthcare-Destination Retail~~ uses or other future uses in same phase, are visible from streets, they— should be designed with architectural detail consistent with the architectural character of the community. Unless limited by dimensions, sloped ramps should be located internal to the structure so that they are not visible along front facades. Attention should be given to the design of parking garage interior lighting to avoid light spillage. Vehicular access to and from parking should be designed with the consideration of the public safety and ~~ambulation needs of patients and other special populations and~~ to minimize pedestrian conflicts
  - e) Light fixtures on top of parking structures should be limited in height and shielded as appropriate, ~~unless required by the FAA for emergency services.~~
  - f) Parking areas may have control gates for parking control purposes.
- 4) Site Lighting
- a) High pressure sodium vapor (yellow-orange) lighting is prohibited. Lighting must be a “white” light such as metal halide.
  - b) Pole mounted light fixtures must direct the light downward, and the lens must not project below the housing of the fixture. Fixture height should not exceed 30 feet above grade for any office or commercial use located adjacent to or across the street from a proposed residential use.
  - c) Decorative fixtures may be used and should be compatible with the fixtures proposed by other developments within Cabin Branch along pedestrian walkways.
  - d) All private exterior lighting must be full cut-off and all up lighting must have hoods/deflectors.
  - e) ~~Helicopter pad site lighting shall comply with FAA requirements.~~

B. ARCHITECTURAL DESIGN - While not prescribing to a specific architectural style, the design of all office, employment, and ~~Destination Retailhealthcare~~ building elevations in terms of size, massing, details and the use of materials and colors, should be compatible and complimentary- with the architectural character of the community. Buildings should be sympathetic to their natural environment.

1) General Requirements

a) Building elevations are important and should be designed “in the round” with suitable details and materials on all sides to the extent practical given service/ loading requirements. Buildings should have an entrance facing a street or commons. ~~For buildings subject to healthcare uses, additional entrances are allowed where special requirements for security, public and/or patient safety and consideration of ambulation needs of patients and other special populations is essential.~~ Large or long expanses of walls must be broken up by appropriate detailing such as reveals, piers, pilasters, projections or decorative patterns.

b) Canopies extending from the retail uses must contain recessed lighting that does not extend below the surface of the canopy.

c) The architectural design plans for the retail area must include an aesthetically appropriate comprehensive tenant sign program.

## 2) Mechanical Equipment

a) Electrical and mechanical equipment, generators, etc., whether located on the roof or on the ground, must be screened from public view in a manner compatible with the architectural elements of the building or by the use of berms, walls, fences, plant material, placement underground, or other appropriate means.

b) Noise producing mechanical equipment should be located and screened so as to moderate any impact upon adjacent uses. This includes loading areas that may service refrigeration trucks in proximity to residential.

## 3) Service Areas

a) Service, loading, and trash disposal areas including exterior dumpsters and compactors should occur away from or be screened from streets, common areas, parks and plazas. Such enclosures must have self-closing solid or semi-solid metal gates. Chain link fencing, with or without “slats” or inserts is not permitted.

## 4) There shall be a consistent architectural theme or style incorporated into the retail area.

a) Materials/color palate should be complementary to surrounding residential development.

b) Design features should include variation in roof form/materials, massing, detailing, offset wall planes, shade and shadow control, asymmetrical facades, building materials, and/or varied coloration.

c) Tree lined, ample sidewalks shall be provided along the main street and the streets that form the perimeter of the retail area.

d) The use of awnings, iron work, formal and informal outdoor seating, tables, umbrellas, pennants, and other special treatments should be used to create a sense of place and a comfortable, inviting environment

## 5) The architectural design plans for the retail area must include a comprehensive tenant sign program and criteria.

## C. LANDSCAPE DESIGN

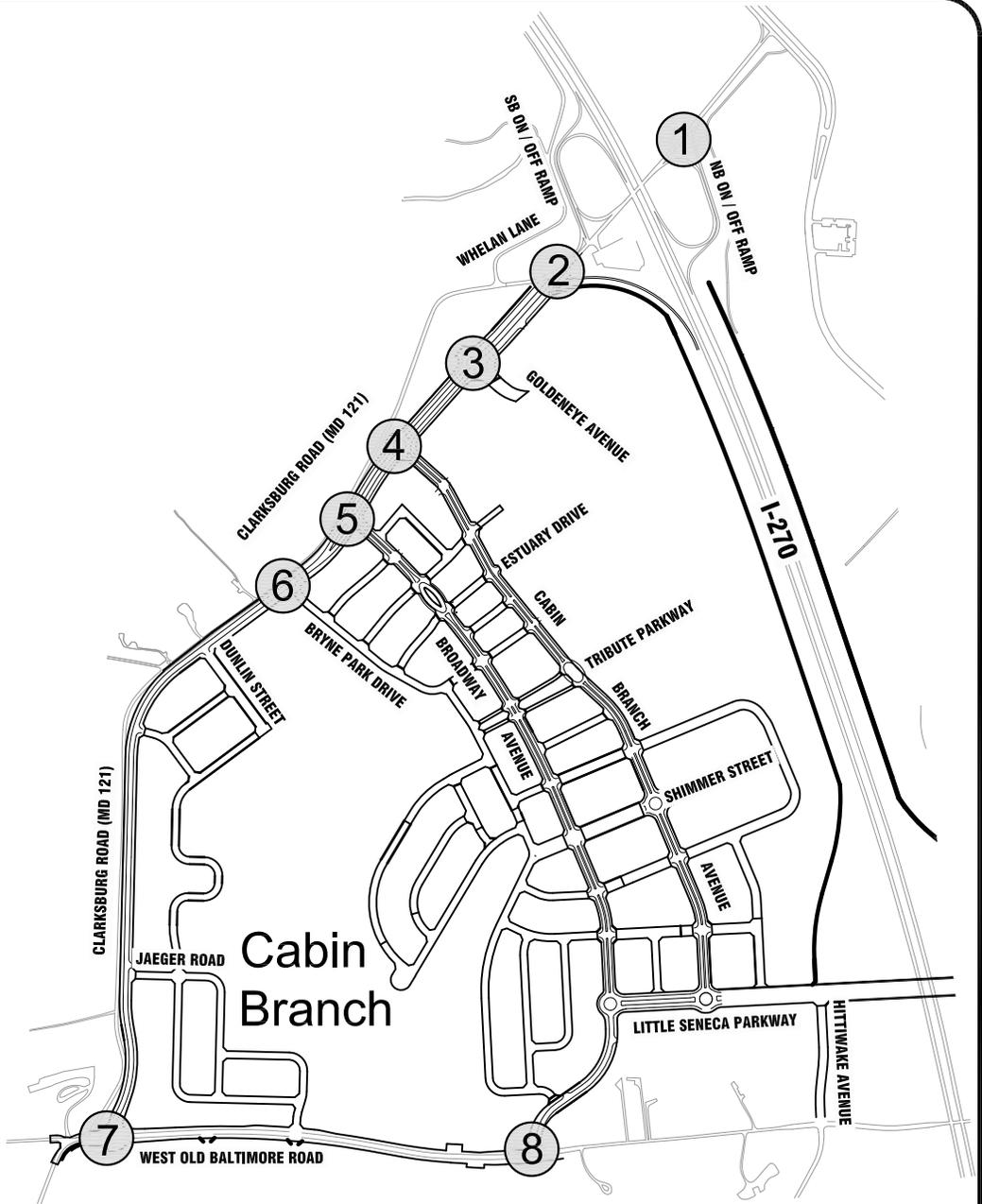
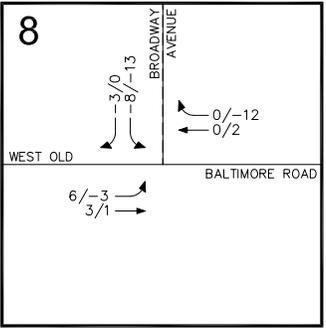
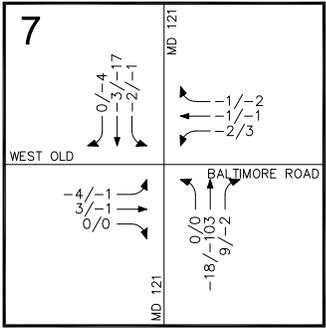
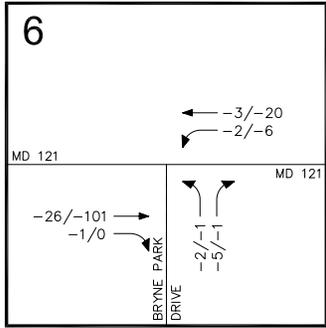
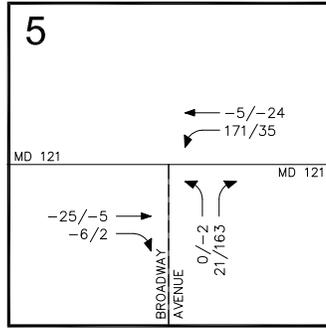
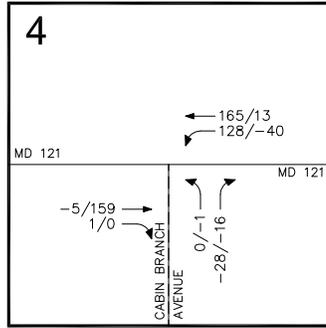
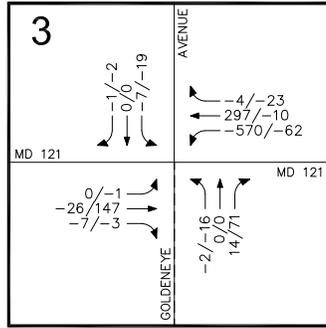
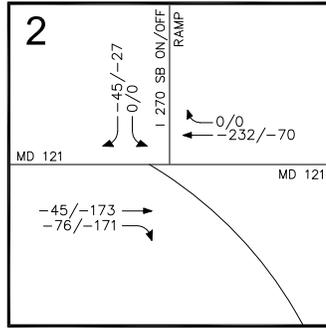
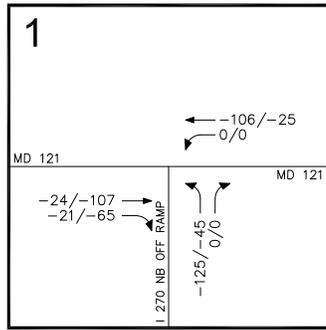
### 1) General Requirements

a) Landscaping must be installed as an integral feature of each project. This includes finished grading, seeding, sodding, decorative ground cover, foundation plantings, shrubs, shade trees, flowering trees and evergreen trees. Irrigation is recommended in critical locations.

b) The proposed landscape design concept must:

i) Reinforce architectural design objectives.

- ii) Buffer views of parking areas, service areas, mechanical equipment, and utility boxes, etc. with a combination of deciduous and evergreen trees and shrubs.
  - iii) Buffer storm water management facilities.
  - iv) Provide canopy trees along streets, parking aisles and within planting islands.
  - v) Provide color, texture and visual interest.
- c) Parking areas must be buffered either by existing vegetation, new landscape material, landscaped berms, evergreen hedges or decorative walls.
  - d) Landscape islands and areas must be planted with trees, shrubs, grass, or ground cover.
  - e) Landscape plans must be prepared by and signed and sealed by a professional landscape architect or engineer.



\* Base Plans are conceptual and subject to change with site plan submittal.

Figure A  
Differences in Site-Generated Traffic Forecasts  
(CB 2004 LATR vs. CB 2014 With Outlet)

AM PEAK HOUR  
PM PEAK HOUR  
000/000





DEPARTMENT OF PERMITTING SERVICES

Isiah Leggett  
County Executive

Diane R. Schwartz Jones  
Director

September, 30 2014

Mr. Philip R. Hughes, P.E.  
Rodgers Consulting, Inc.  
19847 Century Blvd., Suite 200  
Germantown, Maryland 20874

Re: **Final Water Quality Plan** Request for  
Clarksburg Premium Outlets at Cabin Branch  
Preliminary Plan #: 12003110B  
SM File #: 264517  
Tract Size/Zone: 84.24 acres/MXPD  
Total Concept Area: 84.24  
Watershed: Little Seneca

Dear Mr. Hughes:

Based on a review by the Department of Permitting Services Review Staff, the Final Quality Plan (FWQP) for the above mentioned site is **acceptable**. The Final Water Quality Plan proposes to meet required stormwater management goals via permeable pavement, biowales and micro-bioretenion. All of the previously required goals and conditions noted on the Preliminary Water Quality Plan still apply. A BMP monitoring fee will be required for the area of this FWQP.

The following **conditions** will need to be addressed **during** the detailed sediment control/stormwater management plan stage:

1. A detailed review of the stormwater management computations will occur at the time of detailed plan review.
2. Provide safe conveyance of the 10 and 100 year storms through Pond#11 and safe conveyance and non-erosive outfall of the 10 year storm in drainage area #12.

This list may not be all-inclusive and may change based on available information at the time of Final Water Quality Plan review.

Payment of a stormwater management contribution in accordance with Section 2 of the Stormwater Management Regulation 4-90 is **not required**.

This letter must appear on the sediment control/stormwater management plan at its initial submittal. The concept approval is based on all stormwater management structures being located outside of the Public Utility Easement, the Public Improvement Easement, and the Public Right of Way unless specifically approved on the concept plan. Any divergence from the information provided to this office; or additional information received during the development process; or a change in an applicable Executive Regulation may constitute grounds to rescind or amend any approval actions taken, and to reevaluate the site for additional or amended stormwater management requirements. If there are subsequent additions or modifications to the development, a separate concept request shall be required.

Mr. Philip Hughes  
September 30, 2014  
Page 2

If you have any questions regarding these actions, please feel free to contact Leo Galanko at 240-777-6242.

Sincerely,



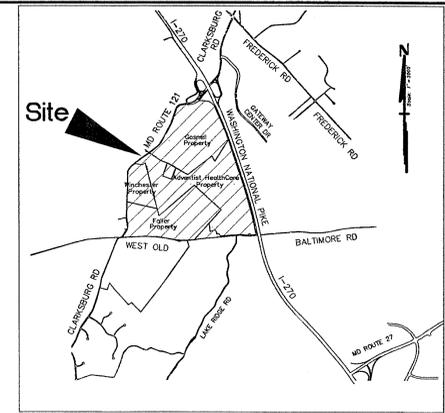
Mark C. Etheridge, Manager  
Water Resources Section  
Division of Land Development Services

MCE: me lmg

cc: C. Conlon  
SM File # 264517

ESD Acres:	approx. 84.24
STRUCTURAL Acres:	N/A
WAIVED Acres:	N/A

# STORMWATER MANAGEMENT & WATER QUALITY CONCEPT PLAN CABIN BRANCH



Cabin Branch

VICINITY MAP  
1" = 3000'



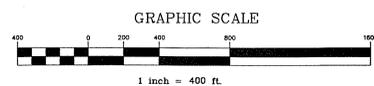
SHEET INDEX REVISED SHEETS

1	COVER SHEET
2	EXISTING CONDITIONS
3	GOSNELL PROPERTY
3A	GOSNELL PROPERTY - REVISED DA 11, 12, & 13
4	ADVENTIST PROPERTY
4A	ADVENTIST PROPERTY - REVISED DA 1, 12 & 15
5	WINCHESTER PROPERTY
6	FALLER PROPERTY
7	DRAINAGE AREA 1
8	DRAINAGE AREA 2
9	DRAINAGE AREAS 3 & 9
10	DRAINAGE AREA 4
11	DRAINAGE AREAS 5 & 6
12	DRAINAGE AREAS 7 & 8
13	DRAINAGE AREA 11
14	DRAINAGE AREAS 12, 13, & 14
15	DRAINAGE AREA 15
15A	REVISED AREA - NORTH
15B	REVISED AREA - SOUTH
16	DETAILS

LEGEND:

	10' EX. CONTOUR
	2' EX. CONTOUR
	RIGHT OF WAY LINE
	100 YR. FLOODPLAIN
	EX. INTERMITTENT STREAM OR PERENNIAL STREAM
	SOILS
	WETLANDS, NATURAL REGENERATION
	WETLANDS BUFFER
	STREAM VALLEY BUFFER
	PROPERTY LINE
	PROPOSED DRAINAGE DIVIDE
	PROPOSED WATER QUALITY DRAINAGE DIVIDE
	REVISED AREA

PLAN  
SCALE: 1" = 400'



**CALL "MISS UTILITY" AT  
1-800-257-7777  
48 Hours Before Start Of Construction**

The excavator must notify all public utility companies with underground facilities in the area of proposed excavation and have those facilities located by the utility companies prior to commencing excavation. The excavator is responsible for compliance with requirements of Chapter 36A of the Montgomery County Code.

REVISION	DATE	REVISION	DATE	REVISION	DATE
REVISED PER DPA 13-02	05/20/14				

**Contract Purchaser:**  
Gosnell et al.  
8130 Boone Blvd.  
Vienna, VA 22182  
Tel: (703) 893-1221, Fax: (703) 893-6630

## STORMWATER MANAGEMENT & WATER QUALITY CONCEPT PLAN COVER SHEET



Rodgers Consulting, Inc.  
5260 Gaither Road  
Cathetersburg, MD 20877  
301.948.4700  
301.948.6256 (fax)  
301.253.6689  
www.rodgers.com

BY	DATE
BASE DATA	R&A
DESIGNED	RF 12/02
DRAWN	RF 12/02
REVIEWED	

RODGERS CONTACT:

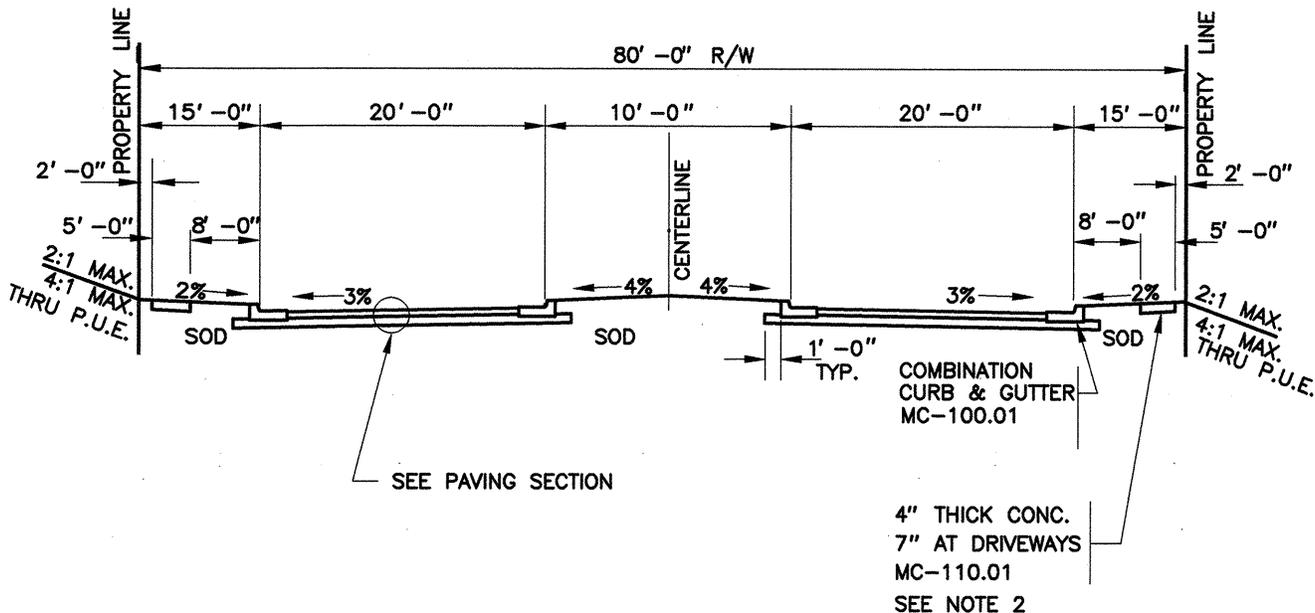
RELEASE FOR

BY: \_\_\_\_\_ D/TE: \_\_\_\_\_

## Cabin Branch

ELECTION DISTRICT No. 2  
CLARKSBURG, MONTGOMERY COUNTY, MARYLAND

SCALE: 1" = 400'
JOB No. 782A
DATE: DEC 2002
SHEET No. 1 of 15



TYPICAL ROAD SECTION

3" BITUMINOUS CONCRETE SURFACE COURSE IN TWO 1 1/2" LAYERS
7" BITUMINOUS CONCRETE BASE COURSE
8" GRADED AGGREGATE BASE COURSE IN TWO LAYERS
*
APPROVED SUBGRADE

PREFERRED

3" BITUMINOUS CONCRETE SURFACE COURSE IN TWO 1 1/2" LAYERS
9" BITUMINOUS CONCRETE BASE COURSE IN TWO 4 1/2" LAYERS
4" GRADED AGGREGATE BASE COURSE
*
APPROVED SUBGRADE

ALTERNATE

\* SUBGRADE DRAINS REQUIRED  
SEE MC-525.01

PAVING SECTION  
GENERAL NOTES

- REFER TO MARYLAND STATE HIGHWAY ADMINISTRATION SPECIFICATIONS FOR MATERIALS AND METHODS OF CONSTRUCTION.
- WHERE BUILDINGS ARE GENERALLY LOCATED AT THE PROPERTY LINE, THE SIDEWALK SHALL EXTEND FROM CURB TO PROPERTY LINE WITH A 5' WIDE BY 5' LONG (OR BY VARIABLE OR CONTINUOUS LENGTH) SPACE FOR TREE PLANTINGS.
- OBSTRUCTIONS IN THE SIDEWALK THAT VIOLATE THE CLEAR WALKING SURFACE REQUIREMENTS OF THE "AMERICANS WITH DISABILITIES ACT" OF 1990 WILL NOT BE ALLOWED.
- TYPICAL TOP OF CURB (LOW SIDE) ELEVATION = FLOWLINE (HIGH SIDE) ELEVATION -0.19' (FOR 6" CURB HEIGHT).
- THIS STANDARD PROVIDES ONE TRAVEL LANE IN EACH DIRECTION WITHOUT LEFT TURN STORAGE LANES.
- WHEN A TRAFFIC BARRIER IS WARRANTED INCREASE THE RIGHT OF WAY TO ALLOW THE TRAFFIC BARRIER TO BE PLACED OUTSIDE OF THE SIDEWALK.

P:\JBT\STD\MC219.01 7-1-94 9:39:05 am EST

APPROVED JAN 5/96  
DATE

*Indy Madon*  
DIRECTOR, DEPT. OF TRANS.

*Edouard*  
CHIEF, DIV. OF ENG. SERVICES

REVISED

MONTGOMERY COUNTY  
DEPARTMENT OF TRANSPORTATION

**COMMERCIAL/INDUSTRIAL  
DUAL ROAD**  
80' RIGHT OF WAY

**STANDARD NO. MC-219.01**